

ADDENDUM NO. ONE (1)

AIRFIELD PAVEMENT REHABILITATION – PHASE 2

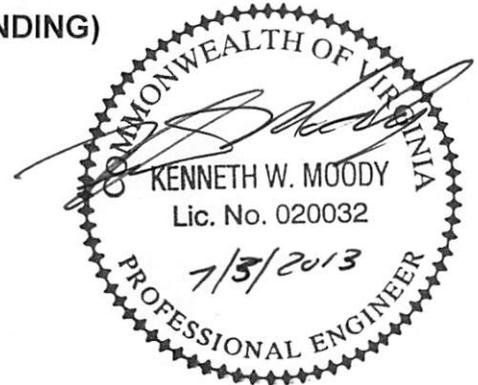
**LYNCHBURG REGIONAL AIRPORT
LYNCHBURG, VIRGINIA**

**AIP PROJECT NO. 3-51-0029-036 (PENDING)
DELTA PROJECT NO. 12121**

JULY 3, 2013

From: Delta Airport Consultants, Inc.
1805 Sardis Road North, Suite 101
Charlotte, North Carolina 28270

To: All Bidders of Record



This Addendum is hereby made a part of the contract documents and specifications of the above referenced project. All other requirements of the original specification shall remain in effect in their respective order. Acknowledge receipt of this addendum by inserting its number and date in the proposal form.

PROPOSAL

1. **REPLACE** the "PROPOSAL" with the attached "REVISED PROPOSAL" of Addendum No. One (1).

SPECIFICATIONS

2. **REPLACE** the "INVITATION TO BID" with the attached "REVISED INVITATION TO BID" of Addendum No. One (1).
3. **REPLACE** the "PRE-BID CONFERENCE" with the attached "REVISED PRE-BID CONFERENCE" of Addendum No. One (1).
4. **REVISE** Item No. 1 of the "SUPPLEMENTAL INSTRUCTIONS TO CONTRACTORS" to read as follows:
 - "1. The Contractor shall perform, with his organization, an amount of work equal to at least 30 percent of the total contract cost."

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5. **REVISE** the second sentence of the first paragraph of Item No. 7 of the “SUPPLEMENTAL INSTRUCTIONS TO CONTRACTORS” to read as follows:

“To be given consideration, requests for clarification must be received in writing at least five (5) business days prior to the date fixed for opening of bids.”

6. **REPLACE** the “BID BOND” with the attached “BID BOND” of Addendum No. One (1).

PLANS

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7. **REPLACE** the “SUMMARY OF QUANTITIES” for Schedule 1 and Schedule 2 with the attached “SUMMARY OF QUANTITIES” of Addendum No. One (1).

SHEET 4 OF 69

8. **REVISE** General Note No. 9 to read as follows:

“9. RUNWAY AND/OR TAXIWAY LIGHTS SHALL BE DE-ENERGIZED OR PROPERLY COVERED IN A MANNER APPROVED BY THE ENGINEER ON CLOSED RUNWAY(S) AND ALL CLOSED SECTIONS OF TAXIWAYS (NO SEPARATE PAY ITEM).”

9. **REVISE** General Note No. 20 to read as follows:

“20. THE CONTRACTOR SHALL PROVIDE A LIGHTWEIGHT STRAIGHT EDGE IN ACCORDANCE WITH THE TECHNICAL SPECIFICATIONS. THE STRAIGHT EDGE SHALL HAVE A HANDLE WITH A TWO (2) FOOT HEIGHT. THE CONTRACTOR SHALL PROVIDE AN EMPLOYEE TO ASSIST THE RESIDENT PROJECT REPRESENTATIVE IN CONDUCTING THE STRAIGHT EDGE TEST REQUIRED BY THE TECHNICAL SPECIFICATIONS (NO SEPARATE PAY ITEM).”

10. **REVISE** Haul Route Note No. 2 to read as follows:

“2. THE CONTRACTOR SHALL WASTE MILLINGS FROM THE PROJECT ON THE EXISTING AIRPORT SERVICE / HAUL ROADS WITHIN THE PROJECT LIMITS AS ORDERED BY THE ENGINEER. THE ASPHALT SHALL BE BROKEN UP AND ROLLED / COMPACTED TO THE SATISFACTION OF THE ENGINEER AND OWNER. REMAINING

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MATERIAL SHALL BE STOCKPILED ON AIRPORT PROPERTY AS ORDERED BY THE ENGINEER.”

- 11. REVISE** Demolition Note No. 8 to read as follows:

“8. THE CONTRACTOR SHALL PROTECT AND RETAIN ALL ELECTRICAL DEMOLITION ITEMS WHICH THE OWNER CHOOSES TO MAINTAIN POSSESSION OF. THESE ITEMS INCLUDE, BUT ARE NOT LIMITED TO, FIXTURES, TRANSFORMERS, SIGNS, AND SIGN PANELS. THE CONTRACTOR SHALL DELIVER THE APPLICABLE ITEMS TO A DESIGNATED STORAGE LOCATION ON THE AIRPORT AS ORDERED BY THE OWNER.”

- 12. REVISE** Demolition Note No. 18 to read as follows:

“18. THE CONTRACTOR SHALL MAINTAIN THE RUNWAY AND TAXIWAY LIGHTING SYSTEMS/CIRCUITS FOR OPEN RUNWAYS OR OPEN SEGMENTS OF TAXIWAYS AT ALL TIMES. THE USE OF TEMPORARY CABLE SHALL BE ANTICIPATED (NO PAY ITEM).”

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- 13. REPLACE** Sheet 11 of 69 with the attached Sheet 11 of 69 of Addendum No. One (1).

SHEET 13 OF 69

- 14. REPLACE** Sheet 13 of 69 with the attached Sheet 13 of 69 of Addendum No. One (1).

SHEET 20 OF 69

- 15. REPLACE** Sheet 20 of 69 with the attached Sheet 20 of 69 of Addendum No. One (1).

SHEET 22 OF 69

- 16. REPLACE** Sheet 22 of 69 with the attached Sheet 22 of 69 of Addendum No. One (1).

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SHEET 23 OF 69

- 17. REPLACE** Sheet 23 of 69 with the attached Sheet 23 of 69 of Addendum No. One (1).

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- 18. REPLACE** the “Trench, Conduit, & Duct Bank Schedule” with the revised schedule shown on Attachment No. 44-1 of Addendum No. One (1)

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- 19. REPLACE** the “Trench, Conduit, & Duct Bank Schedule” with the revised schedule shown on Attachment No. 44-1 of Addendum No. One (1).

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- 20. REPLACE** the “Trench, Conduit, & Duct Bank Schedule” with the revised schedule shown on Attachment No. 44-1 of Addendum No. One (1).
- 21. REVISE** the flag notes at each end of the existing Taxiway “B” duct bank at approximately Station 56+42 as shown on Attachment No. 46-1 of Addendum No. One (1).

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- 22. REPLACE** the “Trench, Conduit, & Duct Bank Schedule” with the revised schedule shown on Attachment No. 44-1 of Addendum No. One (1).
- 23. REVISE** the flag note at the proposed 4-Unit Pullcan Plaza at approximately Station 70+55, Baseline “B” as shown on Attachment No. 47-1 of Addendum No. One (1).

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- 24. ADD** the attached “BASE MOUNTED FIXTURES – OPTIONAL PRECAST BASE DETAIL” of Addendum No. One (1).

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- 25. REPLACE** Sheet 56 of 69 with the attached Sheet 56 of 69 of Addendum No. One (1).

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SHEET 61 OF 69

- 26. REPLACE** Sheet 61 of 69 with the attached Sheet 61 of 69 of Addendum No. One (1).

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ATTACHMENTS:

1. Questions and Answers
2. Revised Invitation to Bid
3. Revised Pre-Bid Conference
4. Revised Bid Bond
5. Attachment No. 2-1
6. Attachment No. 2-2
7. Sheet No. 11 of 69
8. Sheet No. 13 of 69
9. Sheet No. 20 of 69
10. Sheet No. 22 of 69
11. Sheet No. 23 of 69
12. Attachment No. 44-1
13. Attachment No. 46-1
14. Attachment No. 47-1
15. Attachment No. 53-1
16. Sheet No. 56 of 69
17. Sheet No. 61 of 69
18. Revised Proposal

END OF ADDENDUM NO. ONE (1)

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AIRFIELD PAVEMENT REHABILITATION – PHASE 2

**LYNCHBURG REGIONAL AIRPORT
LYNCHBURG, VIRGINIA**

QUESTIONS AND ANSWERS

This list of Questions and Answers includes those asked at the Pre-Bid Conference, during the bidding period, and those asked on similar AIP-funded projects.

1. **QUESTION:** Is topsoil stripping required for embankment areas?

ANSWER: Yes. The area beneath the embankment shall be stripped in accordance with Item P-152-2.5 "Preparation of Embankment".

2. **QUESTION:** Is topsoil stripping and redistribution required for this project?

ANSWER: Yes. Topsoil shall be stripped from both cut and fill areas, salvaged, stockpiled, and then redistributed in all unpaved areas (both cut and fill) upon completion of grading. There will be no separate measurement or payment for handling and placement of topsoil. The Contractor should note that the final grades shown on the plans are finished grades, thus requiring over-excavation (no pay item) to compensate for the layer of topsoil. Refer to Item P-152-2.5 and Item P-152-2.10 for additional information.

3. **QUESTION:** Will topsoil be redistributed upon completion of the grading operations?

ANSWER: Yes. Topsoil shall be salvaged from stripping or grading operations. The topsoil shall be distributed in accordance with Item P-152-2.10 "Topsoil".

4. **QUESTION:** What constitutes muck or unsuitable excavation?

ANSWER: Muck and unsuitable are defined in Item P-152-1.2.c and Item P-152-1.3 respectively, and are materials which are considered unsuitable for use in embankment construction due to their physical properties (containing organics, decaying materials, etc.). High in-place moisture content will not be grounds for classifying a material as muck or unsuitable. The Contractor is

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expected to aerate and dry the material, as necessary, to reach optimum moisture contents.

5. QUESTION: Will high in-place moisture content be grounds for classifying a material as muck?

ANSWER: No. The Engineer will determine if the material is suitable or “muck” based upon the physical properties of the material (e.g., liquid limits, plastic limits, CBR, etc.) High in-place moisture content will not be grounds for determining material to be “muck”. The Contractor shall be responsible for proper handling and drying of all material.

6. QUESTION: Does the price for muck excavation include backfill?

ANSWER: No. As outlined in Item P-152-2.2.b, excavated areas shall be refilled with P-154 Subbase.

7. QUESTION: Will there be any relief to the Contractor if aerating and drying saturated material is a difficult endeavor?

ANSWER: No additional payment will be made to the Contractor for handling and drying of saturated material, whether his attempts are successful or unsuccessful. The Contractor may, at his option and his own costs, replace, supplement, or treat the saturated material to improve workability. Supplements or treatments may include mixing with a different soil material, or treating with lime or cement.

8. QUESTION: Will the Contractor be required to spread topsoil in the cut section at the completion of the project?

ANSWER: Yes. In accordance with Item P-152-2.10 “Topsoil”, the Contractor shall be required to spread topsoil in the cut areas to a uniform depth of 4-6 inches after compaction. There will be no separate measurement for payment for the handling and placing of the topsoil in areas of new construction. The Contractor should note that the final grades shown on the plans are finished grades, thus requiring additional excavation (no pay item) to compensate for the layer of topsoil.

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9. QUESTION: Will the Contractor be paid twice if he has to stockpile and handle excavation material twice?
- ANSWER: No. In accordance with Item P-152-3.1 "Method of Measurement", separate measurement shall not be made for handling or stockpiling materials.
10. QUESTION: Will any additional compensation be made for adverse field conditions such as excavation and/or trenching through rock?
- ANSWER: No. All bidders should examine the site and satisfy themselves as to the field conditions before submitting their bid.
11. QUESTION: Is the Contractor responsible for construction layout and staking?
- ANSWER: Yes. In accordance with Section 50-06 "Construction Layout and Stakes", the Engineer shall furnish the Contractor with horizontal and vertical control only in the form of bench marks, and the Contractor must furnish all additional stakes for the layout and construction of the work and preserving all stakes and benchmarks through the duration of the project.
12. QUESTION: Does the Engineer have the option to test subgrades and any other non-stabilized layers of the pavement box using the proof rolling method?
- ANSWER: Yes. If the type of material precludes testing in accordance with the respective specification, the Engineer may utilize proof rolling with a loaded tandem axle dump truck or similar piece of rubber tired equipment to identify areas needing repairs. Any areas that rut or pump shall be repaired. In addition, the Engineer may elect to test any compacted area utilizing proof rolling. In this instance, any areas that rut or pump also shall be repaired, even if test results have been obtained that show density has been achieved.

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13. QUESTION: Will the Contractor be responsible for subgrade acceptance testing?
- ANSWER: No. The Owner will provide the personnel to complete the required acceptance testing. However, the Contractor will be responsible for his own quality control testing. Acceptance testing will not be performed as a quality check for the Contractor. Any failed acceptance test shall be billed to the Contractor. Reference is directed to Section 100 "Contractor's Quality Control Requirements" and Special Provision SP-19 "Testing General."
14. QUESTION: Does pavement removal only include the depth of pavement?
- ANSWER: No. The pavement removal includes the removal of the bituminous surface and base course(s).
15. QUESTION: May recycled asphalt products be included in the P-401 Bituminous Surface Course?
- ANSWER: No.
16. QUESTION: May recycled asphalt products be included in the P-403 Bituminous Base Course?
- ANSWER: No.
17. QUESTION: May the Contractor use pre-cast sign pads or light bases for areas within the Runway 4-22 safety area?
- ANSWER: Yes; however, pre-cast concrete sign pads or light bases shall be monolithic as shown in the plan details.
18. QUESTION: May the Contractor use pre-cast electrical duct?
- ANSWER: No. Cast in-place concrete encased duct must be used on this project.

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19. QUESTION: Is the Contractor responsible for repair to utilities in service which are damaged by the Contractor's men or equipment?
- ANSWER: Yes.
20. QUESTION: Is the conduit and trench under the sign foundations considered incidental to the unit price per sign?
- ANSWER: Yes. Any item required to install the sign which is located within the boundary of the sign foundation (above or below) shall be considered incidental to the unit price for the sign.
21. QUESTION: May a high compression ground connection be used in lieu of a Cadweld connection?
- ANSWER: No. Only exothermic ground connections shall be utilized.
22. QUESTION: Will any electrical equipment be reused?
- ANSWER: Yes, the items to be reused or relocated are indicated on the plans.
23. QUESTION: Are the DBE (Disadvantaged Business Enterprise) percent participation figures goals or requirements?
- ANSWER: The DBE percent participation figures are goals which the Contractor shall make a good faith effort to obtain. The Contractor will be required to submit evidence of good faith effort with the proposal. The Owner encourages the Contractor to utilize DBE subcontractors wherever practical.
24. QUESTION: Will Davis Bacon wage rates be required on this project?
- ANSWER: Yes. All affected Contractor personnel and all subcontractors should be placed in one of the wage rate classifications included in Section 200 of the specifications. The Contractor may submit a request for a classification with supporting wage documentation if necessary.

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25. QUESTION: Is the Contractor required to provide an office trailer for the Engineer?
- ANSWER: Yes. The office trailer shall meet the requirements of Section 60-05.
26. QUESTION: Will the resident project representative's field office and associated equipment be measured for payment?
- ANSWER: No, the cost shall be considered incidental to Item No. 1, P-100 "Mobilization".
27. QUESTION: When will the project notice-to-proceed be issued?
- ANSWER: Upon successful completion of the FAA grant and construction contract, it is anticipated that the notice-to-proceed will be issued in September 2012.
28. QUESTION: Are there any equipment height restrictions?
- ANSWER: Yes. Equipment shall conform to the heights and distances specified in Section 130 "Airport Safety Requirements During Construction". Any equipment or objects penetrating the Part 77 imaginary surfaces shall be coordinated in advance with the Engineer. Obstructions penetrating the Part 77 surfaces shall be marked and/or lighted in accordance with FAA guidelines.
29. QUESTION: Does the Contractor have to bid both schedules?
- ANSWER: No.
30. QUESTION: What is the schedule for awarding the contract?
- ANSWER: The contract documents allow bid to be held for up to ninety (90) days; however, it is the Owner's intent to award the contract as soon as practically possible.

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31. QUESTION: Will 10 percent retainage be required?
- ANSWER: Yes. The FAA 10 percent retainage policy outlined in Section 90-06 "Partial Payments" will apply on this project.
32. QUESTION: Do the contract documents allow for any adjustment in contract time due to unusually adverse weather conditions?
- ANSWER: Yes. There are provisions in the contract documents for adjustment in contract time due to unusually adverse weather conditions. However, the contract time has been established based upon the receipt of normal precipitation for the project area. Refer to SP-14 "Contract Time Extensions" for additional information. This is a calendar day contract, therefore, it is anticipated that overtime and weekend work will be required to complete the project within the allotted contract time. Completion of the work within the prescribed contract time is essential to the Owner.
33. QUESTION: Will the Contractor be required to reseed areas of the project which yield a sparse cover from initial seeding?
- ANSWER: Yes. The Contractor will be responsible for obtaining a "good stand of grass with regard to color and density". No additional compensation will be made for overseeding.
34. QUESTION: Do all of the demolished structures need to be hauled off-site by the Contractor?
- ANSWER: Yes, with the exception of the residual materials from the designated areas of pavement milling; they will be retained by the Owner.
35. QUESTION: Is there an on-site disposal area for clean select material and material from the muck excavation?
- ANSWER: Yes. Clean excavated soils shall be placed as a controlled fill in the area shown on the plans. Material from the muck excavation and all other demolished, surplus, or waste materials shall be disposed of off airport property by the Contractor.

