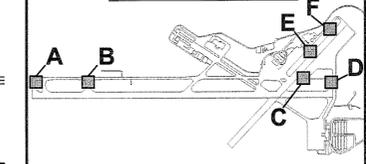


INSET VIEW INDEX

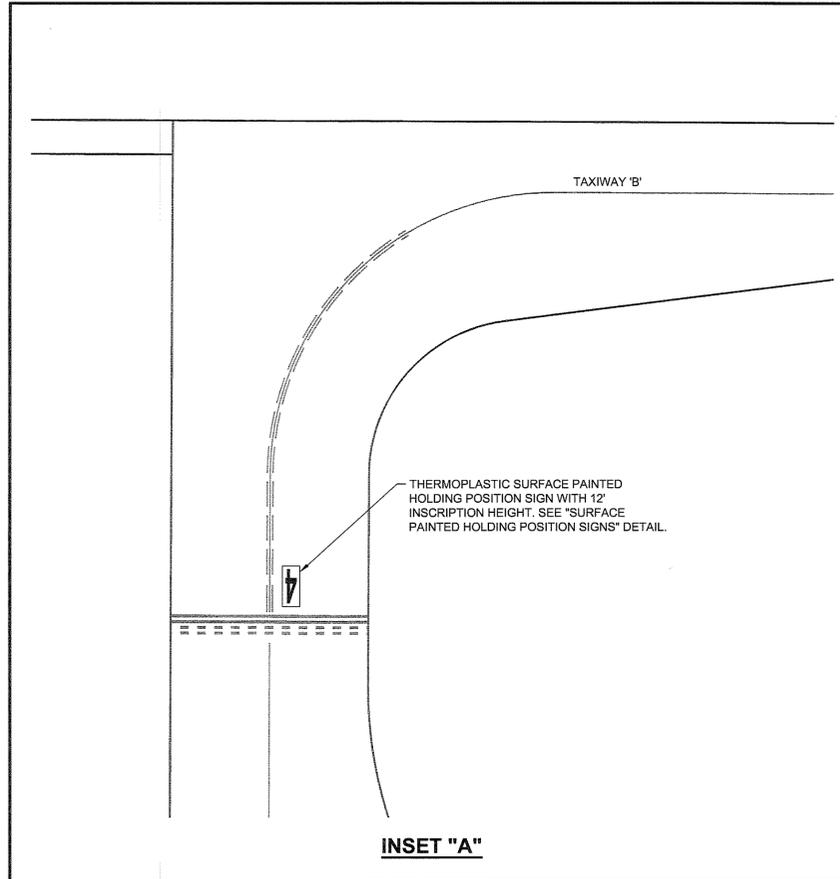
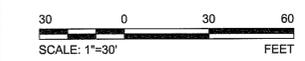


LEGEND

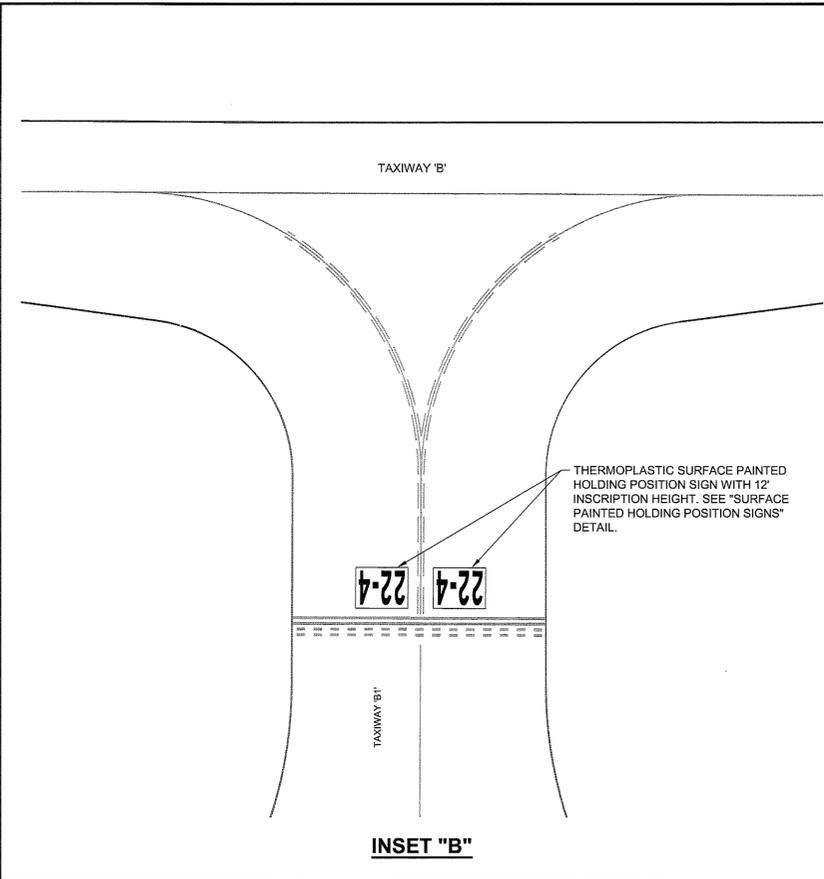
- EXISTING MARKING
- - - PROPOSED MARKING

NOTES

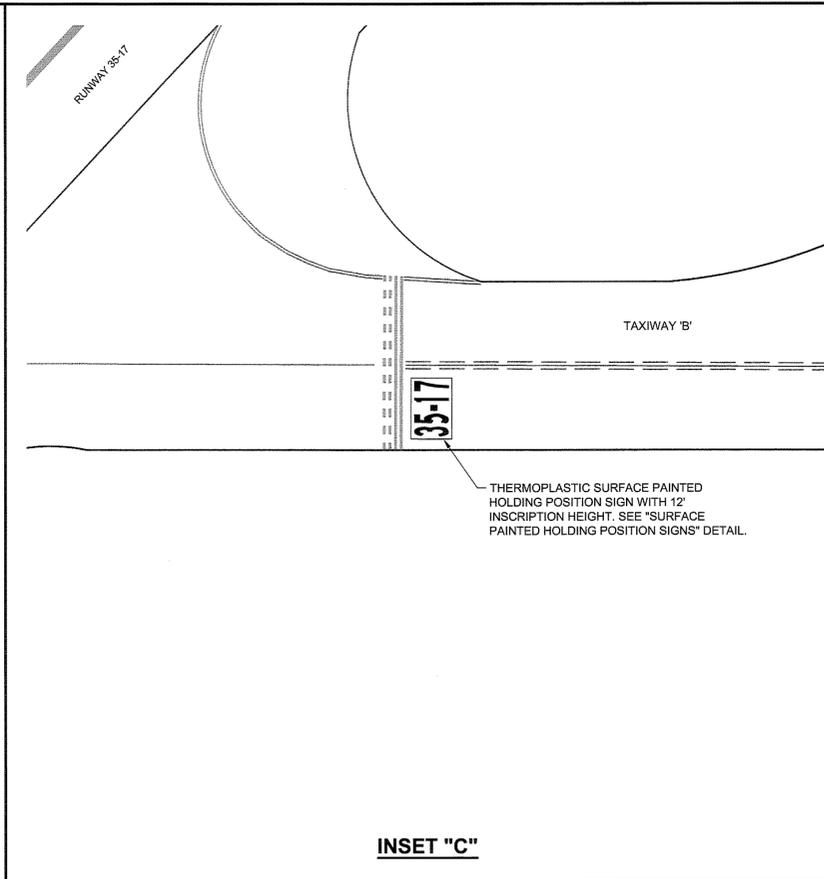
1. ALL STATIONS AND OFFSETS GIVEN WITH RESPECT TO BASELINE "RW" (RUNWAY 4-22 CENTERLINE) UNLESS OTHERWISE NOTED.
2. REFER TO "MARKING NOTES & DETAILS" SHEET FOR MARKING NOTES.
3. LOCATIONS OF EXISTING MARKINGS ARE APPROXIMATE AND SHALL BE VERIFIED IN THE FIELD BY THE CONTRACTOR.
4. ALL SURFACE PAINTED RUNWAY HOLDING POSITION SIGNS SHALL UTILIZE PREFORMED THERMOPLASTIC MARKINGS.
5. EXISTING SURFACE PAINTED HOLDING POSITION SIGNS ARE TO BE REMOVED UNDER ITEM P-819 PRIOR TO PLACEMENT OF NEW THERMOPLASTIC SURFACE PAINTED HOLDING POSITION SIGNS.
6. THE SIZES SHOWN FOR THE THERMOPLASTIC SURFACE PAINTED HOLDING POSITION SIGNS ARE APPROXIMATE AND SHALL BE CONFIRMED IN THE FIELD WITH THE ENGINEER AND OWNER PRIOR TO MANUFACTURE AND INSTALLATION.
7. INSTALLATION OF THE SIGNS (INCLUDING PAINT REMOVAL) SHALL BE COORDINATED WITH THE ENGINEER AND OWNER.