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36. QUESTION: Can bond forms from Surety companies be used in lieu of documents in specifications?
- ANSWER: Yes, as long as they "say" the same thing.
37. QUESTION: Do the bid item prices have to be written out long hand on the proposal?
- ANSWER: The proposal shall be completed in accordance with Section 20-07 "Preparation of Proposal". The bidder shall state the price (written in ink or typed) both in words and numerals.
38. QUESTION: Will the Contractor be responsible for damage to existing roads even if the Contractor uses legally loaded dump trucks?
- ANSWER: The Contractor shall preserve and protect the existing pavements from damage due to construction activities. Existing pavements which are damaged shall be repaired at the Contractor's expense.
39. QUESTION: Can the 5 gallon bucket to be used for the bucket aviation barricade be metal or plastic?
- ANSWER: Yes.
40. QUESTION: Is the Storm Water Pollution Prevention Plan (SWPPP) required to be sealed by a registered Professional Engineer?
- ANSWER: No.
41. QUESTION: Is hydroseeding required?
- ANSWER: Yes. As outlined in Item T-908-2.1 "Mulch Material", the mulch shall be cellulose-fiber or wood-pulp mulch commercially available for use in spray applications.

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42. QUESTION: Will the Contractor be required to pay for an additional resident project representative during nighttime shifts.
- ANSWER: No. The Owner shall provide all resident project representative services.
43. QUESTION: What permits are required to commence construction?
- ANSWER: The permits required shall be identified and procured by the Contractor prior to the start of construction. The Contractor shall coordinate which permits are required with Campbell County. The Contractor shall be required to obtain a Campbell County Business License and post the required Erosion and Sediment Control Bond.
44. QUESTION: Is there a water source available on-site?
- ANSWER: Yes. The Contractor may obtain water from existing hydrants located in the corporate area; however, the Contractor must provide a meter and meter any water used. The Contractor shall reimburse the Owner for the cost of the water.
45. QUESTION: Will the Contractor be required to treat the surface of all pavements to be overlaid with a soil sterilant?
- ANSWER: No. Treatment will only be required in cracks or other areas exhibiting vegetation.
46. QUESTION: Is the use of a certain type of software required for the SP-14 CPM Project Schedule?
- ANSWER: No.
47. QUESTION: Where will the test strip for the P-401 Bituminous Surface be located?
- ANSWER: The test strip will be located on Taxiway 'B3'.

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48. QUESTION: Is the Prime Contractor required to complete a certain percentage of the work?
- ANSWER: As outlined in Section 80-01, the Contractor shall perform, with his organization, an amount of work equal to at least 30 percent of the total contract amount.
49. QUESTION: Is the Contractor required to be licensed in the Commonwealth of Virginia at the time of bid?
- ANSWER: Yes.
50. QUESTION: Is an Excel file of the Proposal form available to bidders?
- ANSWER: No.
51. QUESTION: May the Contractor submit his bid on a computer printout?
- ANSWER: Yes, provided the format contains the Item No., Spec. No., approximate quantity, and description in the exact order provided in the Proposal. The printout shall be attached to the Proposal form provided for submission to the Owner.
52. QUESTION: If muck material is encountered above final grade, will pavement for excavation of the muck material be made under muck excavation or unclassified excavation?
- ANSWER: Unclassified excavation.
53. QUESTION: May the Contractor mix the P-304 Cement-Treated Base in place?
- ANSWER: No. As outlined in Item P-304-4.1 "Mixing", the mixer shall be a batch or continuous-flow type stationary mixer and shall be equipped with calibrated metering and feeding devices that introduce the aggregate, cement, and water into the mixer in the specified quantities.

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54. QUESTION: What is the maximum time allowed between mixing and compaction of the P-304 Cement Treated Base?
- ANSWER: In accordance with Item P-304-5.8 "Finishing", all placement, compaction, and finishing operations shall be completed within 2 hours from the start of mixing. Material not completed within the 2-hour time limit shall be removed and replaced at the Contractor's expense.
55. QUESTION: Will the gradation band for VDOT 21A Crushed Aggregate Base Course meet the gradation requirements for the P-304 Cement Treated Base?
- ANSWER: Yes. The Contractor may utilize VDOT 21A Crushed Aggregate for the P-304 Cement Treated Base. All appropriate certifications and test results must be submitted as per Item P-304 of the specifications.
56. QUESTION: Does all P-304 Cement Treated Base receive an application of cure material.
- ANSWER: Yes. Cement Treated Base shall be cured in accordance with Item P-304-5.10 "Curing".
57. QUESTION: Are all proposed reinforced concrete pipe to be O-ring joints?
- ANSWER: Yes. In accordance with Item D-701-3.4 "Joining Pipe", rubber ring gaskets shall be installed to form a flexible water tight seal. However, offset joint with profile gasket may be substituted provided it meets ASTM C443.
58. QUESTION: How will pavement be made for work completed before the construction notice-to-proceed?
- ANSWER: The only item that can be paid for prior to the construction notice-to-proceed is stored materials. Specifications Section 90-07, "Payment for Materials On Hand" addresses the requirements for receiving payment for stored materials.

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59. QUESTION: Will the Contractor be required to provide a 10-year employment history on all personnel working within the Air Operations Area (AOA)?

ANSWER: No. A 10-year employment history is only required for personnel that apply for an airport security badge. All personnel working within the AOA must obtain a badge or be under the direct supervision of a person with a badge. Personnel in charge of gate openings will also be required to have a badge.

60. QUESTION: Is the Contractor required to utilize a Registered Land Surveyor for construction stakeout?

ANSWER: No.

61. QUESTION: Is the Contractor required to provide a full-time safety officer for the project?

ANSWER: No.

62. QUESTION: May the required bidders list information be submitted after the bid?

ANSWER: No, as outlined in Section 200 the required information shall be submitted at the time of the bid.

63. QUESTION: How often is the Contractor required to review and update their schedule?

ANSWER: As outlined in SP-13, the schedule shall be reviewed at the end of each month with the Resident Project Representative and updated as necessary.

64. QUESTION: Are the residual materials from the milling required to be stockpiled on-site for the Owner?

ANSWER: Yes.

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65. QUESTION: Can P-401 bonuses be used to offset joint penalties?

ANSWER: Yes.

66. QUESTION: Can laser controls be used in lieu of a fixed string line?

ANSWER: As outlined in Item P-401-4.10, the Contractor shall use a string line for the first lift. Provided the grades meet the tolerances of Item P-401-5.2b(6), then successive lifts may be placed with laser control.

67. QUESTION: Is there an oil escalation clause that allows the Contractor to receive reimbursement if the asphalt oil price increases during the project?

ANSWER: No.

68. QUESTION: What are the depths of the existing pavements?

ANSWER: The depths of the existing pavements are outlined in the Geotechnical Subsurface Exploration Report included as Appendix "A" in the Volume 2 Specifications.

69. QUESTION: How will the Contractor be paid if they elect to use P-403 Bituminous Base Course in lieu of P-304 Cement Treated Base Course?

ANSWER: Payment for the P-403 Bituminous Base Course will be made at the contract unit price per square yard for P-304 Cement Treated Base.

70. QUESTION: Will P-403 Bituminous Base Course used in lieu of P-304 Cement Treated Base be subject to the acceptance criteria outlined in Item P-403 of the specifications?

ANSWER: Yes.

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71. QUESTION: Can the joints for the P-401 Bituminous Surface Course and P-403 Bituminous Base Course be cut back using a rotary asphalt cutting attachment or is saw cutting required?
- ANSWER: The joints shall be saw cut. A rotary asphalt cutting attachment shall not be used.
72. QUESTION: Do the taxiway lights remain on during nighttime work periods?
- ANSWER: Yes, if the taxiway is open; MITLs on closed sections of taxiways shall be de-energized or obscured.
73. QUESTION: Is the Contractor required to maintain the runway and taxiway lighting systems/circuits throughout the project?
- ANSWER: Yes. The Contractor shall maintain the runway and taxiway lighting systems/circuits for open pavements at all times. The use of temporary cable and ducts shall be anticipated and will be considered incidental to the project.
74. QUESTION: Does the P-401 Bituminous Concrete specification include an upper limit for mat density.
- ANSWER: Yes. As noted in Table 5 of Item P-401, the upper limit for mat density is 101.3.
75. QUESTION: What is the intent of the upper limit for mat density?
- ANSWER: The inclusion of the upper limit for mat density is to prevent over-rolling of the pavement and subsequent reduced pavement life resulting from crushing of the aggregate.
76. QUESTION: How will the Contractor be paid if they elect to use P-306 Econocrete Base Course in lieu of P-304 Cement Treated Base Course?
- ANSWER: Payment for the P-306 Econocrete Base Course will be made at the contract unit price per square yard for P-304 Cement Treated Base

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77. QUESTION: Will P-306 Econocrete Base Course used in lieu of P-304 Cement Treated Base be subject to the acceptance criteria outlined in Item P-403 of the specifications?

ANSWER: Yes.

78. QUESTION: May the Contractor elect to mill the pavement shown on the plans to be removed?

ANSWER: Yes, the Contractor may elect to mill pavement removal areas; however, separate payment shall not be made for the milling. Payment for the pavement removal shall be made at the contract unit price per square yard for P-140 "Pavement Removal".

79. QUESTION: If the Contractor elects to mill the pavement removal areas, may he retain the millings or are they required to be stockpiled on-site?

ANSWER: The Contractor may retain millings resulting from areas of pavement removal; however, the Owner will accept the millings and allow the material to be stockpiled on-site if preferred by the Contractor.

80. QUESTION: On Sheet 4 of 69, Haul Route Note No. 2, is there a gradation that will be required for the broken up pavement prior to it being used on the haul routes?

ANSWER: No. As previously noted in this addendum, only residual material from the milling shall be placed on haul routes.

81. QUESTION: Is the surplus pavement stockpile located on Airport property?

ANSWER: Yes, the location for the millings stockpile will be located on Airport property at a location to be determined by the Owner.

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82. QUESTION: Will the Contractor be required to mill the pavement being removed in order to dispose of at the Owner's stockpile or can we remove it with an excavator? If an excavator is acceptable, is there a maximum size for the pavement?

ANSWER: No, the Contractor is not required to mill the areas of pavement removal; an excavator may be used to remove the pavement. However, if an excavator is used, the material shall be disposed of off-site.

83. QUESTION: Can the stone base being removed be used as embankment?

ANSWER: No, as outlined in Item P-140 "Pavement Removal" of the specifications, the stone base shall be disposed of off-site by the Contractor.

84. QUESTION: Does the Contractor have a stockpile location for the demolished base courses or will the Contractor be required to haul it off-site?

ANSWER: As noted above, the demolished base courses shall be disposed of off-site by the Contractor.

85. QUESTION: Are electronic (AutoCAD) files of the plans available to Contractors?

ANSWER: No; however, electronic (AutoCAD) files will be made available to the successful Contractor.

86. QUESTION: Our insurance carrier has a problem with the bid bond form; our bonding company is located in Connecticut, not the Commonwealth of Virginia as shown.

ANSWER: A revised Bid Bond form with a blank space for the location of the bonding company has been included as part of this addendum.

ADDENDUM NO. ONE (1)

87. QUESTION: Please clarify if the removal of CTB and Base Aggregates is paid under the Pavement Removal item or the Unclassified Excavation item.

ANSWER: The removal of CTB and Base Aggregates will be paid for under P-140 "Pavement Removal".

88. QUESTION: Will the Engineer consider accepting cement-treated base in lieu of asphalt base? Essentially, the more concrete work involved the more cost effective it is to mobilize their portable plant.

ANSWER: No; however in the event of limited funding availability, the use of P-306 Econocrete Base Course may be considered for the Taxiway "B3" PCC Pavement Section.

89. QUESTION: Is there a deadline for bidder questions?

ANSWER: The deadline for bidder questions was Wednesday, July 3, 2012 at 5:00 p.m.

REVISED INVITATION TO BID

AIRFIELD PAVEMENT REHABILITATION – PHASE 2

Sealed bids for the Airfield Pavement Rehabilitation – Phase 2 project at Lynchburg Regional Airport will be received by the City of Lynchburg's Procurement Division, Third Floor, City Hall until 3:00 p.m. EST, Wednesday, July 10, 2013 then publicly opened and read aloud in the Bidder's Room, Third Floor City Hall, Lynchburg, Virginia.

The successful bidder must comply with provisions set out in this contract including Equal Employment Opportunity, and Department of Labor Requirements.

Plans, Specifications, and other Contract Documents are open to the public at the Airport Manager's Conference Room, Lynchburg Regional Airport, 350 Terminal Drive, Suite 100, Lynchburg, Virginia and may be viewed and printed from the City's webpage: <http://www.lyncburgva./current-solicitations>. Plans, specifications for download and a mailed CD containing PDF documents can be purchased at www.deltaairport.com for a nonrefundable fee of \$50.00.

Hard copy specifications and half size plans can be purchased for an additional nonrefundable fee of \$100.00, made payable to Delta Airport Consultants, 1805 Sardis Road North, Suite 101, Charlotte, North Carolina 28270, Telephone: (704) 521-9101, Fax: (704) 521-9109. Printed copies of full size plans will not be provided.

Bids must be accompanied by a certified check or a bid bond in the amount of not less than 5 percent of the total amount bid. A Contract Performance Bond and a Contract Payment Bond each equal to 100 percent of the Contract Price will be required for the successful Contractor. No bid may be withdrawn for a period of ninety (90) calendar days after the date of receiving bids.

A Pre-Bid Conference has been scheduled for 1:30 p.m., Wednesday, June 19, 2013 at the Lynchburg Regional Airport, Terminal Building, Airport Manager's Conference Room. All questions regarding this project should be sent to Stephanie Suter, Telephone: (434) 455-3970, E-mail: stephanie.suter@lynchburgva.gov and received by 5:00 p.m., Wednesday, July 3, 2013.

LYNCHBURG REGIONAL AIRPORT

AIRFIELD PAVEMENT REHABILITATION – PHASE 2

REVISED PRE-BID CONFERENCE

DATE: Wednesday, June 19, 2013

TIME: 1:30 p.m.

LOCATION: Lynchburg Regional Airport
Airport Manager's Conference Room
Terminal Building
350 Terminal Drive, Suite 100
Lynchburg, Virginia

All bidders are requested to attend to discuss the proposed construction and the plans and specifications. All potential bidders are strongly urged to attend.

ii

AIP PROJECT NO. 3-51-0029-036 (PENDING)
DELTA PROJECT NO. 12121

BID BOND

KNOW ALL MEN BY THESE PRESENTS, THAT WE _____
_____ as Principal, hereinafter called the Principal, and
_____ a corporation duly organized under the
laws of the _____ as Surety, hereinafter called the Surety, are held
and firmly bound unto City of Lynchburg as Obligee, hereinafter called the Obligee, in
the sum of _____
Dollars (\$_____), for the payment of which sum well and truly to be made, the
said Principal and the said Surety, bind ourselves, our heirs, executors, administrators,
successors and assigns, jointly and severally, firmly by these presents.

WHEREAS, the principal has submitted a bid for Airfield Pavement Rehabilitation –
Phase 2 at Lynchburg Regional Airport.

BID BOND

NOW, THEREFORE, if the Obligee shall accept the bid of the Principal and the principal shall enter into a Contract with the Obligee in accordance with the terms of such bid, and give such bond or bonds as may be specified in the bidding or Contract Documents with good and sufficient surety for the faithful performance of such Contract and for the prompt payment of labor and material furnished in the prosecution thereof, or in the event of the failure of the principal to enter such Contract and give such bond or bonds, if the principal shall pay to the Obligee the difference not to exceed the penalty hereof between the amount specified in said bid and such larger amount for which the Obligee may in good faith contract with another party to perform the Work covered by said bid, then this obligation shall be null and void, otherwise to remain in full force and effect.

Signed and sealed this ____ day of _____ 2013.

(Principal) (Seal)

(Title)

(Witness)

(Surety) (Seal)

(Title)

(Witness)

SUMMARY OF QUANTITIES

Addendum No. One (1)

Schedule 1 (Bituminous)

**Airfield Pavement and Apron Rehabilitation - Phase 2
Construct Taxiway "B3" / Rehabilitate Taxiway "D" and Taxiway "E"**

Lynchburg Regional Airport
Lynchburg, Virginia

AIP Project No. 3-51-0029-036 (Pending)
Delta Project No. VA 12121

Date: July 3, 2013

Item No.	Spec. No.	Description	Unit	Total Quantity	AIP Quantity	Non-AIP Quantity
1	P-100	MOBILIZATION	LS	1	1	0
2	P-140	PAVEMENT REMOVAL	SY	37,000	37,000	0
3	P-150	MISCELLANEOUS AND ELECTRICAL DEMOLITION	LS	1	1	0
4	P-150	REMOVE L-850C IN-PAVEMENT HIRL	EA	2	2	0
5	P-150	REMOVE L-861 BASE MOUNTED MIRL	EA	1	1	0
6	P-150	REMOVE L-861T BASE MOUNTED MITL	EA	15	15	0
7	P-150	REMOVE L-861T BASE MOUNTED MITL FIXTURE	EA	8	8	0
8	P-150	REMOVE L-862 BASE MOUNTED HIRL	EA	2	2	0
9	P-150	REMOVE ELECTRICAL MANHOLE/HANDHOLE	EA	12	12	0
10	P-150	REMOVE PULLCAN/JUNCTION STRUCTURE	EA	3	3	0
11	P-150	REMOVE AIRFIELD GUIDANCE SIGN	EA	26	26	0
12	P-150	REMOVE WINDCONE	EA	1	1	0
13	P-152	UNCLASSIFIED EXCAVATION	CY	27,000	27,000	0
14	P-152	MUCK EXCAVATION	CY	5,300	5,300	0
15	P-154	SUBBASE COURSE	CY	9,200	9,200	0
16	P-156	EROSION AND SEDIMENT CONTROL	LS	1	1	0
17	P-156	SILT FENCE	LF	1,000	1,000	0
18	P-160	ASPHALTIC PAVEMENT SURFACE MILLING	SY	600	600	0
19	P-304	CEMENT TREATED BASE COURSE (6")	SY	14,000	14,000	0
20	P-401	BITUMINOUS SURFACE COURSE	TN	4,700	4,700	0
21	P-403	BITUMINOUS BASE COURSE	TN	1,800	1,800	0
22	P-405	GROUND STABILIZATION FABRIC	SY	15,700	15,700	0
23	M-103	CLOSED RUNWAY MARKER (OWNER-FURNISHED)	EA	2	2	0
24	M-103	CLOSED RUNWAY MARKER (CONTRACTOR-FURNISHED)	EA	2	2	0
25	M-103	CLOSED TAXIWAY MARKER (CONTRACTOR-FURNISHED)	EA	7	7	0
26	M-107	AVIATION BARRICADES (OWNER-FURNISHED)	EA	1,500	1,500	0
27	M-107	BUCKET BARRICADES (CONTRACTOR-FURNISHED)	EA	21	21	0
28	P-619	PAINT REMOVAL	SF	6,000	4,000	2,000
29	P-620	PERMANENT RUNWAY AND TAXIWAY PAINTING	SF	15,000	15,000	0
30	P-620	INITIAL RUNWAY AND TAXIWAY PAINTING	SF	15,000	15,000	0
31	P-620	THERMOPLASTIC SURFACE PAINTED RUNWAY HOLDING POSITION SIGN	EA	13	5	8
32	D-701	15" RCP, CLASS V	LF	576	576	0
33	D-701	18" RCP, CLASS V	LF	545	545	0
34	D-701	15" VDOT ES-1 FLARED END SECTION	EA	2	2	0
35	D-701	18" VDOT ES-1 FLARED END SECTION	EA	1	1	0
36	D-701	15" PIPE COLLAR	EA	1	1	0
37	D-705	6" PVC UNDERDRAIN PIPE	LF	100	100	0
38	D-751	36" DIA. AIRCRAFT RATED MANHOLE W/ FRAME AND SOLID LID (185,000 # DUAL WHEEL GEAR)	EA	1	1	0
39	D-751	60" DIA. AIRCRAFT RATED MANHOLE W/ FRAME AND SOLID LID (185,000 # DUAL WHEEL GEAR)	EA	1	1	0
40	D-751	72" DIA. MANHOLE, ST'D. MH-2 WITH ECCENTRIC CONE	EA	1	1	0
41	D-751	60" DIA. DROP INLET, ST'D. DI-7B WITH TYPE I GRATE	EA	2	2	0
42	T-901	SEEDING	AC	15	15	0
43	T-908	MULCHING	AC	15	15	0

SUMMARY OF QUANTITIES

Addendum No. One (1)

Schedule 1 (Bituminous)

Airfield Pavement and Apron Rehabilitation - Phase 2

Construct Taxiway "B3" / Rehabilitate Taxiway "D" and Taxiway "E"

Lynchburg Regional Airport
Lynchburg, Virginia

AIP Project No. 3-51-0029-036 (Pending)
Delta Project No. VA 12121

Date: July 3, 2013

Item No.	Spec. No.	Description	Unit	Total Quantity	AIP Quantity	Non-AIP Quantity
44	L-108	NO. 8 AWG L-824C CABLE, 5KV	LF	25,000	25,000	0
45	L-108	NO. 6 AWG SOLID BARE COPPER COUNTERPOISE WIRE	LF	4,100	4,100	0
46	L-108	NO. 6 AWG SOLID BARE COPPER COUNTERPOISE WIRE FOR LIGHTNING PROTECTION - PLOWED	LF	7,300	7,300	0
47	L-110	ELECTRICAL DUCT BANK, 1 WAY - 2" PVC CONDUIT	LF	100	100	0
48	L-110	ELECTRICAL DUCT BANK, 2 WAY - 2" PVC CONDUIT	LF	800	800	0
49	L-110	ELECTRICAL DUCT BANK, 4 WAY - 2" PVC CONDUIT	LF	700	700	0
50	L-110	ELECTRICAL CONDUIT, 1 WAY - 2" PVC CONDUIT	LF	8,200	8,200	0
51	L-110	ELECTRICAL CONDUIT, 2 WAY - 2" PVC CONDUIT	LF	400	400	0
52	L-110	ELECTRICAL CONDUIT, 4 WAY - 2" PVC CONDUIT	LF	2,000	2,000	0
53	L-110	ELECTRICAL CONDUIT - BORED UNDER PAVEMENT, 2 WAY - 2" HDPE	LF	30	30	0
54	L-110	ELECTRICAL CONDUIT - BORED UNDER PAVEMENT, 4 WAY - 2" HDPE	LF	200	200	0
55	L-115	ELECTRICAL JUNCTION STRUCTURE - ELECTRICAL HANDHOLE (AIRCRAFT RATED)	EA	1	1	0
56	L-115	ELECTRICAL JUNCTION STRUCTURE, L-867 PULLCAN	EA	9	9	0
57	L-115	ELECTRICAL JUNCTION STRUCTURE, 2 UNIT L-867 PULLCAN PLAZA	EA	6	6	0
58	L-115	ELECTRICAL JUNCTION STRUCTURE, 4 UNIT L-867 PULLCAN PLAZA	EA	11	11	0
59	L-125	L-858 AIRFIELD GUIDANCE SIGN (1-2 CHAR.)	EA	1	1	0
60	L-125	L-858 AIRFIELD GUIDANCE SIGN (3-4 CHAR.)	EA	19	19	0
61	L-125	L-858 AIRFIELD GUIDANCE SIGN (5-6 CHAR.)	EA	6	6	0
62	L-125	L-858 AIRFIELD GUIDANCE SIGN (CONTACT ATC SIGN)	EA	1	1	0
63	L-125	L-850C IN-PAVEMENT HIRL	EA	1	1	0
64	L-125	L-861 BASE MOUNTED MIRL	EA	2	2	0
65	L-125	L-861T BASE MOUNTED MITL WITH RELOCATED FIXTURE	EA	6	6	0
66	L-125	L-861T(L) BASE MOUNTED MITL (LED)	EA	116	116	0
67	L-125	L-861T(L) BASED MOUNTED MITL (LED) ON EXISTING BASE	EA	15	15	0
68	L-125	L-862 BASE MOUNTED HIRL	EA	3	3	0
69	L-125	L-853 TYPE II RETROREFLECTIVE MARKER	EA	5	5	0
70	L-125	REINSTALL AND UPGRADE WIND CONE	EA	1	1	0
71	L-129	NO. 6 AWG, L-824C CABLE, 600V	LF	16,000	16,000	0
72	L-129	25 PAIR SHIELDED CONTROL CABLE	LF	1,000	1,000	0
73	L-129	WIND SENSOR POWER CABLE	LF	700	700	0
74	L-129	WIND SENSOR COMMUNICATIONS CABLE	LF	700	700	0
75	L-130	RELOCATE WIND SENSOR	LS	1	1	0
76	R-414	RIPRAP, CLASS I	SY	100	100	0
77	R-606	SOIL STABILIZATION MAT, EC-2	SY	100	100	0
78	SP-28	WATERLINE ADJUSTMENT ALLOWANCE	LS	1	1	0

SUMMARY OF QUANTITIES

Addendum No. One (1)

Schedule 2 (PCC)

Airfield Pavement and Apron Rehabilitation - Phase 2

Construct Taxiway "B3" / Rehabilitate Taxiway "D" and Taxiway "E"

Lynchburg Regional Airport
Lynchburg, Virginia

AIP Project No. 3-51-0029-036 (Pending)
Delta Project No. VA 12121

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4	P-150	REMOVE L-850C IN-PAVEMENT HIRL	EA	2	2	0
5	P-150	REMOVE L-861 BASE MOUNTED MIRL	EA	1	1	0
6	P-150	REMOVE L-861T BASE MOUNTED MITL	EA	137	137	0
7	P-150	REMOVE L-861T BASE MOUNTED MITL FIXTURE	EA	15	15	0
8	P-150	REMOVE L-862 BASE MOUNTED HIRL	EA	2	2	0
9	P-150	REMOVE ELECTRICAL MANHOLE/HANDHOLE	EA	12	12	0
10	P-150	REMOVE PULLCAN/JUNCTION STRUCTURE	EA	3	3	0
11	P-150	REMOVE AIRFIELD GUIDANCE SIGN	EA	26	26	0
12	P-150	REMOVE WINDCONE	EA	1	1	0
13	P-152	UNCLASSIFIED EXCAVATION	CY	27,000	27,000	0
14	P-152	MUCK EXCAVATION	CY	5,600	5,600	0
15	P-154	SUBBASE COURSE	CY	7,000	7,000	0
16	P-156	EROSION AND SEDIMENT CONTROL	LS	1	1	0
17	P-156	SILT FENCE	LF	1,000	1,000	0
18	P-160	ASPHALTIC PAVEMENT SURFACE MILLING	SY	600	600	0
19	P-304	CEMENT TREATED BASE COURSE (4")	SY	14,500	14,500	0
20	P-401	BITUMINOUS SURFACE COURSE	TN	600	600	0
21	P-403	BITUMINOUS BASE COURSE	TN	1,500	1,500	0
22	P-405	GROUND STABILIZATION FABRIC	SY	16,800	16,800	0
23	P-501	PORTLAND CEMENT CONCRETE PAVEMENT (12")	SY	16,200	16,200	0
24	P-601	EXPANSION JOINT	LF	3,000	3,000	0
25	M-103	CLOSED RUNWAY MARKER (OWNER-FURNISHED)	EA	2	2	0
26	M-103	CLOSED RUNWAY MARKER (CONTRACTOR-FURNISHED)	EA	2	2	0
27	M-103	CLOSED TAXIWAY MARKER (CONTRACTOR-FURNISHED)	EA	7	7	0
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29	M-107	BUCKET BARRICADES (CONTRACTOR-FURNISHED)	EA	21	21	0
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40	D-751	36" DIA. AIRCRAFT RATED MANHOLE W/ FRAME AND SOLID LID (185,000 # DUAL WHEEL GEAR)	EA	1	1	0
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42	D-751	72" DIA. MANHOLE, ST'D. MH-2 WITH ECCENTRIC CONE	EA	1	1	0
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SUMMARY OF QUANTITIES

Addendum No. One (1)

Schedule 2 (PCC)

Airfield Pavement and Apron Rehabilitation - Phase 2

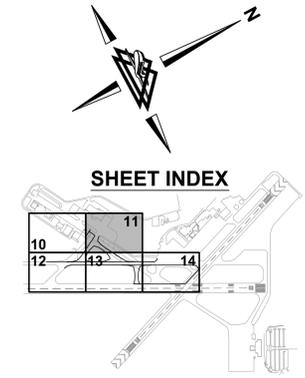
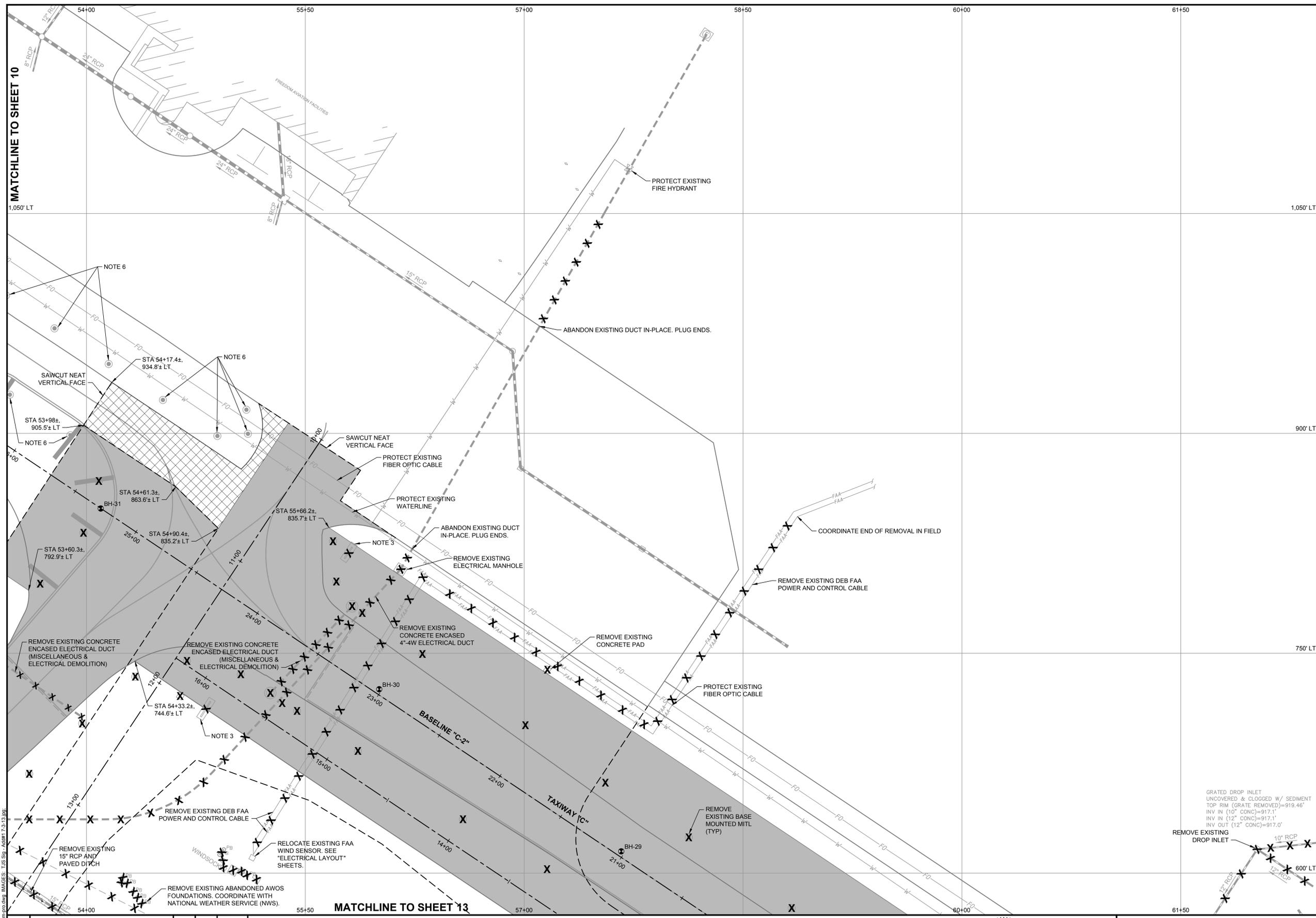
Construct Taxiway "B3" / Rehabilitate Taxiway "D" and Taxiway "E"

Lynchburg Regional Airport
Lynchburg, Virginia

AIP Project No. 3-51-0029-036 (Pending)
Delta Project No. VA 12121

Date: July 3, 2013

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45	T-908	MULCHING	AC	15	15	0
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47	L-108	NO. 6 AWG SOLID BARE COPPER COUNTERPOISE WIRE	LF	4,100	4,100	0
48	L-108	NO. 6 AWG SOLID BARE COPPER COUNTERPOISE WIRE FOR LIGHTNING PROTECTION - PLOWED	LF	7,300	7,300	0
49	L-110	ELECTRICAL DUCT BANK, 1 WAY - 2" PVC CONDUIT	LF	100	100	0
50	L-110	ELECTRICAL DUCT BANK, 2 WAY - 2" PVC CONDUIT	LF	800	800	0
51	L-110	ELECTRICAL DUCT BANK, 4 WAY - 2" PVC CONDUIT	LF	700	700	0
52	L-110	ELECTRICAL CONDUIT, 1 WAY - 2" PVC CONDUIT	LF	8,200	8,200	0
53	L-110	ELECTRICAL CONDUIT, 2 WAY - 2" PVC CONDUIT	LF	400	400	0
54	L-110	ELECTRICAL CONDUIT, 4 WAY - 2" PVC CONDUIT	LF	2,000	2,000	0
55	L-110	ELECTRICAL CONDUIT - BORED UNDER PAVEMENT, 2 WAY - 2" HDPE	LF	30	30	0
56	L-110	ELECTRICAL CONDUIT - BORED UNDER PAVEMENT, 4 WAY - 2" HDPE	LF	200	200	0
57	L-115	ELECTRICAL JUNCTION STRUCTURE - ELECTRICAL HANDHOLE (AIRCRAFT RATED)	EA	1	1	0
58	L-115	ELECTRICAL JUNCTION STRUCTURE, L-867 PULLCAN	EA	9	9	0
59	L-115	ELECTRICAL JUNCTION STRUCTURE, 2 UNIT L-867 PULLCAN PLAZA	EA	6	6	0
60	L-115	ELECTRICAL JUNCTION STRUCTURE, 4 UNIT L-867 PULLCAN PLAZA	EA	11	11	0
61	L-125	L-858 AIRFIELD GUIDANCE SIGN (1-2 CHAR.)	EA	1	1	0
62	L-125	L-858 AIRFIELD GUIDANCE SIGN (3-4 CHAR.)	EA	19	19	0
63	L-125	L-858 AIRFIELD GUIDANCE SIGN (5-6 CHAR.)	EA	6	6	0
64	L-125	L-858 AIRFIELD GUIDANCE SIGN (CONTACT ATC SIGN)	EA	1	1	0
65	L-125	L-850C IN-PAVEMENT HIRL	EA	1	1	0
66	L-125	L-861 BASE MOUNTED MIRL	EA	2	2	0
67	L-125	L-861T BASE MOUNTED MITL WITH RELOCATED FIXTURE	EA	6	6	0
68	L-125	L-861T(L) BASE MOUNTED MITL (LED)	EA	116	116	0
69	L-125	L-861T(L) BASED MOUNTED MITL (LED) ON EXISTING BASE	EA	15	15	0
70	L-125	L-862 BASE MOUNTED HIRL	EA	3	3	0
71	L-125	L-853 TYPE II RETROREFLECTIVE MARKER	EA	5	5	0
72	L-125	REINSTALL AND UPGRADE WIND CONE	EA	1	1	0
73	L-129	NO. 6 AWG, L-824C CABLE, 600V	LF	16,000	16,000	0
74	L-129	25 PAIR SHIELDED CONTROL CABLE	LF	1,000	1,000	0
75	L-129	WIND SENSOR POWER CABLE	LF	700	700	0
76	L-129	WIND SENSOR COMMUNICATIONS CABLE	LF	700	700	0
77	L-130	RELOCATE WIND SENSOR	LS	1	1	0
78	R-414	RIPRAP, CLASS I	SY	100	100	0
79	R-606	SOIL STABILIZATION MAT, EC-2	SY	100	100	0
80	SP-28	WATERLINE ADJUSTMENT ALLOWANCE	LS	1	1	0