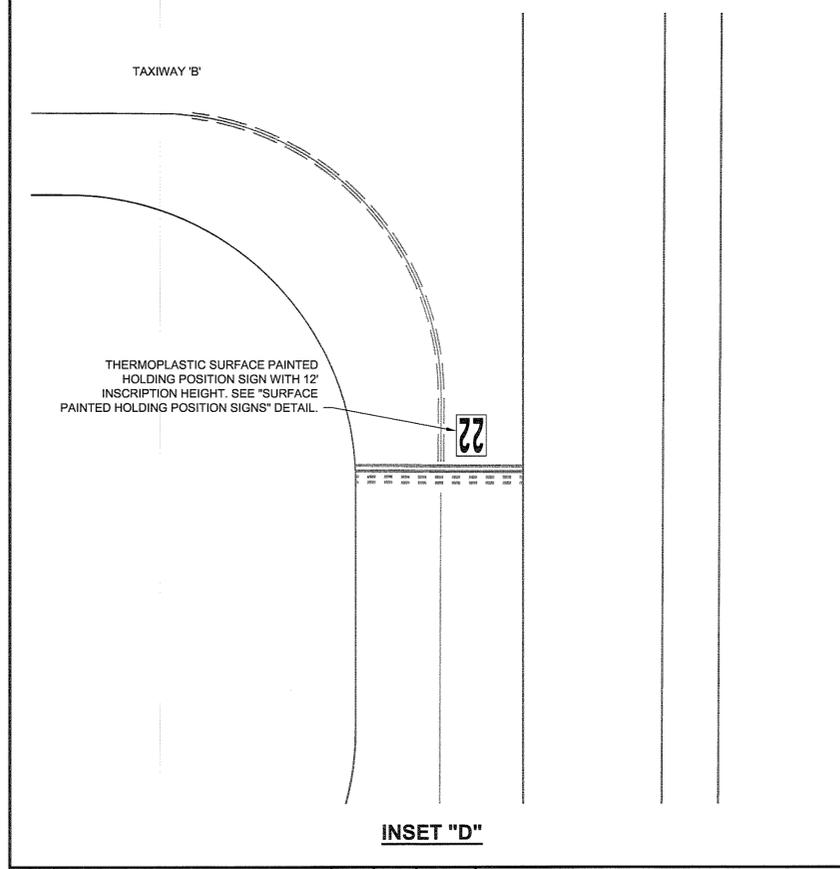
INSET "A"



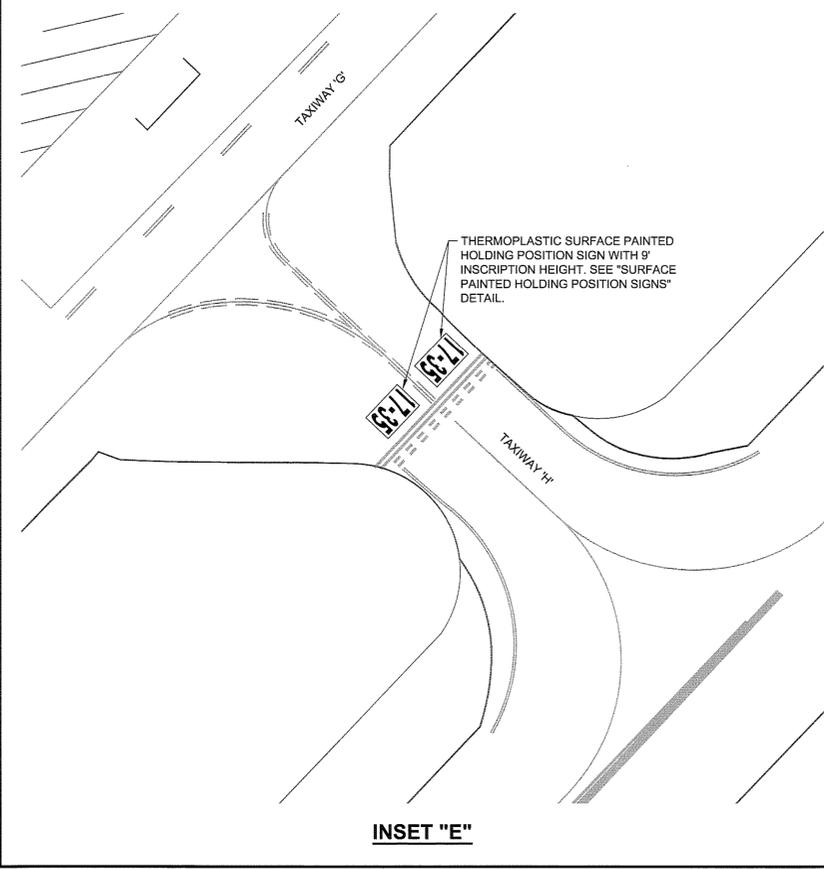
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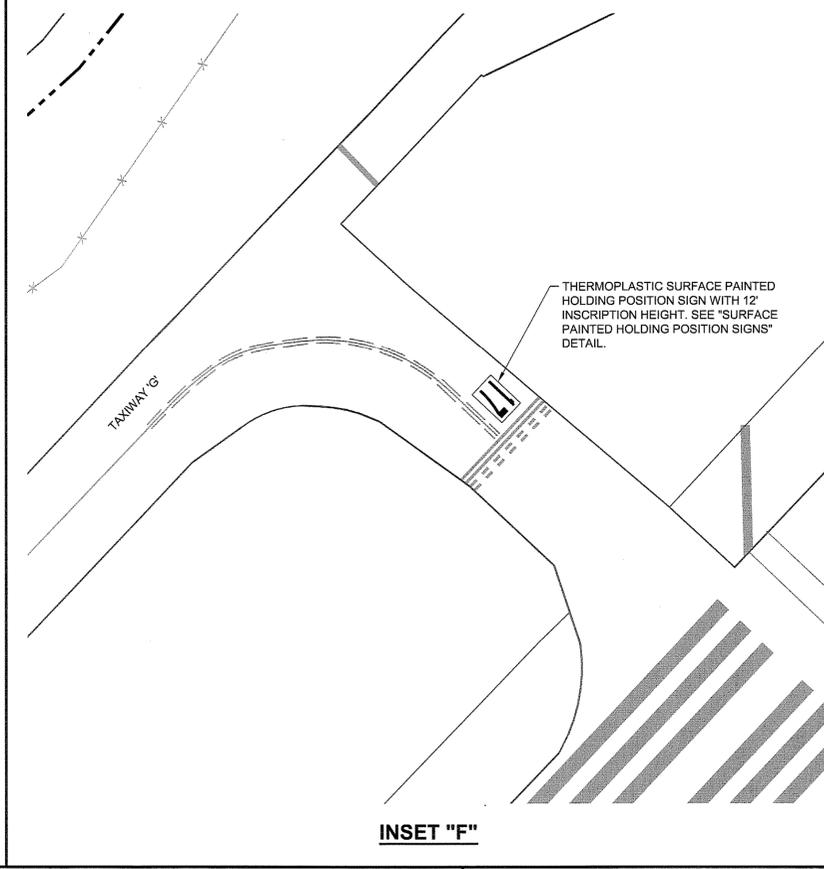
INSET "C"