- LEGEND**
- ▲ BM-X BENCHMARK LOCATION
 - B-X BORING LOCATION (APPROX)
 - EXISTING DROP INLET
 - ⊗ EXISTING DROP INLET TO BE REMOVED
 - ▭ EXISTING FLARED END SECTION
 - ⊗ EXISTING FLARED END SECTION TO BE REMOVED
 - ⊗ EXISTING ENDWALL/HEADWALL/WINGWALL
 - ⊗ EXISTING ENDWALL/HEADWALL/WINGWALL TO BE REMOVED
 - EXISTING STORM MAN-HOLE
 - ⊗ EXISTING STORM MAN-HOLE TO BE REMOVED
 - EXISTING SANITARY MANHOLE
 - EXISTING UNDERDRAIN CLEANOUT
 - ▭ EXISTING AIRFIELD GUIDANCE SIGN
 - ⊗ EXISTING AIRFIELD GUIDANCE SIGN TO BE REMOVED
 - EXISTING RUNWAY LIGHT
 - ⊗ EXISTING RUNWAY LIGHT TO BE REMOVED
 - EXISTING RUNWAY THRESHOLD LIGHT
 - ⊗ EXISTING RUNWAY THRESHOLD LIGHT TO BE REMOVED
 - EXISTING TAXIWAY LIGHT
 - ⊗ EXISTING TAXIWAY LIGHT TO BE REMOVED
 - ⊗ EXISTING TIE-DOWN ANCHOR
 - ⊗ EXISTING TIE-DOWN ANCHOR TO BE REMOVED
 - EXISTING WIND CONE
 - EXISTING ROADWAY SIGN
 - ⊗ EXISTING ROADWAY SIGN TO BE REMOVED
 - ▭ EXISTING STORM DRAIN
 - ⊗ EXISTING STORM DRAIN TO BE REMOVED
 - EXISTING ELECTRICAL CONDUIT
 - ⊗ EXISTING ELECTRICAL CONDUIT TO BE REMOVED
 - EXISTING ELECTRICAL DUCT
 - ⊗ EXISTING ELECTRICAL DUCT TO BE REMOVED
 - EXISTING AIRFIELD ELECTRICAL LINE
 - EXISTING UNDERGROUND ELECTRICAL LINE
 - EXISTING FAA-CONTROLLED CABLE
 - EXISTING FENCE
 - EXISTING RUNWAY OBJECT FREE AREA
 - EXISTING RUNWAY SAFETY AREA
 - EXISTING SANITARY SEWER LINE
 - EXISTING TAXIWAY OBJECT FREE AREA
 - EXISTING TAXIWAY SAFETY AREA
 - ▭ EXISTING CONCRETE DITCH/PAD
 - ▭ PROPOSED CONCRETE DITCH REMOVAL
 - ▭ PROPOSED NOMINAL 2" PAVEMENT MILLING
 - ▭ PROPOSED PAVEMENT REMOVAL

- NOTES**
- ALL STATIONS AND OFFSETS GIVEN WITH RESPECT TO BASELINE "RW" (RUNWAY 4-22 CENTERLINE) UNLESS OTHERWISE NOTED.
 - REFER TO "PROJECT NOTES" SHEET FOR DEMOLITION NOTES.
 - REMOVE EXISTING AIRFIELD GUIDANCE SIGN AND BASE STOCKPILE AS ORDERED BY THE ENGINEER.
 - REMOVE EXISTING CONDUIT AND CABLE IN AREAS OF PAVEMENT REMOVAL AS ORDERED BY THE ENGINEER.
 - REFER TO "GRADING & DRAINAGE" SHEETS FOR INSTALLATION OF EROSION AND SEDIMENT CONTROL MEASURES.
 - REMOVE EXISTING FIXTURE AND TRANSFORMER. PROTECT EXISTING BASE TO REMAIN.
- GRATED DROP INLET UNCOVERED & CLOGGED W/ SEDIMENT
 TOP RIM (GRATE REMOVED)=919.46'
 INV IN (10" CONC)=917.1'
 INV IN (12" CONC)=917.1'
 INV OUT (12" CONC)=917.0'
- REMOVE EXISTING DROP INLET
- SCALE: 1"=30' FEET

NO.	REVISIONS	BY	APP.	DATE
1	ADDENDUM NO. ONE (1)	DWS	TJS	7/3/13

1805 Sardis Road North, Suite 101
 Charlotte, North Carolina 28270
 phone: (704) 521-9101 • fax: (704) 521-9109
 www.deltairport.com



**DELTA AIRPORT
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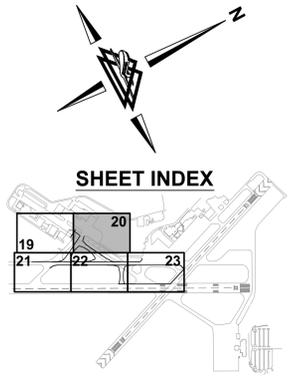
AIRFIELD PAVEMENT REHABILITATION-PHASE 2		AIP NO. 3-51-0029-036 (PENDING)	JOB NO. 12121
DEMOLITION PLAN		DRAWN BY: DWS	SHEET 11
LYNCHBURG REGIONAL AIRPORT		DESIGNED BY: TJS	OF 69
SCALE: 1" = 30'	DATE: MAY 2013		

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DRAINAGE SUMMARY
 D2 60" DIA. DROP INLET, STD. DI-78 WITH TYPE I GRATE
 GRATE ELEV. = 921.52
 15" RCP INV. OUT = 917.00
 STA 62+02.7, 616.1' LT, BL "RW"

PIPE SUMMARY
 D2-D3 379 LF 15" RCP CLASS V
 SLOPE = 1.47%



- LEGEND**
- ▲ BM-X BENCHMARK LOCATION
 - B-X BORING LOCATION (APPROX)
 - × 918.33 EXISTING SPOT ELEVATION
 - + 905.44 PROPOSED SPOT ELEVATION
 - (DX) PROPOSED STRUCTURE NUMBER
 - EXISTING DROP INLET
 - PROPOSED DROP INLET
 - ▭ EXISTING FLARED END SECTION
 - ▭ PROPOSED FLARED END SECTION
 - ▭ EXISTING ENDWALL/HEADWALL/WINGWALL
 - ▭ PROPOSED ENDWALL/HEADWALL/WINGWALL
 - EXISTING STORM MANHOLE
 - PROPOSED STORM MANHOLE
 - EXISTING SANITARY MANHOLE
 - EXISTING UNDERDRAIN CLEANOUT
 - EXISTING FIRE HYDRANT
 - EXISTING AIRFIELD GUIDANCE SIGN
 - EXISTING RUNWAY LIGHT
 - EXISTING RUNWAY THRESHOLD LIGHT
 - EXISTING TAXIWAY LIGHT
 - EXISTING TIE-DOWN ANCHOR
 - EXISTING WIND CONE
 - EXISTING ROADWAY SIGN
 - PROPOSED ROCK CHECK DAM
 - PROPOSED CULVERT INLET PROTECTION
 - PROPOSED INLET PROTECTION
 - PS PERMANENT SEEDING
 - ▭ EXISTING STORM DRAIN
 - ▭ PROPOSED STORM DRAIN
 - ▭ EXISTING EDGE DRAIN/UNDERDRAIN
 - ▭ EXISTING DITCH
 - ▭ PROPOSED DIVERSION DIKE
 - ▭ EXISTING ELECTRICAL CONDUIT
 - ▭ EXISTING ELECTRICAL DUCT
 - ▭ EXISTING AIRFIELD ELECTRICAL LINE
 - ▭ EXISTING UNDERGROUND ELECTRICAL LINE
 - ▭ FAA EXISTING FAA-CONTROLLED CABLE
 - ▭ EXISTING FENCE
 - ▭ SF PROPOSED SILT FENCE
 - ▭ LOD- LIMITS OF DISTURBANCE
 - ▭ RSA EXISTING RUNWAY SAFETY AREA
 - ▭ SAN EXISTING SANITARY SEWER LINE
 - ▭ TSA EXISTING TAXIWAY SAFETY AREA
 - ▭ EXISTING CONCRETE DITCHPAD
 - ▭ PROPOSED TAXIWAY "B3" PAVEMENT
 - ▭ PROPOSED TAXIWAY "D" AND "E" PAVEMENT
 - ▭ PROPOSED VARIABLE DEPTH OVERLAY

- NOTES**
1. ALL STATIONS AND OFFSETS GIVEN WITH RESPECT TO BASELINE "RW" (RUNWAY 4-22 CENTERLINE) UNLESS OTHERWISE NOTED.
 2. REFER TO "GEOMETRIC LAYOUT" SHEETS FOR STAKE OUT INFORMATION.
 3. REFER TO "DEMOLITION PLAN" SHEETS FOR REMOVAL OF EXISTING STRUCTURES, PAVEMENT REMOVAL LIMITS, PAVEMENT MILLING LIMITS, ETC.
 4. REFER TO "ELECTRICAL LAYOUT" SHEETS FOR LOCATION AND DIMENSIONS OF PROPOSED ELECTRICAL ITEMS.
 5. UTILIZE BITUMINOUS TAXIWAY "B3" PAVEMENT SECTION FOR TAXIWAY "B" RECONSTRUCTION (ALTERNATE 1 AND 2)
- SCALE: 1"=30'
 0 30 60 FEET

NO.	REVISIONS	BY	APP.	DATE
1	ADDENDUM NO. ONE (1)	DWS	TJS	7/3/13

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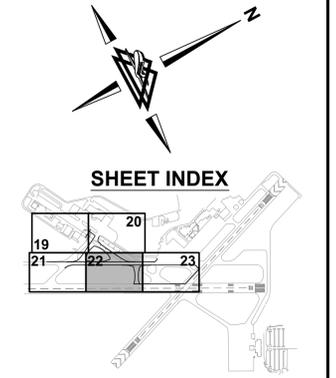
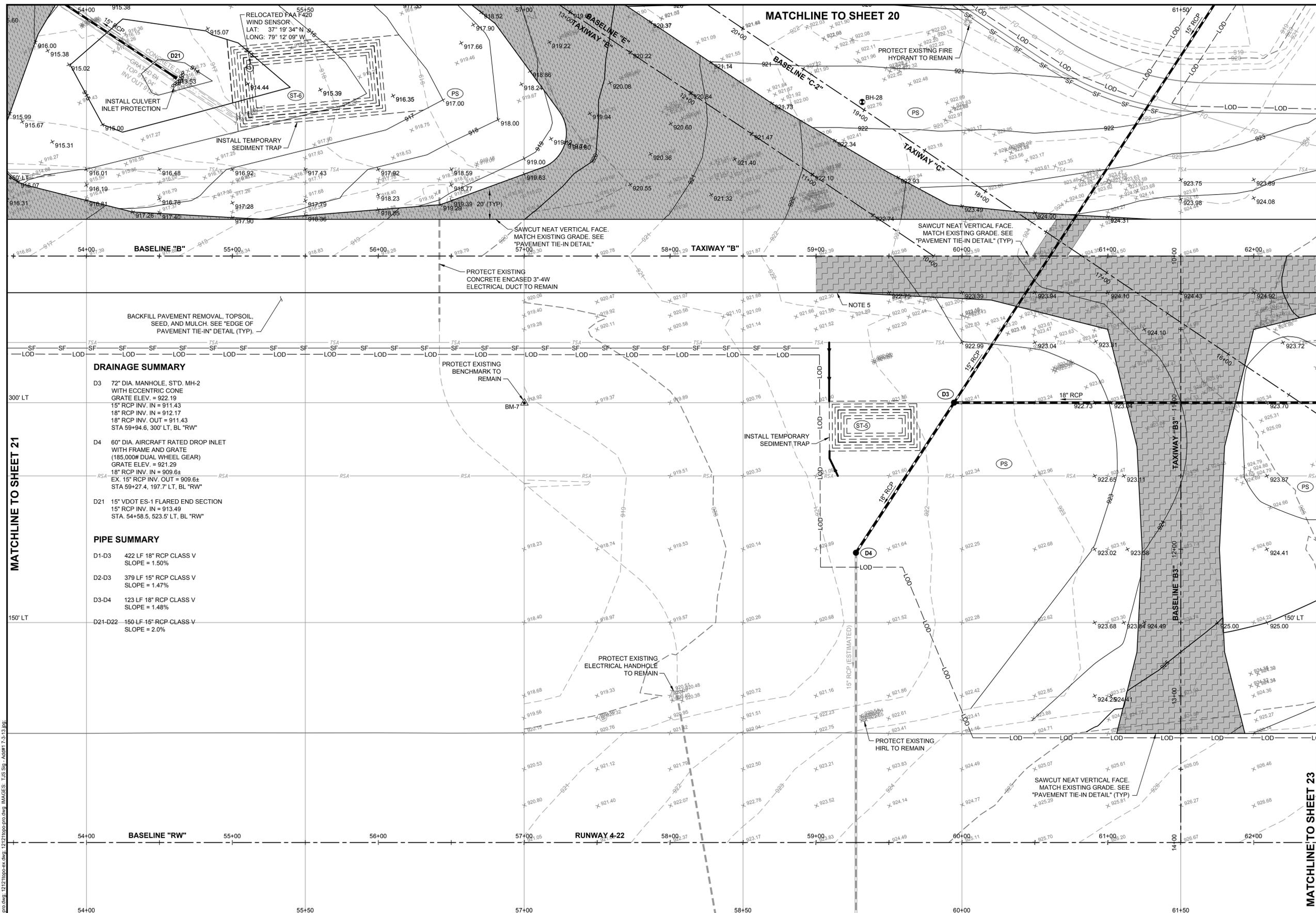


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AIRFIELD PAVEMENT REHABILITATION-PHASE 2		AIP NO. 3-51-0029-036 (PENDING)	JOB NO. 12121
GRADING & DRAINAGE		DRAWN BY: DWS	SHEET 20
LYNCHBURG REGIONAL AIRPORT		DESIGNED BY: TJS	OF 69
SCALE: 1" = 30'	DATE: MAY 2013		

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- LEGEND**
- BM-X BENCHMARK LOCATION
 - B-X BORING LOCATION (APPROX)
 - X 918.33 EXISTING SPOT ELEVATION
 - + 905.44 PROPOSED SPOT ELEVATION
 - (DX) PROPOSED STRUCTURE NUMBER
 - EXISTING DROP INLET
 - PROPOSED DROP INLET
 - EXISTING FLARED END SECTION
 - PROPOSED FLARED END SECTION
 - EXISTING ENDWALL/HEADWALL/WINGWALL
 - PROPOSED ENDWALL/HEADWALL/WINGWALL
 - EXISTING STORM MANHOLE
 - PROPOSED STORM MANHOLE
 - EXISTING SANITARY MANHOLE
 - EXISTING UNDERDRAIN CLEANOUT
 - EXISTING FIRE HYDRANT
 - EXISTING AIRFIELD GUIDANCE SIGN
 - EXISTING RUNWAY LIGHT
 - EXISTING RUNWAY THRESHOLD LIGHT
 - EXISTING TAXIWAY LIGHT
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 - EXISTING ROADWAY SIGN
 - PROPOSED ROCK CHECK DAM
 - PROPOSED CULVERT INLET PROTECTION
 - PROPOSED INLET PROTECTION
 - PERMANENT SEEDING
 - EXISTING STORM DRAIN
 - PROPOSED STORM DRAIN
 - EXISTING EDGE DRAIN/UNDERDRAIN
 - EXISTING DITCH
 - PROPOSED DIVERSION DIKE
 - EXISTING ELECTRICAL CONDUIT
 - EXISTING ELECTRICAL DUCT
 - EXISTING AIRFIELD ELECTRICAL LINE
 - EXISTING UNDERGROUND ELECTRICAL LINE
 - EXISTING FAA-CONTROLLED CABLE
 - EXISTING FENCE
 - PROPOSED SILT FENCE
 - LOD- LIMITS OF DISTURBANCE
 - EXISTING RUNWAY SAFETY AREA
 - EXISTING SANITARY SEWER LINE
 - EXISTING TAXIWAY SAFETY AREA
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 - PROPOSED TAXIWAY "B3" PAVEMENT
 - PROPOSED TAXIWAY "D" AND "E" PAVEMENT
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- NOTES**
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 5. UTILIZE BITUMINOUS TAXIWAY "B3" PAVEMENT SECTION FOR TAXIWAY "B" RECONSTRUCTION (ALTERNATE 1 AND 2)

AIRFIELD PAVEMENT REHABILITATION-PHASE 2		AIP NO. 3-51-0029-036 (PENDING)	JOB NO. 12121
GRADING & DRAINAGE		DRAWN BY: DWS	SHEET 22
LYNCHBURG REGIONAL AIRPORT		DESIGNED BY: TJS	OF 69
SCALE: 1" = 30'	DATE: MAY 2013		

MATCHLINE TO SHEET 21

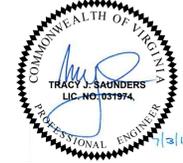
MATCHLINE TO SHEET 23

NO.	REVISIONS	BY	APP.	DATE
1	ADDENDUM NO. ONE (1)	DWS	TJS	7/3/13

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DRAWING: 12121grd.dwg LAYOUT: LS
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PLOT: 7/3/2013 12:39:20 PM
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TEMPORARY SEDIMENT TRAP INSTALLATION NOTES:

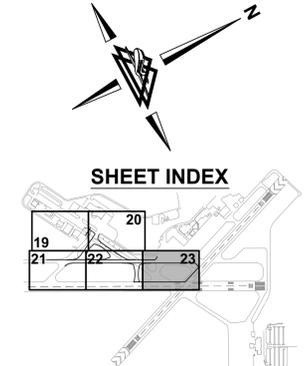
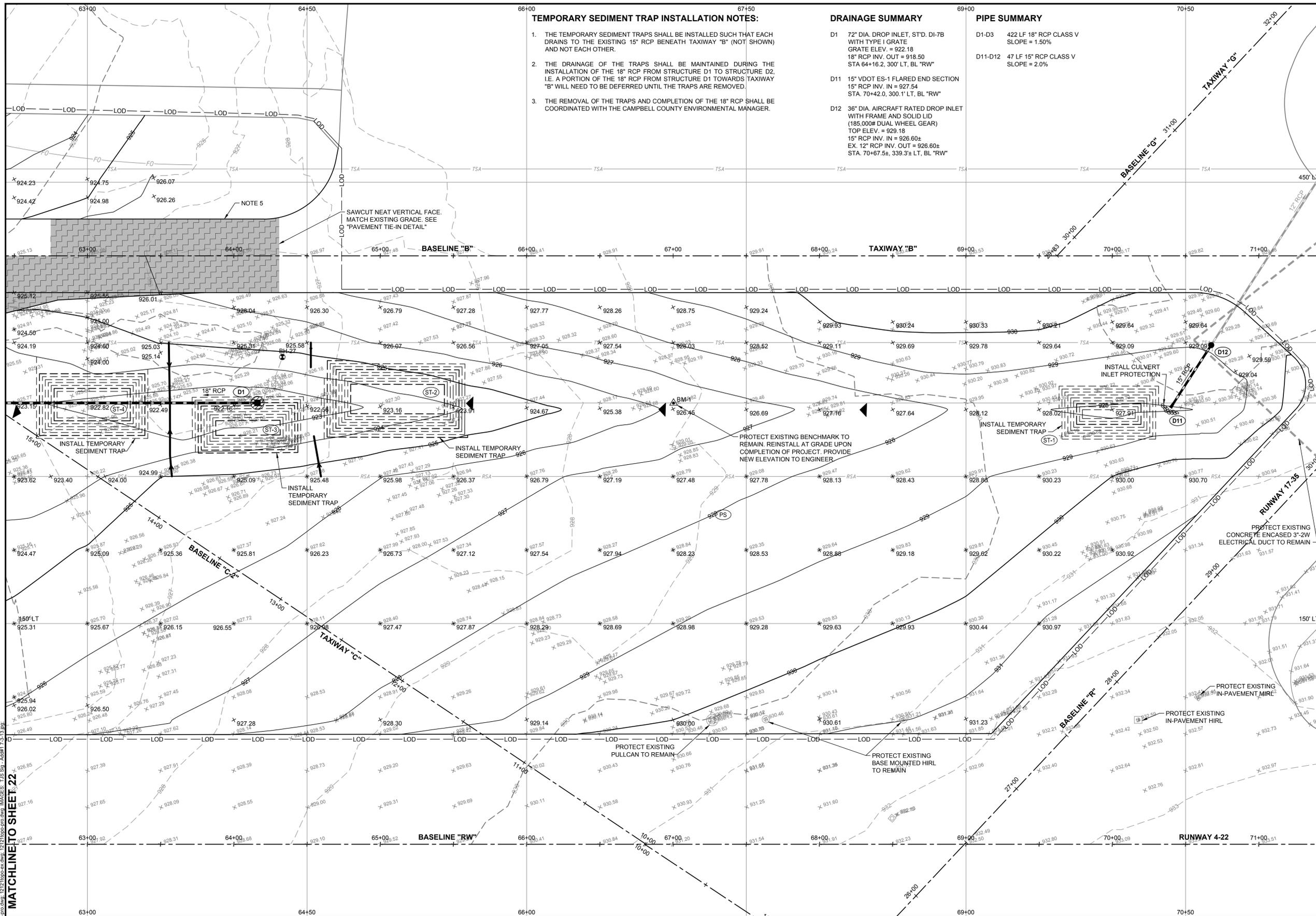
1. THE TEMPORARY SEDIMENT TRAPS SHALL BE INSTALLED SUCH THAT EACH DRAINS TO THE EXISTING 15" RCP BENEATH TAXIWAY "B" (NOT SHOWN) AND NOT EACH OTHER.
2. THE DRAINAGE OF THE TRAPS SHALL BE MAINTAINED DURING THE INSTALLATION OF THE 18" RCP FROM STRUCTURE D1 TO STRUCTURE D2. I.E. A PORTION OF THE 18" RCP FROM STRUCTURE D1 TOWARDS TAXIWAY "B" WILL NEED TO BE DEFERRED UNTIL THE TRAPS ARE REMOVED.
3. THE REMOVAL OF THE TRAPS AND COMPLETION OF THE 18" RCP SHALL BE COORDINATED WITH THE CAMPBELL COUNTY ENVIRONMENTAL MANAGER.

DRAINAGE SUMMARY

- D1 72" DIA. DROP INLET, STD. DI-78 WITH TYPE I GRATE
GRATE ELEV. = 922.18
18" RCP INV. OUT = 918.50
STA 64+16.2, 300' LT, BL "RW"
- D11 15" VDOT ES-1 FLARED END SECTION
15" RCP INV. IN = 927.54
STA. 70+42.0, 300.1' LT, BL "RW"
- D12 36" DIA. AIRCRAFT RATED DROP INLET WITH FRAME AND SOLID LID (185,000# DUAL WHEEL GEAR)
TOP ELEV. = 929.18
15" RCP INV. IN = 926.60±
EX. 12" RCP INV. OUT = 926.60±
STA. 70+67.5±, 339.3'± LT, BL "RW"

PIPE SUMMARY

- D1-D3 422 LF 18" RCP CLASS V
SLOPE = 1.50%
- D11-D12 47 LF 15" RCP CLASS V
SLOPE = 2.0%



LEGEND

- BM-X BENCHMARK LOCATION
- B-X BORING LOCATION (APPROX)
- X 918.33 EXISTING SPOT ELEVATION
- + 905.44 PROPOSED SPOT ELEVATION
- (DX) PROPOSED STRUCTURE NUMBER
- EXISTING DROP INLET
- PROPOSED DROP INLET
- EXISTING FLARED END SECTION
- PROPOSED FLARED END SECTION
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- PROPOSED STORM MANHOLE
- EXISTING SANITARY MANHOLE
- EXISTING UNDERDRAIN CLEANOUT
- EXISTING FIRE HYDRANT
- EXISTING AIRFIELD GUIDANCE SIGN
- EXISTING RUNWAY LIGHT
- EXISTING RUNWAY THRESHOLD LIGHT
- EXISTING TAXIWAY LIGHT
- EXISTING TI-DOWN ANCHOR
- EXISTING WIND CONE
- EXISTING ROADWAY SIGN
- PROPOSED ROCK CHECK DAM
- PROPOSED CULVERT INLET PROTECTION
- PROPOSED INLET PROTECTION
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- EXISTING FAA-CONTROLLED CABLE
- EXISTING FENCE
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- LOD- LIMITS OF DISTURBANCE
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- EXISTING SANITARY SEWER LINE
- EXISTING TAXIWAY SAFETY AREA
- EXISTING CONCRETE DITCH/PAD
- PROPOSED TAXIWAY "B3" PAVEMENT
- PROPOSED TAXIWAY "D" AND "E" PAVEMENT
- PROPOSED VARIABLE DEPTH OVERLAY

NOTES

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30 0 30 60
SCALE: 1"=30'
FEET

NO.	REVISIONS	BY	APP.	DATE
1	ADDENDUM NO. ONE (1)	DWS	TJS	7/3/13

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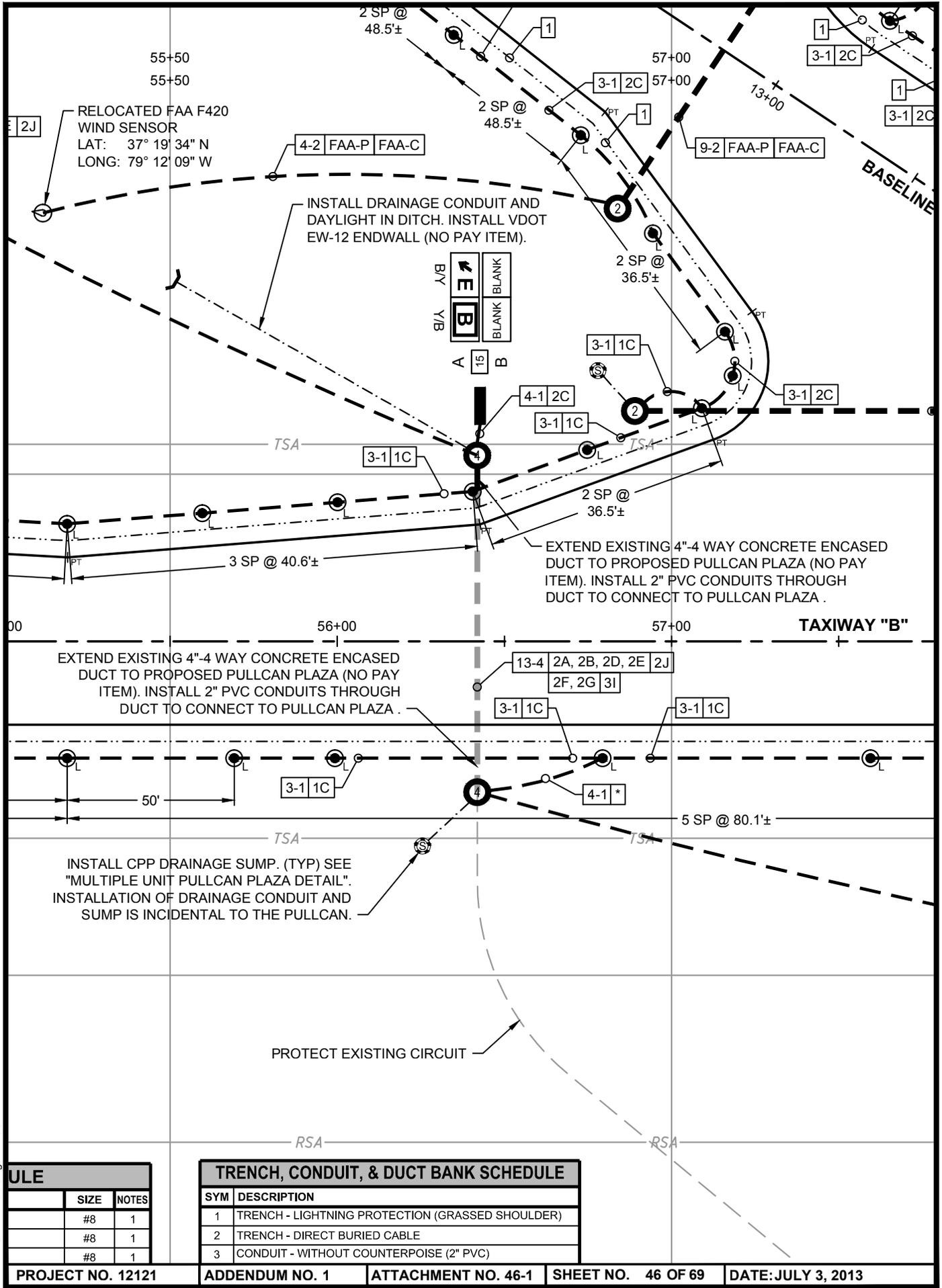
DELTA AIRPORT CONSULTANTS, INC.



AIRFIELD PAVEMENT REHABILITATION-PHASE 2		AIP NO. 3-51-0029-036 (PENDING)	JOB NO. 12121
GRADING & DRAINAGE		DRAWN BY: DWS	SHEET 23
LYNCHBURG REGIONAL AIRPORT		DESIGNED BY: TJS	OF 69
SCALE: 1" = 30'	DATE: MAY 2013		

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TRENCH, CONDUIT, & DUCT BANK SCHEDULE	
SYM	DESCRIPTION
1	TRENCH - LIGHTNING PROTECTION (GRASSED SHOULDER)
2	TRENCH - DIRECT BURIED CABLE
3	CONDUIT - WITHOUT COUNTERPOISE (2" PVC)
4	CONDUIT - WITH COUNTERPOISE (2" PVC)
5	CONDUIT - BORED UNDER PAVEMENT (2" HDPE) +
6	CONDUIT - INSTALLED UNDER NEW PAVED SHOULDER * (2" PVC) (WITH COUNTERPOISE)
6A	CONDUIT - INSTALLED UNDER NEW PAVED SHOULDER * (2" PVC) (WITH COUNTERPOISE)
7	CONDUIT - TRENCHED THROUGH EX. PAVEMENT (2" PVC) *
8	CONDUIT OR DUCT BANK - EXISTING
9	DUCT BANK - INSTALLED UNDER NEW PAVEMENT (2" PVC) +
10	DUCT BANK - INSTALLED UNDER NEW PAVEMENT (4" PVC) +
11	DUCT BANK - TRENCHED THROUGH EX. PAVEMENT (2" PVC) +
12	DUCT BANK - TRENCHED THROUGH EX. PAVEMENT (4" PVC) +
13	CONDUIT - THROUGH EXISTING DUCT (2" PVC)
+ Aircraft Rated Pavement	
* Non-Aircraft Rated Pavement	

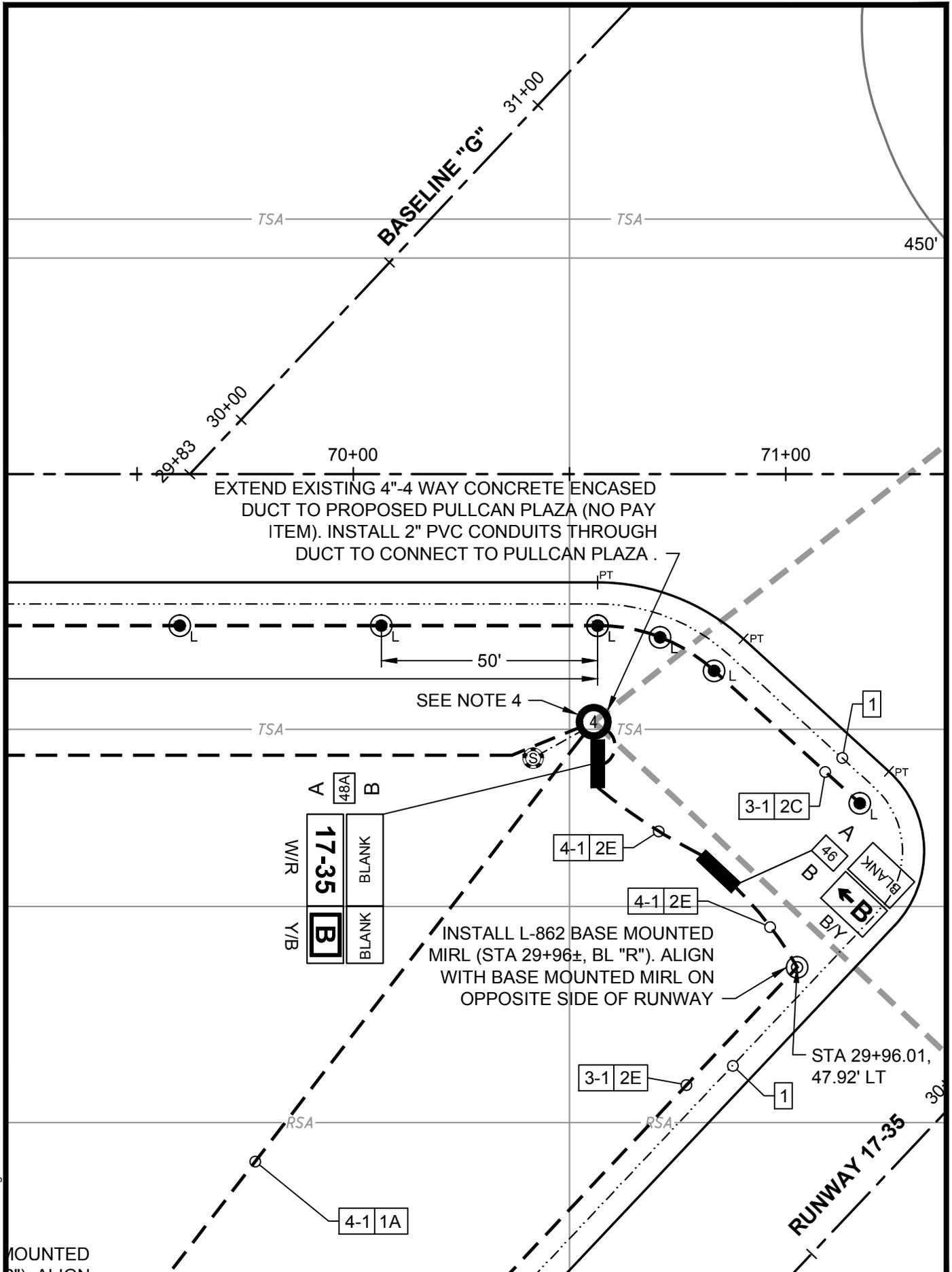


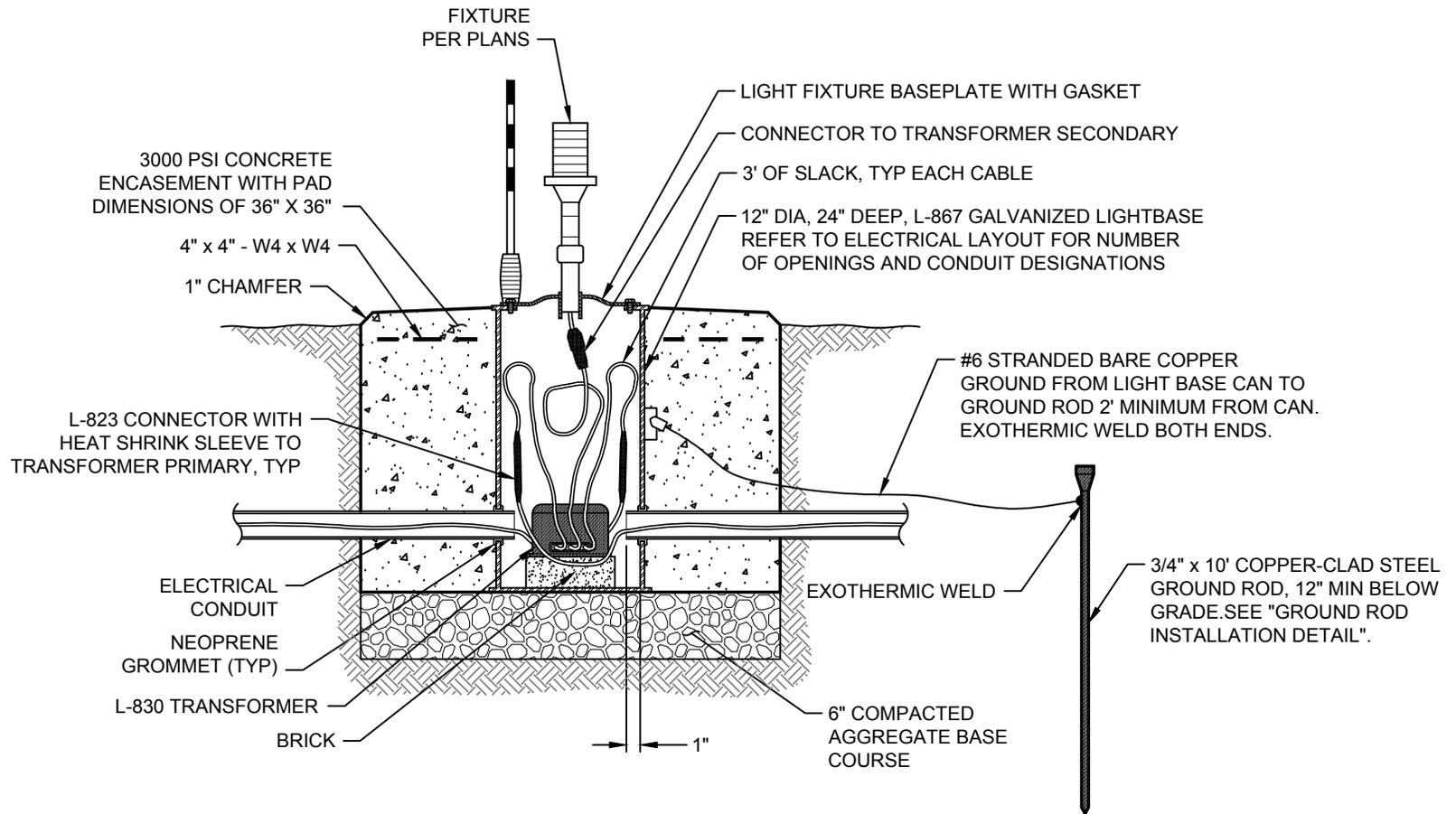
RULE		
	SIZE	NOTES
	#8	1
	#8	1
	#8	1

TRENCH, CONDUIT, & DUCT BANK SCHEDULE	
SYM	DESCRIPTION
1	TRENCH - LIGHTNING PROTECTION (GRASSED SHOULDER)
2	TRENCH - DIRECT BURIED CABLE
3	CONDUIT - WITHOUT COUNTERPOISE (2" PVC)