INSET "D"



INSET "E"



INSET "F"

NO.	REVISIONS	BY	APP.	DATE

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AIRFIELD PAVEMENT REHABILITATION-PHASE 2		AIP NO. 3-51-0029-036 (PENDING)	JOB NO. 12121
SURFACE PAINTED HOLD SIGN REPLACEMENT (NON-AIP)		DRAWN BY: DWS	SHEET 40
LYNCHBURG REGIONAL AIRPORT		DESIGNED BY: TJS	OF 69
SCALE: 1" = 30'	DATE: MAY 2013		

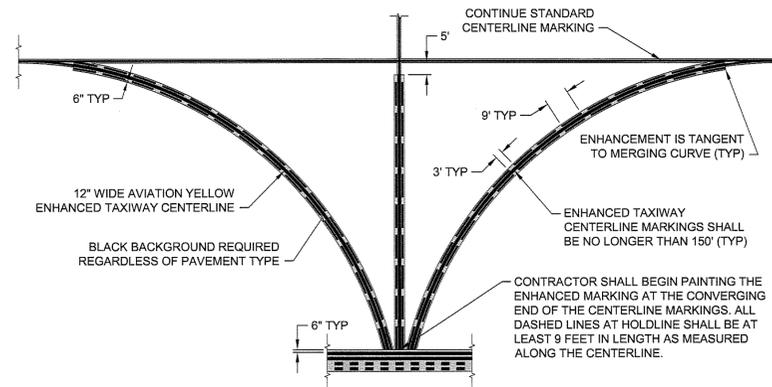
R:\12121\04 CAD\12121.mxd.dwg, L1_5/20/2013 10:59:14 AM, dwg
 XREFS: IMAGES:
 DRAWING: 4/25/12/12121.mxd.dwg LAYOUT: L1

MARKING NOTES:

- ALL MARKINGS SHALL BE IN ACCORDANCE WITH FAA ADVISORY CIRCULAR (AC) 150/5340-1K, "STANDARDS FOR AIRPORT MARKINGS".
- ANY MARKINGS APPLIED ON NEW BITUMINOUS PAVEMENT PRIOR TO THE THIRTY (30) DAY PAVEMENT CURE TIME SHALL BE APPLIED AT 33 PERCENT OF THE SPECIFIED COVERAGE (345 SF/GAL) AND PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE FOOT FOR "INITIAL RUNWAY AND TAXIWAY PAINTING". REFERENCE IS DIRECTED TO ITEM P-620 OF THE SPECIFICATIONS.
- INITIAL MARKINGS SHALL BE APPLIED ON NEW PAVEMENT PRIOR TO OPENING OF PAVEMENT FOR TRAFFIC.
- INITIAL MARKINGS SHALL INCLUDE ALL MARKINGS SHOWN ON THE "MARKING LAYOUT" OR AS COORDINATED WITH THE ENGINEER.
- AFTER PROPER PAVEMENT CURE, ALL MARKINGS ON THE "MARKING LAYOUT" SHALL BE MARKED AT THE FULL RATE (115 SF/GAL) AND PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE FOOT FOR "PERMANENT RUNWAY AND TAXIWAY PAINTING". REFERENCE IS DIRECTED TO ITEM P-620 OF THE SPECIFICATIONS.
- MULTIPLE COATS MAY BE REQUIRED TO OBTAIN THE REQUIRED APPLICATION RATES SPECIFIED IN ITEM P-620.
- GLASS BEADS/REFLECTIVE MEDIA SHALL NOT BE USED IN INITIAL OR BLACK MARKINGS. ALL OTHER MARKINGS SHALL REQUIRE REFLECTIVE MEDIA AS OUTLINED IN ITEM P-620.
- WHERE RUNWAY AND TAXIWAY MARKINGS INTERSECT, RUNWAY MARKINGS SHALL TAKE PRECEDENCE.
- RUNWAY HOLDING POSITION MARKINGS SHALL BE ALIGNED WITH THE HOLDING POSITION SIGNS.
- THE PAVEMENT SHALL BE THOROUGHLY CLEANED PRIOR TO THE APPLICATION OF THE MARKINGS. THE CLEANING SHALL BE ACCOMPLISHED WITH HIGH PRESSURE WATER; THE USE OF CHEMICALS WILL NOT BE PERMITTED.
- ALL STATIONS AND OFFSETS ARE APPROXIMATE AND SHALL BE CONFIRMED IN THE FIELD BY THE CONTRACTOR.
- ALL MARKINGS SHALL BE FIELD MARKED BY THE CONTRACTOR FOR ACCEPTANCE BY THE ENGINEER PRIOR TO PLACEMENT.
- THE CONTRACTOR SHALL PLACE TEMPORARY TAXIWAY MARKINGS AS REQUIRED BY THE CONSTRUCTION PHASING OR AS ORDERED BY THE ENGINEER OR OWNER. PAYMENT FOR THIS WORK SHALL BE MADE AT THE CONTRACT UNIT PRICE PER SQUARE FOOT FOR ITEM P-620 "INITIAL RUNWAY AND TAXIWAY PAINTING".

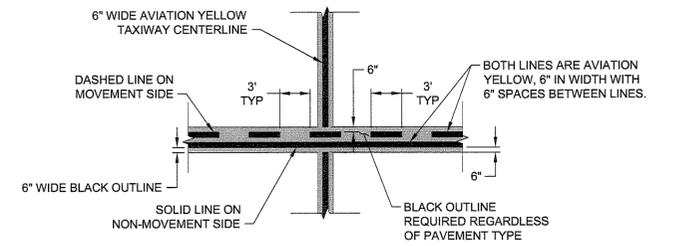
MARKING REMOVAL NOTES:

- PAINT REMOVAL SHALL BE ACCOMPLISHED WITH EQUIPMENT AND IN A MANNER THAT WILL NOT DAMAGE THE PAVEMENT. APPROVED METHODS INCLUDE GRINDING, HIGH PRESSURE WATER, OR SAND BLASTING. THE USE OF SHOT BLASTING OR CHEMICALS WILL NOT BE PERMITTED.
- THE CONTRACTOR SHALL PROVIDE CERTIFICATION THAT THE CONTRACTOR'S EQUIPMENT HAS BEEN USED IN THE PERFORMANCE OF A SIMILAR CONTRACT.
- PAINT REMOVAL SHALL BE DEFINED AS THE REMOVAL OF AT LEAST 95-100 PERCENT OF THE EXISTING MARKING AS DETERMINED BY THE ENGINEER'S VISUAL INSPECTION. IN ADDITION, THE REMOVAL LEVEL IS DEFINED SUCH THAT THERE WILL NOT BE ANY REMAINING SURFACE OF UNDISTURBED PAINT OR AREAS LARGER THAN ONE (1) SQUARE INCH WHERE THE PAVEMENT IS NOT CLEARLY EXPOSED.
- THE METHOD USED FOR PAINT REMOVAL SHALL NOT MATERIALLY DAMAGE THE PAVEMENT. ANY DAMAGE CAUSED BY THE CONTRACTOR'S OPERATIONS SHALL BE CORRECTED AT THE CONTRACTOR'S EXPENSE AND IN A MANNER APPROVED BY THE ENGINEER.
- ACCUMULATIONS OF SAND, WATER, DUST, OR OTHER RESIDUE RESULTING FROM THE PAINT REMOVAL SHALL BE REMOVED AS THE WORK PROGRESSES.
- PRIOR TO THE START OF WORK, PAVEMENT MARKINGS SHALL BE REMOVED FROM A DESIGNATED TEST SECTION, NOT LESS THAN 50 SQUARE YARDS IN SIZE. THE METHOD AND EQUIPMENT USED FOR THE TEST SECTION SHALL BE THE SAME AS THAT INTENDED FOR THE REMAINDER OF THE WORK. THE TEST SECTION SHALL BE INSPECTED AND APPROVED BY THE ENGINEER PRIOR TO BEGINNING ANY FURTHER PAINT REMOVAL.

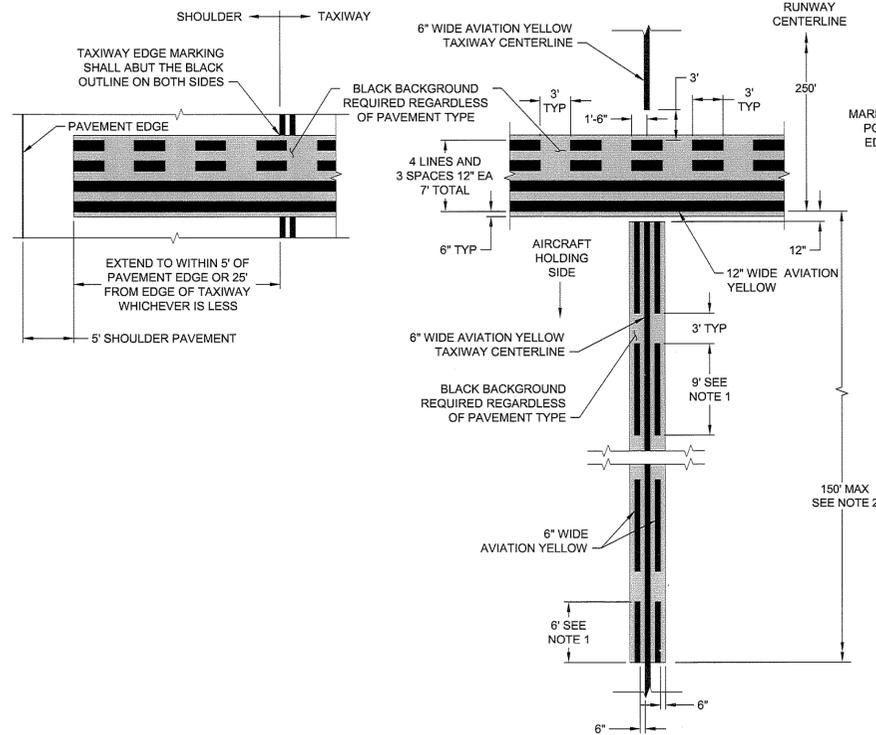


ENHANCED TAXIWAY CENTERLINES AT TAXIWAY INTERSECTION LESS THAN 150'

- NOTES:
- ENHANCED CENTERLINES SHALL STOP 5' (PERPENDICULAR DISTANCE) FROM THE INTERSECTING TAXIWAY CENTERLINE.
 - 6" WIDE BLACK OUTLINES ARE REQUIRED ON ENHANCED CENTERLINE MARKINGS.

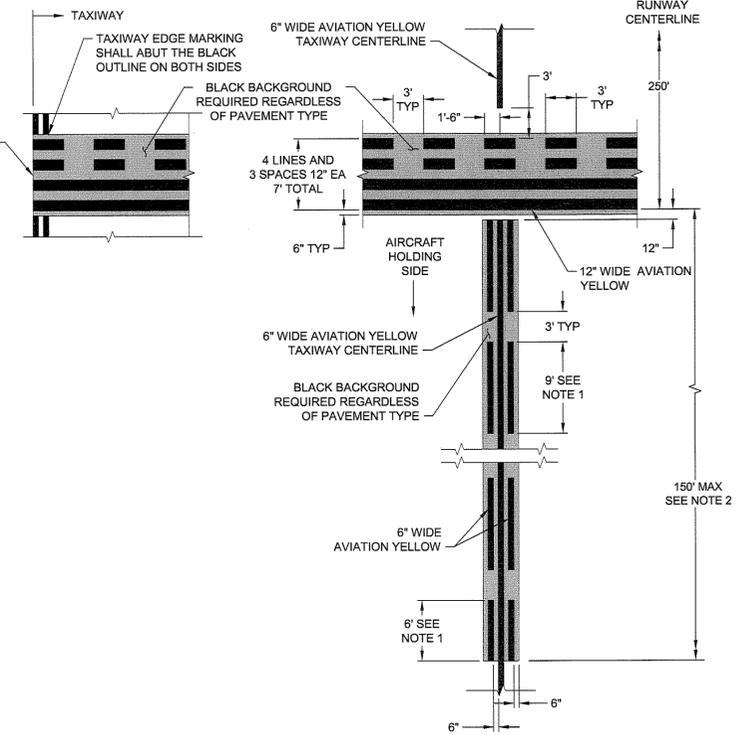


6" NON-MOVEMENT AREA BOUNDARY MARKING WITH 6" TAXIWAY CENTERLINE



ENHANCED RUNWAY HOLDING POSITION MARKING WITH 6" TAXIWAY CENTERLINE (WITH PAVED SHOULDER)

- NOTES:
- 9 FEET AND 6 FEET DIMENSION IS ALONG TAXIWAY CENTERLINE. IN SKEWED CONDITIONS, DIMENSION ON CENTER WILL BE THAT SHOWN. DIMENSION OF 6 INCHES WIDE DASHED ENHANCEMENT LINE WILL BE LONGER OR SHORTER THAN 9 FEET DEPENDING ON SIDE OF SKEW. DASHED LINE SHALL END AT SAME LOCATION RELATIVE TO THE CENTERLINE.
 - IF THE TAXIWAY CENTERLINE TO BE ENHANCED CONTINUES THROUGH A TAXIWAY/TAXIWAY INTERSECTION THAT IS LOCATED WITHIN 150 FEET OF A RUNWAY HOLDING POSITION MARKING, THE TAXIWAY CENTERLINE ENHANCEMENT MUST BE TERMINATED 5 FEET PRIOR TO THE POINT WHERE THE OTHER TAXIWAY CENTERLINE CROSSES THE ENHANCED TAXIWAY CENTERLINE.
 - IF A LIGHT FIXTURE OR SIGN IS LOCATED ON THE TAXIWAY SHOULDER AND ALIGNED WITH THE EXTENDED HOLDING POSITION MARKING, THE EXTENDED HOLDING POSITION MARKING SHOULD BE EXTENDED NO CLOSER THAN 5 FEET TO THE EDGE OF THE LIGHT OR SIGN.