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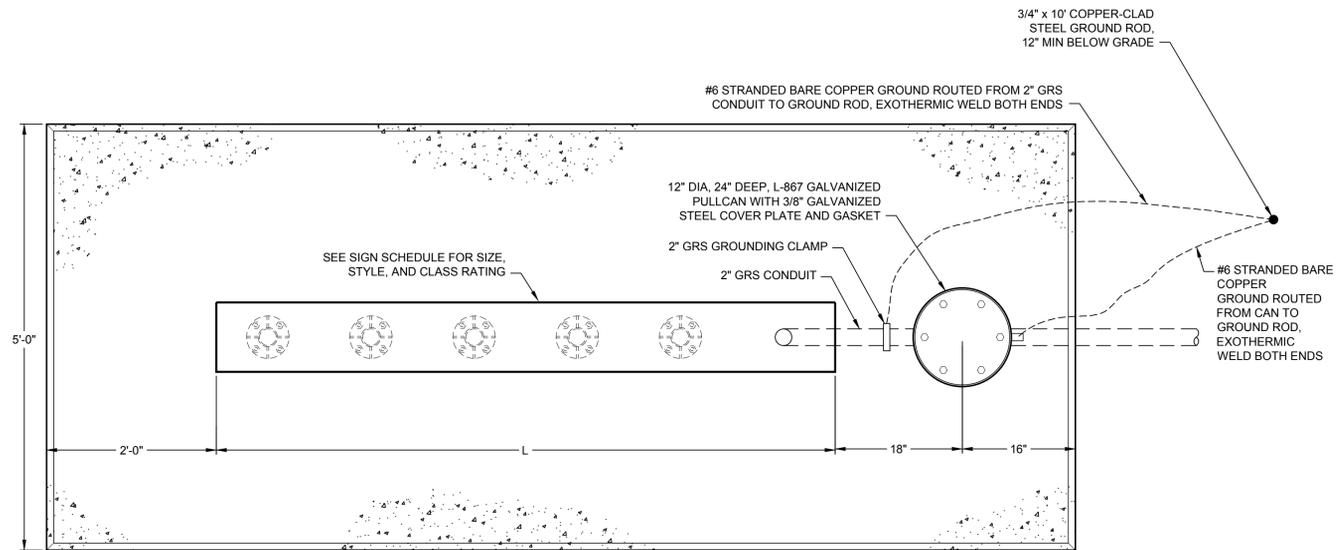


BASE MOUNTED FIXTURES - OPTIONAL PRECAST BASE DETAIL

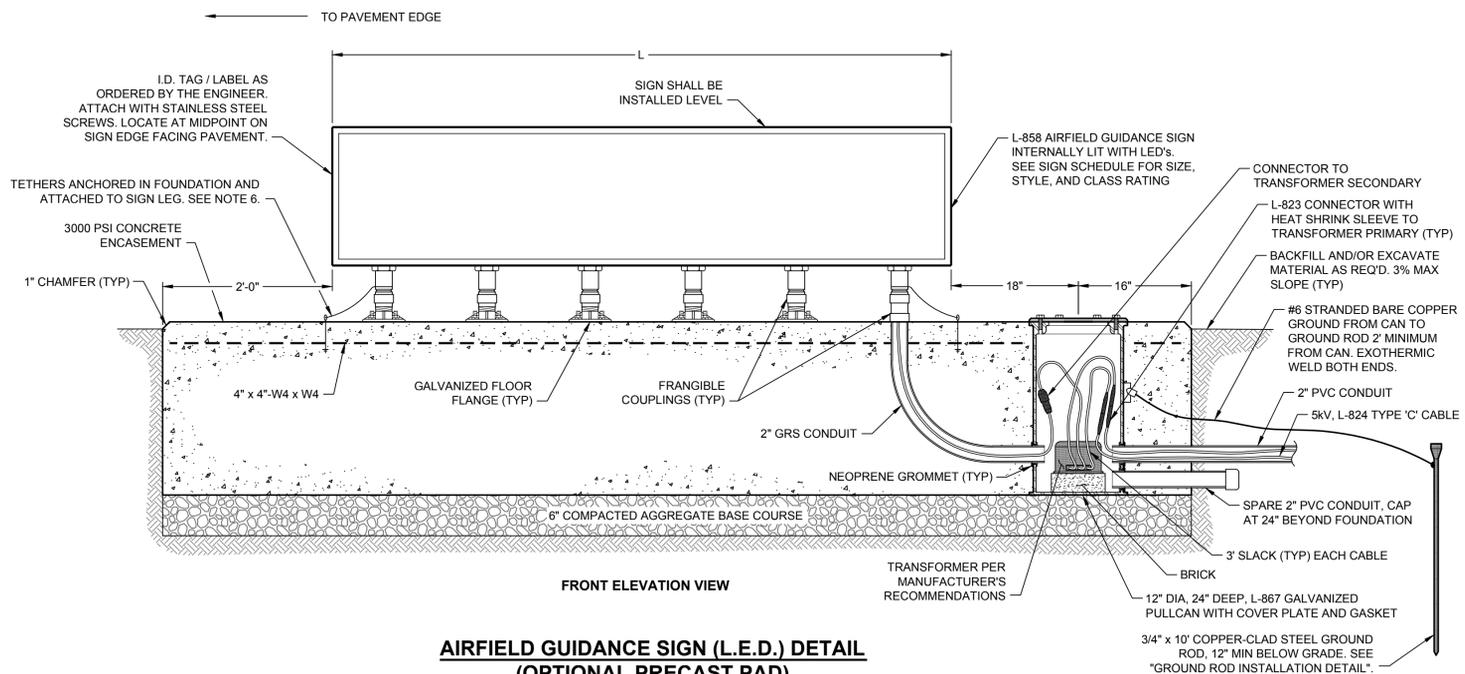
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NOTE:

1. PRECAST BASES MAY ONLY BE USED FOR FIXTURES LOCATED WITHIN THE RUNWAY 4-22 SAFETY AREA.
2. THE USE OF PRECAST BASES IS AT THE CONTRACTOR'S OPTION; NO ADDITIONAL PAYMENT WILL BE MADE FOR THE USE OF PRECAST BASES.



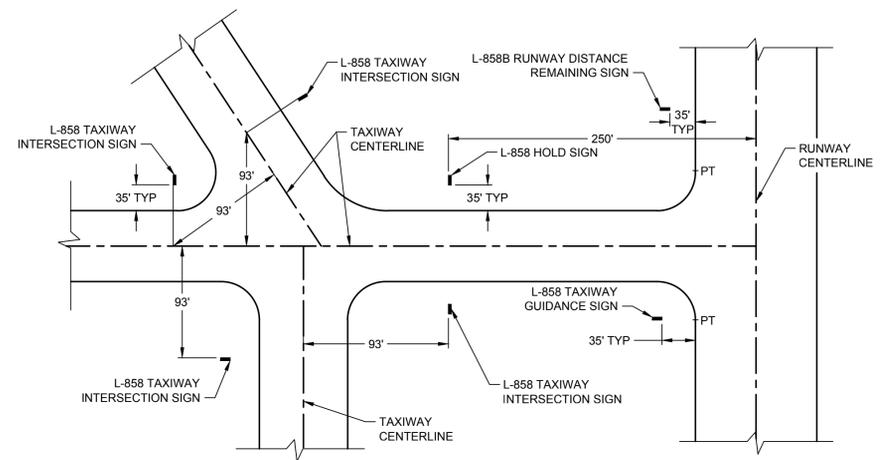
PLAN VIEW



FRONT ELEVATION VIEW

**AIRFIELD GUIDANCE SIGN (L.E.D.) DETAIL
(OPTIONAL PRECAST PAD)**

- NOTES:
1. PROVIDE A L-823 DISCONNECT PLUG AND RECEPTACLE IN THE FRANGIBLE COUPLING OF THE POWER LEG AS REQUIRED.
 2. ALL CONDUIT UNDER THE CONCRETE PAD IS INCIDENTAL TO THE COST OF THE AIRFIELD GUIDANCE SIGN.
 3. ORIENT THE INTERNAL POWER LUGS OF THE AIRFIELD GUIDANCE SIGN SO THAT THEY WILL BE CLOSEST TO THE LEG THROUGH WHICH THE POWER ENTERS THE SIGN HOUSING.
 4. THE #6 STRANDED BARE COPPER GROUND AND THE GROUND ROD ARE INCIDENTAL TO THE COST OF THE AIRFIELD GUIDANCE SIGN.
 5. CONTRACTOR TO COORDINATE DEPTH OF PAD WITH MANUFACTURER'S RECOMMENDED ANCHOR BOLTS.
 6. SINGLE MODULE SIGN REQUIRES ONE TETHER. MULTIPLE MODULE SIGN IN CONTINUOUS FRAME MUST USE TETHER AT BOTH ENDS.
 7. PRECAST PADS MAY ONLY BE USED FOR SIGNS LOCATED WITHIN THE RUNWAY 4-22 SAFETY AREA.
 8. THE USE OF PRECAST PADS IS AT THE CONTRACTOR'S OPTION; NO ADDITIONAL PAYMENT WILL BE MADE FOR THE USE OF PRECAST PADS.



TYPICAL SIGN LAYOUT DETAIL

- NOTE:
1. ALL TAXIWAY TO TAXIWAY INTERSECTION SIGNS SHALL BE COLLOCATED PERPENDICULAR TO THE TAXIWAY CENTERLINE AT THE POINT 93 FEET (MINIMUM) FROM THE INTERSECTING TAXIWAY CENTERLINE, OR AS SHOWN ON THE PLANS.

NO.	REVISIONS	BY	APP.	DATE
1	ADDENDUM NO. ONE (1)	DWS	TJS	7/3/13

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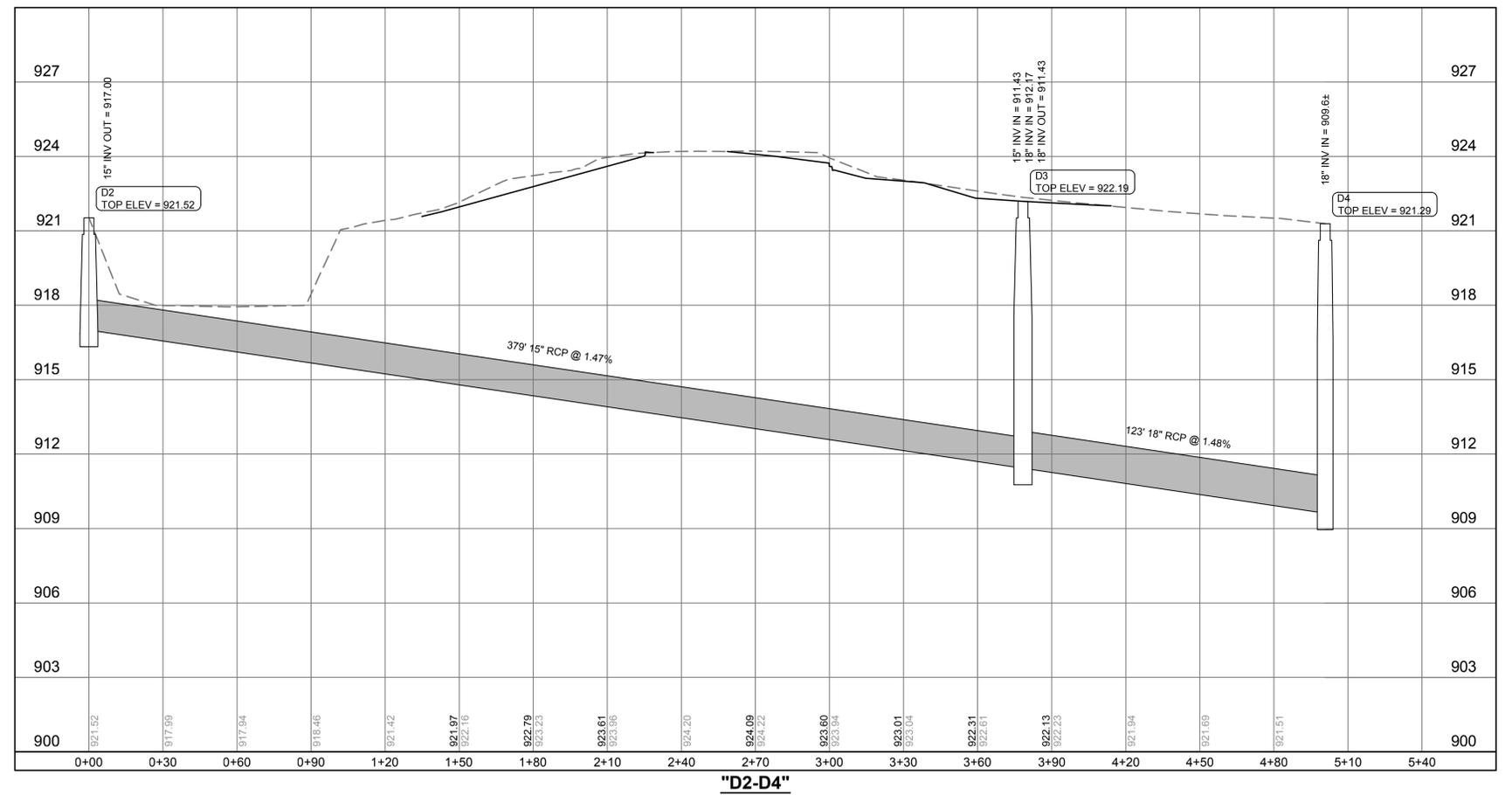
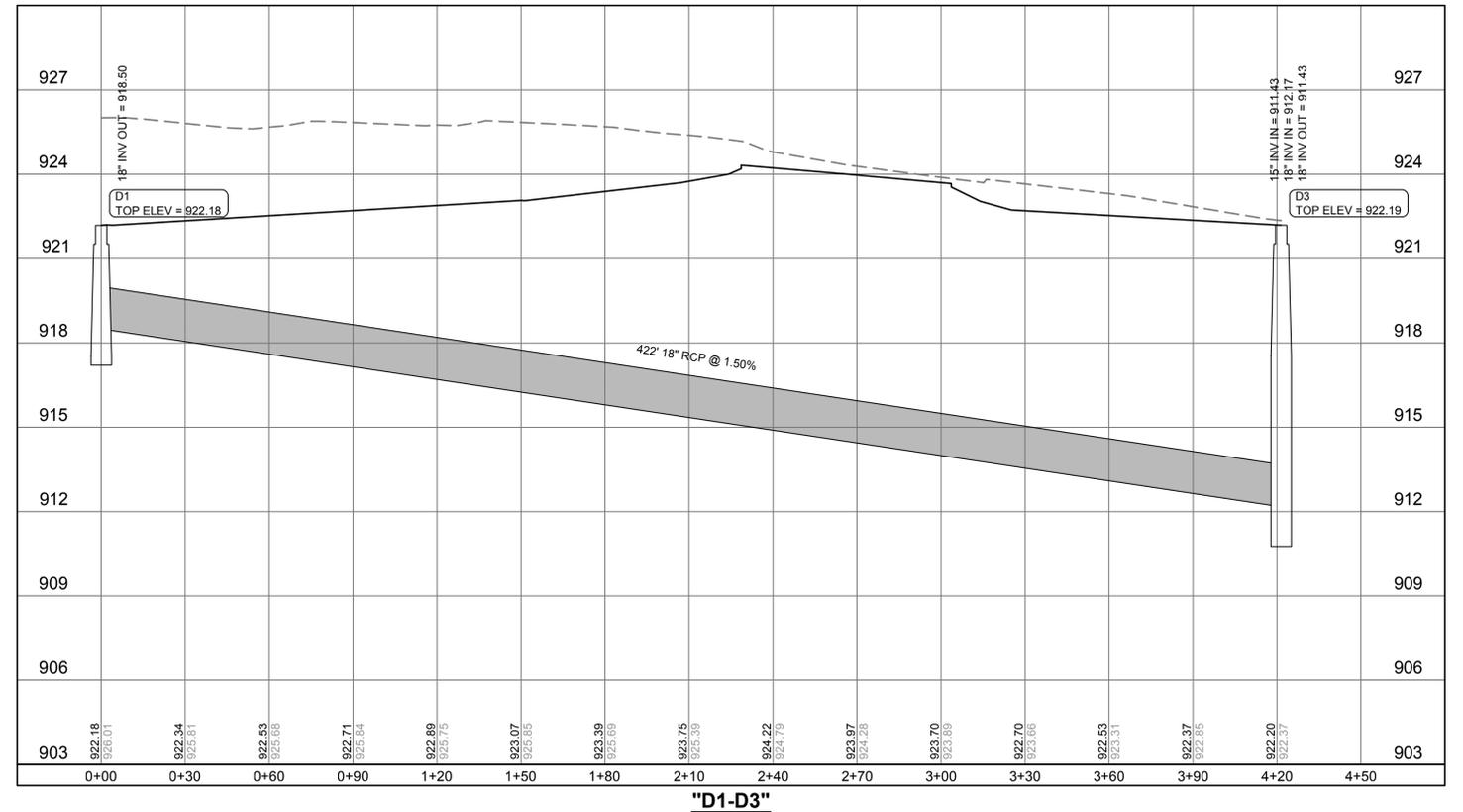
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CONSULTANTS, INC.**



AIRFIELD PAVEMENT REHABILITATION-PHASE 2		AIP NO. 3-51-0029-036 (PENDING)	JOB NO. 12121
AIRFIELD GUIDANCE SIGN DETAILS		DRAWN BY: DWS	SHEET 56
LYNCHBURG REGIONAL AIRPORT		DESIGNED BY: TJS	OF 69
SCALE: NONE	DATE: MAY 2013		

LEGEND

--- EXISTING GRADE
 — PROPOSED GRADE



NO.	REVISIONS	BY	APP.	DATE
1	ADDENDUM NO. ONE (1)	DWS	TJS	7/3/13

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AIRFIELD PAVEMENT REHABILITATION-PHASE 2		AIP NO. 3-51-0029-036 (PENDING)	JOB NO. 12121
DRAINAGE PROFILES		DRAWN BY: DWS	SHEET 61
LYNCHBURG REGIONAL AIRPORT		DESIGNED BY: TJZ	OF 69
SCALE: HORZ. 1" = 30' VERT. 1" = 3'		DATE: MAY 2013	

REVISED PROPOSAL

Name of Bidder

For

AIRFIELD PAVEMENT REHABILITATION – PHASE 2

At

LYNCHBURG REGIONAL AIRPORT
LYNCHBURG, VIRGINIA

SUBMITTED TO

CITY OF LYNCHBURG

AIP PROJECT NO. 3-51-0029-036 (PENDING)

DELTA PROJECT NO. 12121

In compliance with our Invitation for Bids, the undersigned hereby propose to furnish the materials and labor and to perform the work for the completion of items listed in the Schedule below in strict accordance with the Invitation for Bids, Plans, General and Special Provisions, Construction Details, Specifications and all other contract documents for the consideration of the prices quoted in the following schedule of bid items, and agrees, upon receipt of written notice of award, that it will execute a contract in accordance with the bid as accepted and give the required contract bonds with good and sufficient surety, within fifteen (15) calendar days after receipt of notice of formal award of contract and presentation of the prescribed forms.

It is agreed that the undersigned has informed itself fully in regard to all conditions pertaining to the place where the work is to be done; that it has examined the plans and specifications for the work and contractual documents thereto, and has read all the special provisions furnished prior to the opening of bids, and that it has satisfied itself relative to the work to be performed.

It is agreed that the description under each item, being stated, implies although it does not mention, all incidentals and that the prices stated are intended to cover all such work, materials, and incidentals as constitute bidders obligations as described in the specifications, and any details not specifically mentioned, but evidently included in the contract shall be compensated for in the item which most logically includes it.

REVISED PROPOSAL

It is understood that this proposal is submitted for the purpose of obtaining the work included in subject project at the Lynchburg Regional Airport. Said work includes the following general items:

1. Construct Taxiway "B3"
2. Reconstruct / Realign Taxiway "D"
3. Reconstruct / Realign Taxiway "C"

Said work is described in the project contract documents which also include the place, date, and time of opening proposals.

It is understood that separate contracts on individual schedules of work may be awarded, when included in the bid documents.

It is understood that wages not less than the minimum rates or wages, as predetermined for this project by the Secretary of Labor, were used in the preparation of this proposal.

It is understood that all workmanship and materials under all items of work are guaranteed for one year from the date of final acceptance.

It is understood that the Owner reserves the right to accept or reject any or all bids and waive informalities.

It is understood that the quantities of work to be done are approximate only and are intended principally to serve as a guide in evaluation of proposals, with the right reserved by the Owner to delete minor bid items.

The undersigned agrees, that if awarded the contract, it will commence the work not later than ten (10) days from receipt of the Notice to Proceed and that it will complete the work within the time stipulated in this proposal.

It is understood that for each calendar day that any work remains incomplete after the contract time (including all extensions and adjustments as provided in the subsection titled DETERMINATION AND EXTENSION OF CONTRACT TIME) the sum of Three Thousand dollars (\$3,000.00) as liquidated damages will be deducted from any money due or to become due to the Contractor or its Surety. Such deducted sums shall not be deducted as a penalty but shall be considered as liquidation of a reasonable portion of damages that will be incurred by the Owner should the Contractor fail to complete the work in the time provided in its contract.

Enclosed is security as required, consisting of (cash, certified check, or bid bond) _____ payable to the City of Lynchburg, in the amount of \$ _____.

REVISED PROPOSAL

This amount equals 5 percent of the total amount bid submitted by the Contractor.

It is understood that this project is funded by federal, state, and local monies and the Contractor shall be subject to all laws and regulations applicable to recipients of such funds.

The Contractor shall be a licensed Contractor registered with the Commonwealth of Virginia, shall list its registration number at the end of the proposal in the designated location and shall enclose a copy of its licensing certificate. In the event that the registration is pending, or in process, a statement as to the status shall be included instead.

REVISED PROPOSAL

AIP Project No. 3-51-0029-036 (Pending)
Delta Project No. 12121

Schedule 1

Date: _____

Bid Proposal Summary For All Work Depicted In The Plans And Specifications

ITEM NO.	APPROX. QUANTITY	ITEM WITH UNIT PRICE WRITTEN IN WORDS	UNIT PRICES IN FIGURES	EXTENDED TOTAL
			DOLLARS CENTS	DOLLARS CENTS
1 P-100	1 LS	MOBILIZATION _____ _____ PER LUMP SUM		
2 P-140	37,000 SY	PAVEMENT REMOVAL _____ _____ PER SQUARE YARD		
3 P-150	1 LS	MISCELLANEOUS AND ELECTRICAL DEMOLITION _____ _____ PER LUMP SUM		
4 P-150	2 EA	REMOVE L-850C IN-PAVEMENT HIRL _____ _____ PER EACH		
5 P-150	1 EA	REMOVE L-861 BASE MOUNTED MIRL _____ _____ PER EACH		
6 P-150	15 EA	REMOVE L-861T BASE MOUNTED MITL _____ _____ PER EACH		
7 P-150	8 EA	REMOVE L-861T BASE MOUNTED MITL FIXTURE _____ _____ PER EACH		
8 P-150	2 EA	REMOVE L-862 BASE MOUNTED HIRL _____ _____ PER EACH		
9 P-150	12 EA	REMOVE ELECTRICAL MANHOLE/HANDHOLE _____ _____ PER EACH		

REVISED PROPOSAL

AIP Project No. 3-51-0029-036 (Pending)
Delta Project No. 12121

Schedule 1

Date: _____

Bid Proposal Summary For All Work Depicted In The Plans And Specifications

ITEM NO.	APPROX. QUANTITY	ITEM WITH UNIT PRICE WRITTEN IN WORDS	UNIT PRICES IN FIGURES	EXTENDED TOTAL
			DOLLARS CENTS	DOLLARS CENTS
10 P-150	3 EA	REMOVE PULLCAN/JUNCTION STRUCTURE _____ _____ PER EACH		
11 P-150	26 EA	REMOVE AIRFIELD GUIDANCE SIGN _____ _____ PER EACH		
12 P-150	1 EA	REMOVE WINDCONE _____ _____ PER EACH		
13 P-152	27,000 CY	UNCLASSIFIED EXCAVATION _____ _____ PER CUBIC YARD		
14 P-152	5,300 CY	MUCK EXCAVATION _____ _____ PER CUBIC YARD		
15 P-154	9,200 CY	SUBBASE COURSE _____ _____ PER CUBIC YARD		
16 P-156	1 LS	EROSION AND SEDIMENT CONTROL _____ _____ PER LUMP SUM		
17 P-156	1,000 LF	SILT FENCE _____ _____ PER LINEAR FOOT		
18 P-160	600 SY	ASPHALTIC PAVEMENT SURFACE MILLING _____ _____ PER SQUARE YARD		

REVISED PROPOSAL

AIP Project No. 3-51-0029-036 (Pending)
Delta Project No. 12121

Schedule 1

Date: _____

Bid Proposal Summary For All Work Depicted In The Plans And Specifications

ITEM NO.	APPROX. QUANTITY	ITEM WITH UNIT PRICE WRITTEN IN WORDS	UNIT PRICES IN FIGURES	EXTENDED TOTAL
			DOLLARS CENTS	DOLLARS CENTS
19 P-304	14,000 SY	CEMENT TREATED BASE COURSE (6") _____ _____ PER SQUARE YARD		
20 P-401	4,700 TN	BITUMINOUS SURFACE COURSE _____ _____ PER TON		
21 P-403	1,800 TN	BITUMINOUS BASE COURSE _____ _____ PER TON		
22 P-405	15,700 SY	GROUND STABILIZATION FABRIC _____ _____ PER SQUARE YARD		
23 M-103	2 EA	CLOSED RUNWAY MARKER (OWNER-FURNISHED) _____ _____ PER EACH		
24 M-103	2 EA	CLOSED RUNWAY MARKER (CONTRACTOR-FURNISHED) _____ _____ PER EACH		
25 M-103	7 EA	CLOSED TAXIWAY MARKER (CONTRACTOR-FURNISHED) _____ _____ PER EACH		
26 M-107	1,500 EA	AVIATION BARRICADES (OWNER-FURNISHED) _____ _____ PER EACH		
27 M-107	21 EA	BUCKET BARRICADES (CONTRACTOR-FURNISHED) _____ _____ PER EACH		

REVISED PROPOSAL

AIP Project No. 3-51-0029-036 (Pending)
Delta Project No. 12121

Schedule 1

Date: _____

Bid Proposal Summary For All Work Depicted In The Plans And Specifications

ITEM NO.	APPROX. QUANTITY	ITEM WITH UNIT PRICE WRITTEN IN WORDS	UNIT PRICES IN FIGURES		EXTENDED TOTAL	
			DOLLARS	CENTS	DOLLARS	CENTS
28 P-619	6,000 SF	PAINT REMOVAL _____ _____ PER SQUARE FOOT				
29 P-620	15,000 SF	PERMANENT RUNWAY AND TAXIWAY PAINTING _____ _____ PER SQUARE FOOT				
30 P-620	15,000 SF	INITIAL RUNWAY AND TAXIWAY PAINTING _____ _____ PER SQUARE FOOT				
31 P-620	13 EA	THERMOPLASTIC SURFACE PAINTED RUNWAY HOLDING POSITION SIGN _____ _____ PER EACH				
32 D-701	576 LF	15" RCP, CLASS V _____ _____ PER LINEAR FOOT				
33 D-701	545 LF	18" RCP, CLASS V _____ _____ PER LINEAR FOOT				
34 D-701	2 EA	15" VDOT ES-1 FLARED END SECTION _____ _____ PER EACH				
35 D-701	1 EA	18" VDOT ES-1 FLARED END SECTION _____ _____ PER EACH				
36 D-701	1 EA	15" PIPE COLLAR _____ _____ PER EACH				

REVISED PROPOSAL

AIP Project No. 3-51-0029-036 (Pending)
Delta Project No. 12121

Schedule 1

Date: _____

Bid Proposal Summary For All Work Depicted In The Plans And Specifications

ITEM NO.	APPROX. QUANTITY	ITEM WITH UNIT PRICE WRITTEN IN WORDS	UNIT PRICES IN FIGURES	EXTENDED TOTAL
			DOLLARS CENTS	DOLLARS CENTS
46 L-108	7,300 LF	NO. 6 AWG SOLID BARE COPPER COUNTERPOISE WIRE FOR LIGHTNING PROTECTION - PLOWED _____ _____ PER LINEAR FOOT		
47 L-110	100 LF	ELECTRICAL DUCT BANK, 1 WAY - 2" PVC CONDUIT _____ _____ PER LINEAR FOOT		
48 L-110	800 LF	ELECTRICAL DUCT BANK, 2 WAY - 2" PVC CONDUIT _____ _____ PER LINEAR FOOT		
49 L-110	700 LF	ELECTRICAL DUCT BANK, 4 WAY - 2" PVC CONDUIT _____ _____ PER LINEAR FOOT		
50 L-110	8,200 LF	ELECTRICAL CONDUIT, 1 WAY - 2" PVC CONDUIT _____ _____ PER LINEAR FOOT		
51 L-110	400 LF	ELECTRICAL CONDUIT, 2 WAY - 2" PVC CONDUIT _____ _____ PER LINEAR FOOT		
52 L-110	2,000 LF	ELECTRICAL CONDUIT, 4 WAY - 2" PVC CONDUIT _____ _____ PER LINEAR FOOT		
53 L-110	30 LF	ELECTRICAL CONDUIT - BORED UNDER PAVEMENT, 2 WAY - 2" HDPE _____ _____ PER LINEAR FOOT		
54 L-110	200 LF	ELECTRICAL CONDUIT - BORED UNDER PAVEMENT, 4 WAY - 2" HDPE _____ _____ PER LINEAR FOOT		

REVISED PROPOSAL

AIP Project No. 3-51-0029-036 (Pending)
Delta Project No. 12121

Schedule 1

Date: _____

Bid Proposal Summary For All Work Depicted In The Plans And Specifications

ITEM NO.	APPROX. QUANTITY	ITEM WITH UNIT PRICE WRITTEN IN WORDS	UNIT PRICES IN FIGURES	EXTENDED TOTAL
			DOLLARS CENTS	DOLLARS CENTS
55 L-115	1 EA	ELECTRICAL JUNCTION STRUCTURE - ELECTRICAL HANDHOLE (AIRCRAFT RATED) _____ _____ PER EACH		
56 L-115	9 EA	ELECTRICAL JUNCTION STRUCTURE, L-867 PULLCAN _____ _____ PER EACH		
57 L-115	6 EA	ELECTRICAL JUNCTION STRUCTURE, 2 UNIT L-867 PULLCAN PLAZA _____ _____ PER EACH		
58 L-115	11 EA	ELECTRICAL JUNCTION STRUCTURE, 4 UNIT L-867 PULLCAN PLAZA _____ _____ PER EACH		
59 L-125	1 EA	L-858 AIRFIELD GUIDANCE SIGN (1-2 CHAR.) _____ _____ PER EACH		
60 L-125	19 EA	L-858 AIRFIELD GUIDANCE SIGN (3-4 CHAR.) _____ _____ PER EACH		
61 L-125	6 EA	L-858 AIRFIELD GUIDANCE SIGN (5-6 CHAR.) _____ _____ PER EACH		
62 L-125	1 EA	L-858 AIRFIELD GUIDANCE SIGN (CONTACT ATC SIGN) _____ _____ PER EACH		
63 L-125	1 EA	L-850C IN-PAVEMENT HIRL _____ _____ PER EACH		

REVISED PROPOSAL

AIP Project No. 3-51-0029-036 (Pending)
Delta Project No. 12121

Schedule 1

Date: _____

Bid Proposal Summary For All Work Depicted In The Plans And Specifications

ITEM NO.	APPROX. QUANTITY	ITEM WITH UNIT PRICE WRITTEN IN WORDS	UNIT PRICES IN FIGURES	EXTENDED TOTAL
			DOLLARS CENTS	DOLLARS CENTS
64 L-125	2 EA	L-861 BASE MOUNTED MIRL _____ _____ PER EACH		
65 L-125	6 EA	L-861T BASE MOUNTED MITL WITH RELOCATED FIXTURE _____ _____ PER EACH		
66 L-125	116 EA	L-861T(L) BASE MOUNTED MITL (LED) _____ _____ PER EACH		
67 L-125	15 EA	L-861T(L) BASE MOUNTED MITL (LED) ON EXISTING BASE _____ _____ PER EACH		
68 L-125	3 EA	L-862 BASE MOUNTED HIRL _____ _____ PER EACH		
69 L-125	5 EA	L-853 TYPE II RETROREFLECTIVE MARKER _____ _____ PER EACH		
70 L-125	1 EA	REINSTALL AND UPGRADE WIND CONE _____ _____ PER EACH		
71 L-129	16,000 LF	NO. 6 AWG, L-824C CABLE, 600V _____ _____ PER LINEAR FOOT		
72 L-129	1,000 LF	25 PAIR SHIELDED CONTROL CABLE _____ _____ PER LINEAR FOOT		

REVISED PROPOSAL

AIP Project No. 3-51-0029-036 (Pending)
Delta Project No. 12121

Schedule 1

Date: _____

Bid Proposal Summary For All Work Depicted In The Plans And Specifications

ITEM NO.	APPROX. QUANTITY	ITEM WITH UNIT PRICE WRITTEN IN WORDS	UNIT PRICES IN FIGURES	EXTENDED TOTAL
			DOLLARS CENTS	DOLLARS CENTS
73 L-129	700 LF	WIND SENSOR POWER CABLE _____ _____ PER LINEAR FOOT		
74 L-129	700 LF	WIND SENSOR COMMUNICATIONS CABLE _____ _____ PER LINEAR FOOT		
75 L-130	1 LS	RELOCATE WIND SENSOR _____ _____ PER LUMP SUM		
76 R-414	100 SY	RIPRAP, CLASS I _____ _____ PER SQUARE YARD		
77 R-606	100 SY	SOIL STABILIZATION MAT, EC-2 _____ _____ PER SQUARE YARD		
78 SP-28	1 LS	WATERLINE ADJUSTMENT ALLOWANCE _____ _____ PER LUMP SUM		