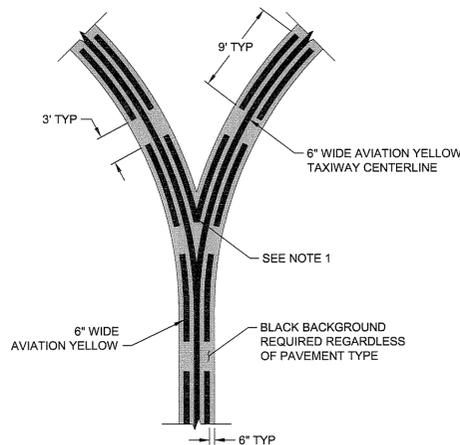


ENHANCED RUNWAY HOLDING POSITION MARKING WITH 6" TAXIWAY CENTERLINE (WITHOUT PAVED SHOULDER)

- NOTES:
- 9 FEET AND 6 FEET DIMENSION IS ALONG TAXIWAY CENTERLINE. IN SKEWED CONDITIONS, DIMENSION ON CENTER WILL BE THAT SHOWN. DIMENSION OF 6 INCHES WIDE DASHED ENHANCEMENT LINE WILL BE LONGER OR SHORTER THAN 9 FEET DEPENDING ON SIDE OF SKEW. DASHED LINE SHALL END AT SAME LOCATION RELATIVE TO THE CENTERLINE.
 - IF THE TAXIWAY CENTERLINE TO BE ENHANCED CONTINUES THROUGH A TAXIWAY/TAXIWAY INTERSECTION THAT IS LOCATED WITHIN 150 FEET OF A RUNWAY HOLDING POSITION MARKING, THE TAXIWAY CENTERLINE ENHANCEMENT MUST BE TERMINATED 5 FEET PRIOR TO THE POINT WHERE THE OTHER TAXIWAY CENTERLINE CROSSES THE ENHANCED TAXIWAY CENTERLINE.
 - IF A LIGHT FIXTURE OR SIGN IS LOCATED ON THE TAXIWAY SHOULDER AND ALIGNED WITH THE EXTENDED HOLDING POSITION MARKING, THE EXTENDED HOLDING POSITION MARKING SHOULD BE EXTENDED NO CLOSER THAN 5 FEET TO THE EDGE OF THE LIGHT OR SIGN.

ENHANCED TAXIWAY W/ 6" CENTERLINE AT CONVERGING TAXIWAYS

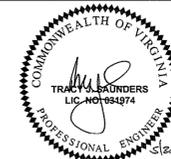
- NOTE:
- AS SHOWN IN THIS CASE, THE V-SHAPED INNER DASHES START AND STOP WITH THE OUTSIDE 9 FOOT DASHES; HOWEVER, THIS MAY NOT ALWAYS BE THE CASE FOR THE INNER DASHES. IF THE V-SHAPES ARE LESS THAN 5 FEET THEY MAY BE OMITTED.



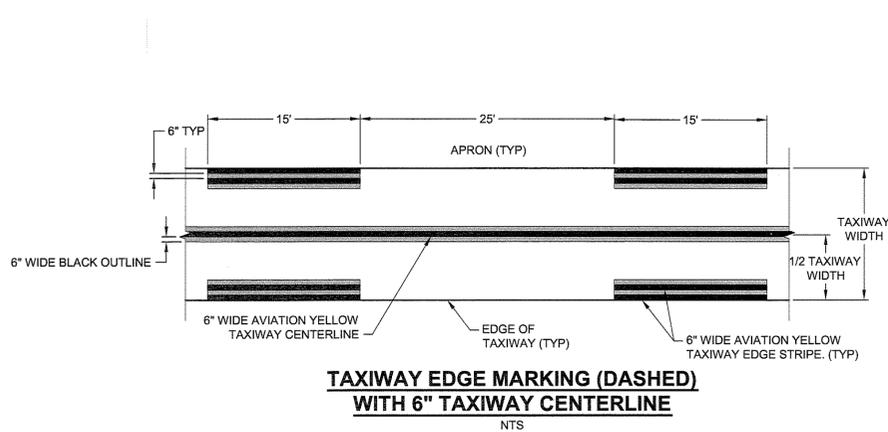
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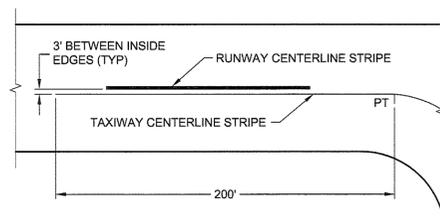


AIRFIELD PAVEMENT REHABILITATION-PHASE 2		AIP NO. 3-51-0029-036 (PENDING)	JOB NO. 12121
MARKING NOTES & DETAILS		DRAWN BY: DWS	SHEET 41
LYNCHBURG REGIONAL AIRPORT		DESIGNED BY: TJS	OF 69
SCALE: NONE	DATE: MAY 2013		