Contract Time: 150 Calendar Days

Liquidated Damages: \$3,000 / Calendar Day

Total Bid Amount \$ _____

SP-1 Non-AIP Insurance Allowance \$ _____

REVISED PROPOSAL

AIP Project No. 3-51-0029-036 (Pending)
Delta Project No. 12121

Schedule 2

Date: _____

Bid Proposal Summary For All Work Depicted In The Plans And Specifications

ITEM NO.	APPROX. QUANTITY	ITEM WITH UNIT PRICE WRITTEN IN WORDS	UNIT PRICES IN FIGURES	EXTENDED TOTAL
			DOLLARS CENTS	DOLLARS CENTS
1 P-100	1 LS	MOBILIZATION _____ _____ PER LUMP SUM		
2 P-140	37,000 SY	PAVEMENT REMOVAL _____ _____ PER SQUARE YARD		
3 P-150	1 LS	MISCELLANEOUS AND ELECTRICAL DEMOLITION _____ _____ PER LUMP SUM		
4 P-150	2 EA	REMOVE L-850C IN-PAVEMENT HIRL _____ _____ PER EACH		
5 P-150	1 EA	REMOVE L-861 BASE MOUNTED MIRL _____ _____ PER EACH		
6 P-150	137 EA	REMOVE L-861T BASE MOUNTED MITL _____ _____ PER EACH		
7 P-150	15 EA	REMOVE L-861T BASE MOUNTED MITL FIXTURE _____ _____ PER EACH		
8 P-150	2 EA	REMOVE L-862 BASE MOUNTED HIRL _____ _____ PER EACH		
9 P-150	12 EA	REMOVE ELECTRICAL MANHOLE/HANDHOLE _____ _____ PER EACH		

REVISED PROPOSAL

AIP Project No. 3-51-0029-036 (Pending)
Delta Project No. 12121

Schedule 2

Date: _____

Bid Proposal Summary For All Work Depicted In The Plans And Specifications

ITEM NO.	APPROX. QUANTITY	ITEM WITH UNIT PRICE WRITTEN IN WORDS	UNIT PRICES IN FIGURES	EXTENDED TOTAL
			DOLLARS CENTS	DOLLARS CENTS
10 P-150	3 EA	REMOVE PULLCAN/JUNCTION STRUCTURE _____ _____ PER EACH		
11 P-150	26 EA	REMOVE AIRFIELD GUIDANCE SIGN _____ _____ PER EACH		
12 P-150	1 EA	REMOVE WINDCONE _____ _____ PER EACH		
13 P-152	27,000 CY	UNCLASSIFIED EXCAVATION _____ _____ PER CUBIC YARD		
14 P-152	5,600 CY	MUCK EXCAVATION _____ _____ PER CUBIC YARD		
15 P-154	7,000 CY	SUBBASE COURSE _____ _____ PER CUBIC YARD		
16 P-156	1 LS	EROSION AND SEDIMENT CONTROL _____ _____ PER LUMP SUM		
17 P-156	1,000 LF	SILT FENCE _____ _____ PER LINEAR FOOT		
18 P-160	600 SY	ASPHALTIC PAVEMENT SURFACE MILLING _____ _____ PER SQUARE YARD		

REVISED PROPOSAL

AIP Project No. 3-51-0029-036 (Pending)
Delta Project No. 12121

Schedule 2

Date: _____

Bid Proposal Summary For All Work Depicted In The Plans And Specifications

ITEM NO.	APPROX. QUANTITY	ITEM WITH UNIT PRICE WRITTEN IN WORDS	UNIT PRICES IN FIGURES	EXTENDED TOTAL
			DOLLARS CENTS	DOLLARS CENTS
19 P-304	14,500 SY	CEMENT TREATED BASE COURSE (4") _____ _____ PER SQUARE YARD		
20 P-401	600 TN	BITUMINOUS SURFACE COURSE _____ _____ PER TON		
21 P-403	1,500 TN	BITUMINOUS BASE COURSE _____ _____ PER TON		
22 P-405	16,800 SY	GROUND STABILIZATION FABRIC _____ _____ PER SQUARE YARD		
23 P-501	16,200 SY	PORTLAND CEMENT CONCRETE PAVEMENT (12") _____ _____ PER SQUARE YARD		
24 P-601	3,000 LF	EXPANSION JOINT _____ _____ PER LINEAR FOOT		
25 M-103	2 EA	CLOSED RUNWAY MARKER (OWNER-FURNISHED) _____ _____ PER EACH		
26 M-103	2 EA	CLOSED RUNWAY MARKER (CONTRACTOR-FURNISHED) _____ _____ PER EACH		
27 M-103	7 EA	CLOSED TAXIWAY MARKER (CONTRACTOR-FURNISHED) _____ _____ PER EACH		

REVISED PROPOSAL

AIP Project No. 3-51-0029-036 (Pending)
Delta Project No. 12121

Schedule 2

Date: _____

Bid Proposal Summary For All Work Depicted In The Plans And Specifications

ITEM NO.	APPROX. QUANTITY	ITEM WITH UNIT PRICE WRITTEN IN WORDS	UNIT PRICES IN FIGURES	EXTENDED TOTAL
			DOLLARS CENTS	DOLLARS CENTS
28 M-107	1,500 EA	AVIATION BARRICADES (OWNER-FURNISHED) _____ _____ PER EACH		
29 M-107	21 EA	BUCKET BARRICADES (CONTRACTOR-FURNISHED) _____ _____ PER EACH		
30 P-605	21,200 LF	JOINT SEALING FILLER _____ _____ PER LINEAR FOOT		
31 P-619	6,000 SF	PAINT REMOVAL _____ _____ PER SQUARE FOOT		
32 P-620	15,000 SF	PERMANENT RUNWAY AND TAXIWAY PAINTING _____ _____ PER SQUARE FOOT		
33 P-620	13 EA	THERMOPLASTIC SURFACE PAINTED RUNWAY HOLDING POSITION SIGN _____ _____ PER EACH		
34 D-701	576 LF	15" RCP, CLASS V _____ _____ PER LINEAR FOOT		
35 D-701	545 LF	18" RCP, CLASS V _____ _____ PER LINEAR FOOT		
36 D-701	2 EA	15" VDOT ES-1 FLARED END SECTION _____ _____ PER EACH		

REVISED PROPOSAL

AIP Project No. 3-51-0029-036 (Pending)
Delta Project No. 12121

Schedule 2

Date: _____

Bid Proposal Summary For All Work Depicted In The Plans And Specifications

ITEM NO.	APPROX. QUANTITY	ITEM WITH UNIT PRICE WRITTEN IN WORDS	UNIT PRICES IN FIGURES	EXTENDED TOTAL
			DOLLARS CENTS	DOLLARS CENTS
37 D-701	1 EA	18" VDOT ES-1 FLARED END SECTION _____ _____ PER EACH		
38 D-701	1 EA	15" PIPE COLLAR _____ _____ PER EACH		
39 D-705	100 LF	6" PVC UNDERDRAIN PIPE _____ _____ PER LINEAR FOOT		
40 D-751	1 EA	36" DIA. AIRCRAFT RATED MANHOLE W/ FRAME AND SOLID LID (185,000 # DUAL WHEEL GEAR) _____ _____ PER EACH		
41 D-751	1 EA	60" DIA. AIRCRAFT RATED MANHOLE W/ FRAME AND SOLID LID (185,000 # DUAL WHEEL GEAR) _____ _____ PER EACH		
42 D-751	1 EA	72" DIA. MANHOLE, ST'D. MH-2 WITH ECCENTRIC CONE _____ _____ PER EACH		
43 D-751	2 EA	60" DIA. DROP INLET, ST'D. DI-7B WITH TYPE I GRATE _____ _____ PER EACH		
44 T-901	15 AC	SEEDING _____ _____ PER ACRE		
45 T-908	15 AC	MULCHING _____ _____ PER ACRE		

REVISED PROPOSAL

AIP Project No. 3-51-0029-036 (Pending)
Delta Project No. 12121

Schedule 2

Date: _____

Bid Proposal Summary For All Work Depicted In The Plans And Specifications

ITEM NO.	APPROX. QUANTITY	ITEM WITH UNIT PRICE WRITTEN IN WORDS	UNIT PRICES IN FIGURES	EXTENDED TOTAL
			DOLLARS CENTS	DOLLARS CENTS
64 L-125	1 EA	L-858 AIRFIELD GUIDANCE SIGN (CONTACT ATC SIGN) _____ _____ PER EACH		
65 L-125	1 EA	L-850C IN-PAVEMENT HIRL _____ _____ PER EACH		
66 L-125	2 EA	L-861 BASE MOUNTED MIRL _____ _____ PER EACH		
67 L-125	6 EA	L-861T BASE MOUNTED MITL WITH RELOCATED FIXTURE _____ _____ PER EACH		
68 L-125	116 EA	L-861T(L) BASE MOUNTED MITL (LED) _____ _____ PER EACH		
69 L-125	15 EA	L-861T(L) BASE MOUNTED MITL (LED) ON EXISTING BASE _____ _____ PER EACH		
70 L-125	3 EA	L-862 BASE MOUNTED HIRL _____ _____ PER EACH		
71 L-125	5 EA	L-853 TYPE II RETROREFLECTIVE MARKER _____ _____ PER EACH		
72 L-125	1 EA	REINSTALL AND UPGRADE WIND CONE _____ _____ PER EACH		

REVISED PROPOSAL

CONTRACTOR'S CERTIFICATION OF NONSEGREGATED FACILITIES

The federally assisted construction contractor certifies that it does not maintain or provide, for its employees, any segregated facilities at any of its establishments and that it does not permit employees to perform services at any location, under its control, where segregated facilities are maintained. The federally assisted construction contractor certifies that it will not maintain or provide, for its employees, segregated facilities at any of its establishments and that it will not permit its employees to perform services at any location, under its control, where segregated facilities are maintained. The federally assisted construction contractor agrees that a breach of this certification is a violation of the equal opportunity clause in this contract. As used in this certification, the term "segregated facilities" means any waiting room, work areas, restrooms and washrooms, restaurants and other eating areas, timeclocks, locker rooms and other storage or dressing areas, parking lots, drinking fountains, recreation or entertainment areas, transportation, and housing facilities provided for employees which are segregated by explicit directives or are in fact segregated on the basis of race, color, religion, or national origin because of habit, local custom, or any other reason. The federally assisted construction contractor agrees that (except where it has obtained identical certifications from proposed subcontractors for specific time periods) it will obtain identical certifications from proposed subcontractors prior to the award of subcontracts exceeding \$10,000 which are not exempt from the provisions of the equal opportunity clause and that it will retain such certifications in its files.

The information above is true and complete to the best of my knowledge.

Name and Title (please type)

Signature

Date

NOTE: The penalty for making false statements in offers is prescribed in 18 U.S.C. 1001

REVISED PROPOSAL

CONTRACTOR'S CERTIFICATION OF ELIGIBILITY

By entering into this contract, the Contractor certifies that neither it (not he or she) nor any person or firm who has an interest in the Contractor's firm is a person or firm ineligible to be awarded Government contracts by virtue of Section 3(a) of the Davis-Bacon Act or 29 CFR 5.12 (a)(1).

That, no part of this contract shall be subcontracted to any person or firm ineligible for award of a Government contract by virtue of the same citation;

The bidder/offeror certifies, by submission of this proposal or acceptance of this contract, that neither it nor its principals is presently debarred, suspended, proposed for debarment, declared ineligible, or voluntarily excluded from participation in this transaction by any Federal department or agency. It further agrees by submitting this proposal that it will include this clause without modification in all lower tier transactions, solicitations, proposals, contracts, and subcontracts. Where the bidder/offer/contractor or any lower tier participant is unable to certify to this statement, it shall attach an explanation to this solicitation/proposal.

That, the information above is true and complete to the best of my knowledge.

Name and Title (please print)

Signature

Title

Date

NOTE: The penalty for making false statements in offers is prescribed in 18 U.S.C. 1001.

REVISED PROPOSAL

**CERTIFICATION FOR CONTRACTS, GRANTS, LOANS
AND COOPERATIVE AGREEMENTS**

The undersigned certifies, to the best of his or her knowledge and belief, that:

(1) No Federal appropriated funds have been paid or will be paid; by or on behalf of the undersigned, to any person for influencing or attempting to influence an officer or employee of any agency, a member of Congress, an officer or employee of Congress, or an employee of a Member of Congress in connection with the awarding of any Federal contract, the making of any Federal grant, the making of any Federal Loan, the entering into of any cooperative agreement, and the extension, continuation, renewal, amendment, or modification of any Federal contract, grant, loan, or cooperative agreement.

(2) If any funds other than Federal appropriated funds have been paid or will be paid to any person for influencing or attempting to influence an officer or employee of any agency, a Member of Congress, an officer or employee of Congress, or an employee of a Member of Congress in connection with this Federal contract grant, loan, or cooperative agreement, the undersigned shall complete and submit Standard Form-LLL, "Disclosure of Lobby Activities," in accordance with its instructions.

(3) The undersigned shall require that the language of this certification be included in the award documents for all subawards at all tiers (including subcontracts, subgrants, and contracts under grants, loans, and cooperative agreements) and that all subrecipients shall certify and disclose accordingly.

This certification is a material representation of fact upon which reliance was placed when this transaction was made or entered into. Submission of this certification is a prerequisite for making or entering into this transaction imposed by section 1352, title 31, U.S. Code. Any person who fails to file the required certification shall be subject to a civil penalty of not less than \$10,000 for each such failure.

Signed: _____

Date: _____

Typed Name and Title of Representative

REVISED PROPOSAL

DISADVANTAGED BUSINESS ENTERPRISE UTILIZATION: The undersigned has satisfied the requirements of the specifications in the following manner (please check the appropriate space):

- The bidder is committed to a minimum of 2.0 % DBE utilization on this project.
- The bidder (if unable to meet the goal of 2.0 % DBE) is committed to a minimum of 2.0 % DBE utilization on this project and has submitted documentation showing good faith effort.

Contractor _____

State Registration
No. _____

By _____
(Signature) (Title)

Address _____

Phone No. _____ Zip Code _____

REVISED PROPOSAL

Fostering Small Business Participation	
Sponsor's Name:	
Airport Name:	
City, State:	
AIP Number:	
Federal Fiscal Year:	

In accordance with the Sponsor's DBE program, the following detailed list shall be completed by Prime Contractor(s) for Construction Work Items. The firms listed below may or may not be certified DBEs.

Small Business Firms to be Utilized (Name, Address, Phone)		Work to be Performed	Total Estimated Cost of Work
Name			
Address			
City, St, Zip			
Telephone			
Is the firm a Certified DBE?	<input type="checkbox"/> Yes <input type="checkbox"/> No		
Name			
Address			
City, St, Zip			
Telephone			
Is the firm a Certified DBE?	<input type="checkbox"/> Yes <input type="checkbox"/> No		
Name			
Address			
City, St, Zip			
Telephone			
Is the firm a Certified DBE?	<input type="checkbox"/> Yes <input type="checkbox"/> No		
City, St, Zip			
Telephone			
Is the firm a Certified DBE?	<input type="checkbox"/> Yes <input type="checkbox"/> No		

(Duplicate form as necessary.)

REVISED PROPOSAL

LETTER OF INTENT

Name of Bidder's Firm: _____

Bidder's Address: _____

City: _____ State _____ Zip _____

Name of DBE Firm: _____

Address: _____

City: _____ State _____ Zip _____

Telephone: _____ Area Code _____

State DBE Certification Number: _____

Description of work to be performed by DBE firm:

Bidder intends to utilize the above-named minority firm for the work described above. The estimated amount of work is valued at \$_____.

If the above-named bidder is not determined to be the successful bidder, the Letter of Intent shall be null and void.

(Copy this page for each minority subcontractor)

REVISED PROPOSAL

BUY AMERICAN CERTIFICATE JAN (1991)

By submitting a bid/proposal under this solicitation, except for those items listed by the offeror below or on a separate and clearly identified attachment to this bid/proposal, the offeror certifies that steel and each manufactured product, is produced in the United States (as defined in the clause Buy American – Steel and Manufactured Products or Buy American – Steel and Manufactured Products for Construction Contracts) and that components of unknown origin are considered to have been produced or manufactured outside the United States.

Offerors may obtain from (insert Sponsor Representative) lists of articles, materials, and supplies excepted from this provision.

PRODUCT	COUNTRY OF ORIGIN
_____	_____
_____	_____
_____	_____
_____	_____
_____	_____
_____	_____
_____	_____
_____	_____

REVISED PROPOSAL

Virginia

CERTIFICATION OF NON-COLLUSION

My signature certifies that the accompanying bid is not the result of or affected by, any act of collusion with another person or company engaged in the same line of business or commerce, or any act of fraud punishable under Title 18.2, Chapter 12, Article 1.1 of the **Code of Virginia**, 1950, as amended. Furthermore, I understand that fraudulent and collusive bidding is a crime under the Virginia Governmental Frauds Act, the Virginia Government Bid Rigging Act, the Virginia Antitrust Act, and Federal Law and can result in fines, prison sentences, and civil damage awards.

I agree to abide by all conditions of this bid and certify that I am authorized to sign this bid for the bidder.

NAME OF BIDDER _____

ADDRESS _____

SIGNATURE _____

NAME (TYPE OR PRINT) _____

OFFICIAL TITLE _____

DATE _____

TELEPHONE NO. ()
Area Code

"REGISTERED VIRGINIA CONTRACTOR'S LICENSE NO." _____

Delete the line above

REVISED PROPOSAL

The undersigned hereby acknowledges the receipt of the following Addenda to the Contract Documents.

Addendum No. One Issued _____ (DATE)
Addendum No. Two Issued _____ (DATE)
Addendum No. Three Issued _____ (DATE)
Addendum No. Four Issued _____ (DATE)
Addendum No. Five Issued _____ (DATE)

Name of Bidder

Signature

Name & Title of Signing Official

Business Address: _____

Telephone: _____

VIRGINIA CONTRACTOR'S LICENSE NO: _____

****END OF REVISED PROPOSAL ****