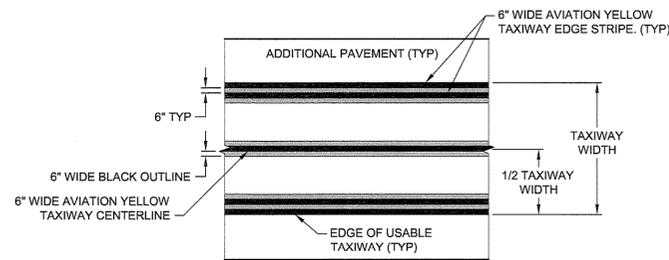
**TAXIWAY EDGE MARKING (DASHED)
WITH 6" TAXIWAY CENTERLINE**

NTS



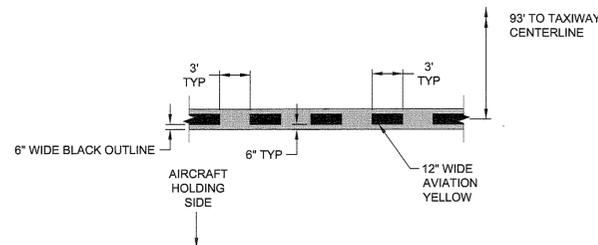
TAXIWAY LEAD-IN DETAIL

NTS



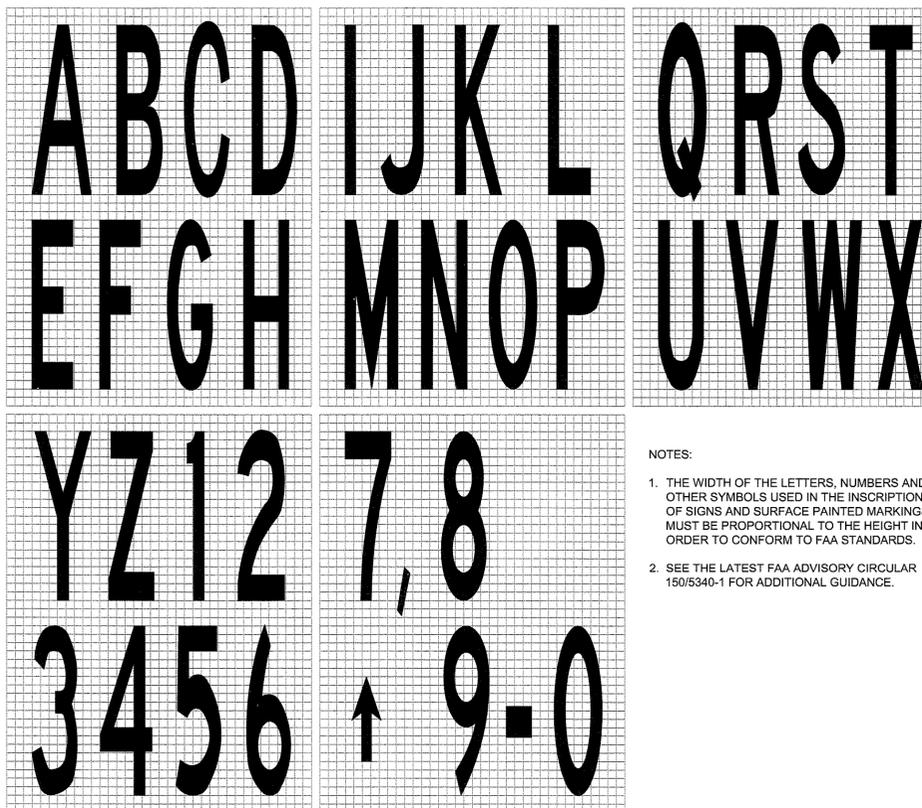
**TAXIWAY EDGE MARKING (CONTINUOUS)
WITH 6" TAXIWAY CENTERLINE**

NTS



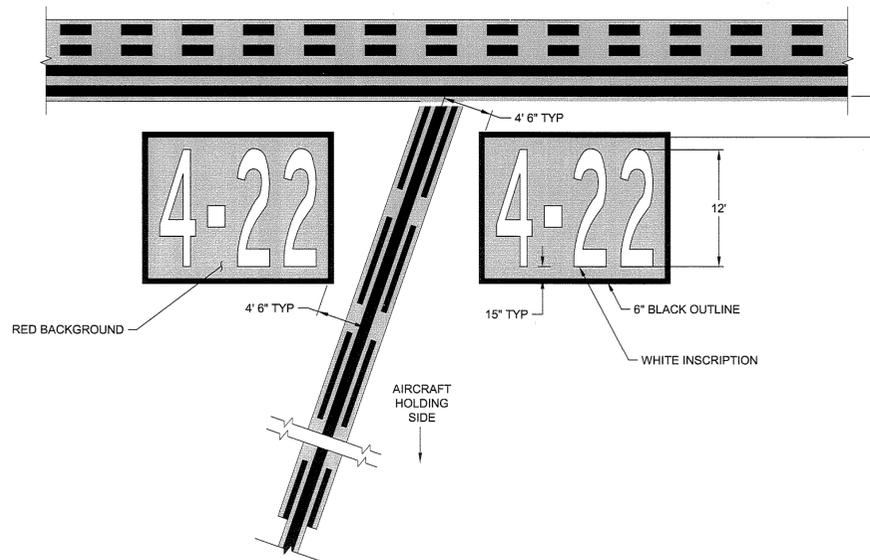
INTERMEDIATE HOLDING POSITION MARKING

NTS



SURFACE PAINTED MARKINGS DETAIL

NTS

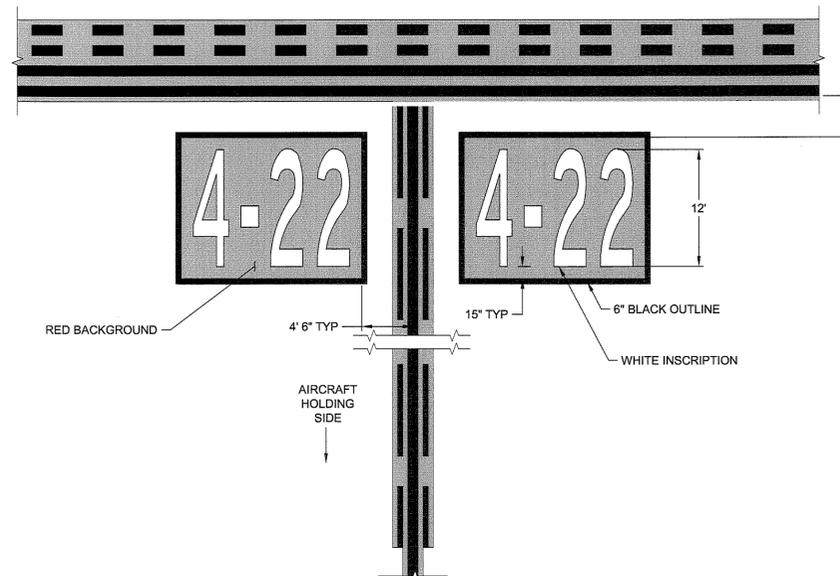


SURFACE PAINTED HOLDING POSITION SIGNS - SKEWED

NTS

NOTES:

1. THIS DETAIL APPLIES ONLY TO SITUATIONS WHERE THE TAXIWAY CENTERLINE AND RUNWAY HOLDING POSITION MARKING ARE NOT PERPENDICULAR.
2. SEE "SURFACE PAINTED MARKINGS DETAIL" FOR INSCRIPTION PROPORTIONAL SIZING.
3. THE SURFACE PAINTED HOLDING POSITION SIGN SHALL BE AT LEAST 2 FEET FROM THE TAXIWAY EDGE MARKING OR TAXIWAY EDGE IF NOT MARKED.
4. STENCIL BRACES MUST BE FILLED IN. ALL INDIVIDUAL CHARACTERS MUST BE PAINTED CONTIGUOUS.

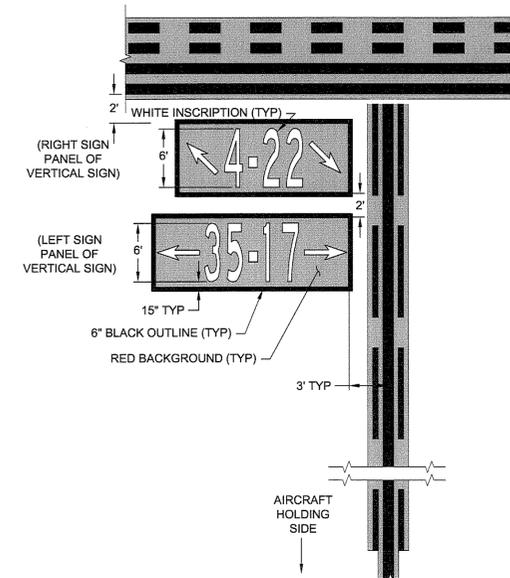


SURFACE PAINTED HOLDING POSITION SIGNS

NTS

NOTES:

1. THIS DETAIL APPLIES ONLY TO SITUATIONS WHERE THE TAXIWAY CENTERLINE AND RUNWAY HOLDING POSITION MARKING ARE PERPENDICULAR.
2. SEE "SURFACE PAINTED MARKINGS DETAIL" FOR INSCRIPTION PROPORTIONAL SIZING.
3. THE SURFACE PAINTED HOLDING POSITION SIGN SHALL BE AT LEAST 2 FEET FROM THE TAXIWAY EDGE MARKING OR TAXIWAY EDGE IF NOT MARKED.
4. STENCIL BRACES MUST BE FILLED IN. ALL INDIVIDUAL CHARACTERS MUST BE PAINTED CONTIGUOUS.



SURFACE PAINTED HOLDING POSITION SIGNS - STACKED

NTS

NOTES:

1. THIS DETAIL APPLIES ONLY TO SITUATIONS WHERE THE TAXIWAY CENTERLINE AND RUNWAY HOLDING POSITION MARKING ARE PERPENDICULAR.
2. SEE "SURFACE PAINTED MARKINGS DETAIL" FOR INSCRIPTION PROPORTIONAL SIZING.
3. THE SURFACE PAINTED HOLDING POSITION SIGN SHALL BE AT LEAST 2 FEET FROM THE TAXIWAY EDGE MARKING OR TAXIWAY EDGE IF NOT MARKED.
4. STENCIL BRACES MUST BE FILLED IN. ALL INDIVIDUAL CHARACTERS MUST BE PAINTED CONTIGUOUS.

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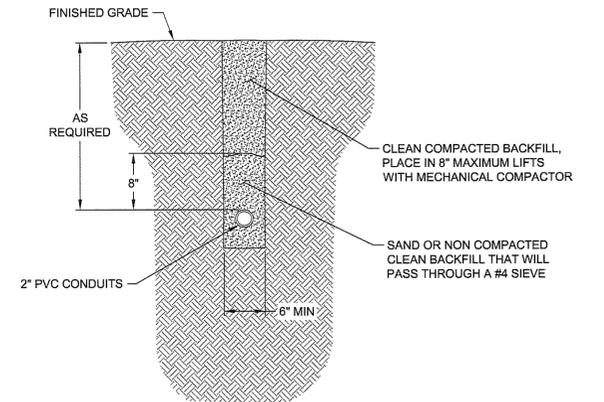


AIRFIELD PAVEMENT REHABILITATION-PHASE 2		AIP NO. 3-51-0029-036 (PENDING)	JOB NO. 12121
MARKING DETAILS		DRAWN BY: LKH	SHEET 42
		DESIGNED BY: DWS	OF 69
LYNCHBURG REGIONAL AIRPORT		SCALE: NONE	DATE: MAY 2013

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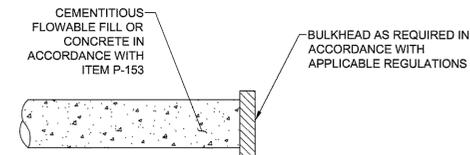
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NO.	REVISIONS	BY	APP.	DATE



DRAINAGE CONDUIT DETAIL

NTS

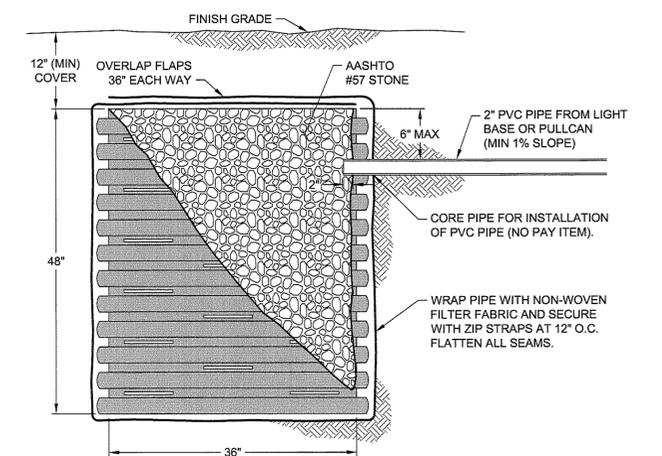


ABANDONED PIPE DETAIL (FILLED)

NTS

NOTES:

1. FILLING OF ABANDONED PIPES SHALL BE CONSIDERED INCIDENTAL TO ITEM P-150 "MISCELLANEOUS AND ELECTRICAL DEMOLITION".
2. ALL ABANDONED PIPES SHALL BE FILLED FOR THEIR ENTIRE LENGTH WITH CEMENTITIOUS FLOWABLE FILL OR CONCRETE, UNLESS OTHERWISE NOTED. NO ABANDONED PIPE SHALL BE LEFT ONLY PARTIALLY FILLED, UNLESS OTHERWISE NOTED.
3. CEMENTITIOUS FLOWABLE FILL SHALL BE IN ACCORDANCE WITH ITEM P-153 "CONTROLLED LOW-STRENGTH MATERIAL (CLSM)".

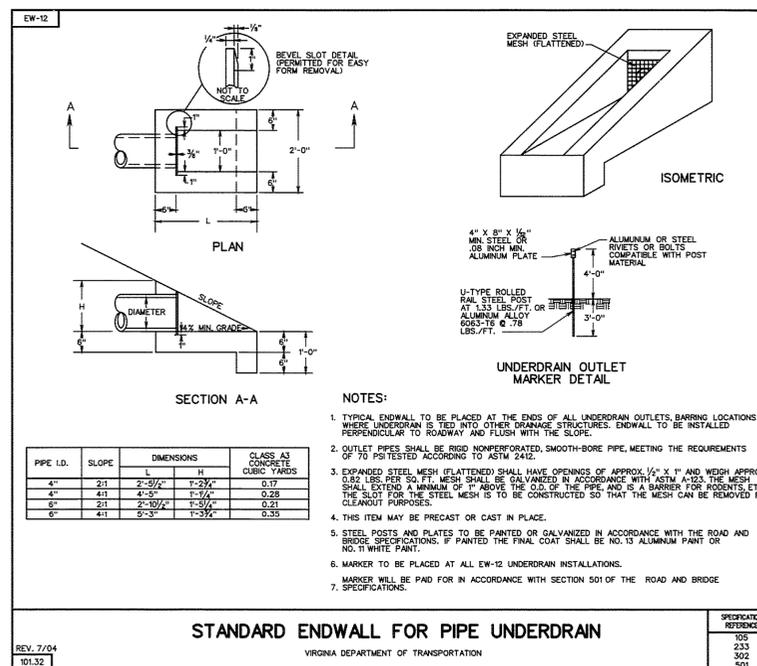


CPP DRAINAGE SUMP DETAIL

NTS

NOTES:

1. MECHANICALLY AUGER HOLE FOR PLACEMENT OF SUMP.
2. CPP SHALL BE PERFORMED ADS N-12 OR APPROVED EQUIVALENT.
3. FILTER FABRIC SHALL BE 8 OZ. NON-WOVEN FABRIC.
4. INSTALLATION OF THE DRAINAGE SUMPS ARE INCIDENTAL TO THE COST OF THE LIGHT BASES, PULLCANS, PULLCAN PLAZAS, AND HANDHOLES.



SECTION A-A

ISOMETRIC

UNDERDRAIN OUTLET MARKER DETAIL

NOTES:

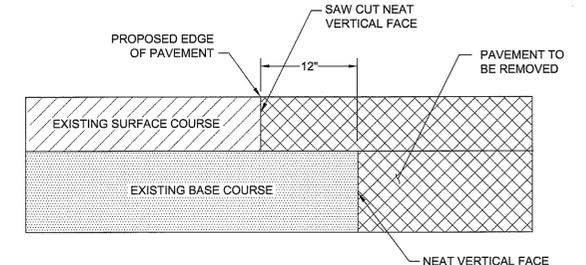
1. TYPICAL ENDWALL TO BE PLACED AT THE ENDS OF ALL UNDERDRAIN OUTLETS, BARRING LOCATIONS WHERE UNDERDRAIN IS TIED INTO OTHER DRAINAGE STRUCTURES. ENDWALL TO BE INSTALLED PERPENDICULAR TO ROADWAY AND FLUSH WITH THE SLOPE.
2. OUTLET PIPES SHALL BE RIGID NONPERFORATED, SMOOTH-BORE PIPE, MEETING THE REQUIREMENTS OF 70 POSTESTED ACCORDING TO ASTM 2442.
3. EXPANDED STEEL MESH (FLATTENED) SHALL HAVE OPENINGS OF APPROX. 1/2" x 1" AND WIDTH APPROX. 0.82 LBS. PER SQ. FT. MESH SHALL BE GALVANIZED IN ACCORDANCE WITH ASTM A-123. THE MESH SHALL EXTEND A MINIMUM OF 1" ABOVE THE O.D. OF THE PIPE AND IS A BARRIER FOR RODENTS, ETC. THE SLOT FOR THE STEEL MESH IS TO BE CONSTRUCTED SO THAT THE MESH CAN BE REMOVED FOR CLEANOUT PURPOSES.
4. THIS ITEM MAY BE PRECAST OR CAST IN PLACE.
5. STEEL POSTS AND PLATES TO BE PAINTED OR GALVANIZED IN ACCORDANCE WITH THE ROAD AND BRIDGE SPECIFICATIONS. IF PAINTED THE FINAL COAT SHALL BE NO. 13 ALUMINUM PAINT OR NO. 11 WHITE PAINT.
6. MARKER TO BE PLACED AT ALL EW-12 UNDERDRAIN INSTALLATIONS.
7. MARKER WILL BE PAID FOR IN ACCORDANCE WITH SECTION 501 OF THE ROAD AND BRIDGE SPECIFICATIONS.

PIPE I.D.	SLOPE	DIMENSIONS		CLASS A3 CONCRETE CUBIC YARDS
		L	H	
4"	2:1	2'-9 1/2"	1'-2 3/4"	0.17
4"	4:1	4'-5"	1'-3/4"	0.28
6"	2:1	2'-9 1/2"	1'-3/4"	0.21
6"	4:1	5'-2"	1'-3/4"	0.35

STANDARD ENDWALL FOR PIPE UNDERDRAIN

VIRGINIA DEPARTMENT OF TRANSPORTATION

REVISION	DATE	BY	APP.	SPECIFICATION REFERENCE
REV. 7/04	10/32			105 233 302 501



PAVEMENT REMOVAL DETAIL

NTS

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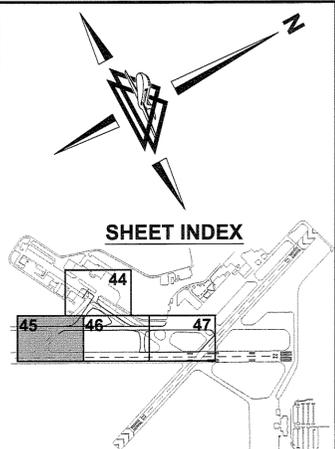
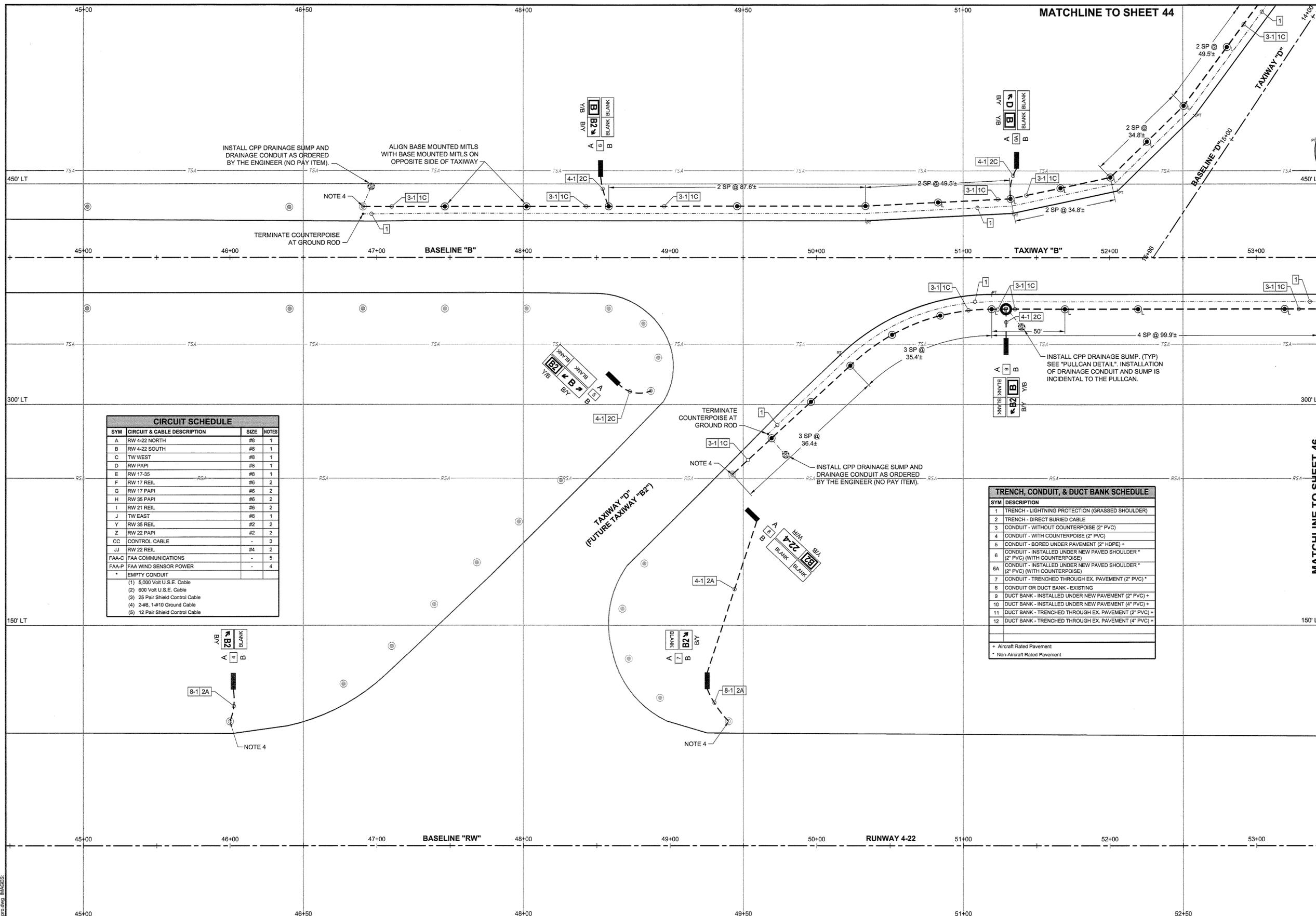


AIRFIELD PAVEMENT REHABILITATION-PHASE 2		AIP NO. 3-51-0029-036 (PENDING)	JOB NO. 12121
MISCELLANEOUS DETAILS		DRAWN BY: DWS	SHEET 43
LYNCHBURG REGIONAL AIRPORT		DESIGNED BY: TJS	OF 69
SCALE: NONE	DATE: MAY 2013		

NO.	REVISIONS	BY	APP.	DATE

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DRAWING: 02/01/med.dwg LAYOUT: L1
XREFS: BACKS



TRENCH, CONDUIT & DUCT BANK LABEL LEGEND

TRENCH, CONDUIT, OR DUCT BANK SYMBOL: 5-3 | 2A | 4B | 3D-#6g, 2C-#4g

INDIVIDUAL CONDUITS: GND WIRE SIZE, NUMBER OF CABLES

SIGN/LIGHT COLOR LEGEND

W - WHITE, R - RED, B - BLACK, Y - YELLOW, O - OBSCURED, G - GREEN, C - CLEAR

CHARACTER OR LENS COLOR, W/R, BACKGROUND OR LENS COLOR

ELECTRICAL SYMBOL LEGEND

SYMBOL STATUS

EXISTING	REMOVE	NEW	RELOCATE	ADJUST
○	⊗	●	⊙	⊕

SYMBOL DESCRIPTION

W	L-850C IN-PAVEMENT HIRL
⊗	L-852T IN-PAVEMENT MITL
⊗	L-853 TYPE II RETROREFLECTIVE MARKER
⊗	L-858 AIRFIELD GUIDANCE SIGN (LED)
⊗	L-861 BASE MOUNTED MRL
⊗	L-861T BASE MOUNTED MITL WITH RELOCATED FIXTURE
⊗	L-861(L) BASE MOUNTED MITL (LED)
⊗	L-861(T) LED FIXTURE ON EXISTING BASE
⊗	L-862 BASE MOUNTED HIRL
⊗	L-862E BASE MOUNTED THRESHOLD LIGHT
⊗	L-867 ELECTRICAL PULLCAN (# INDICATES NUMBER OF CANS)
⊗	ELECTRICAL HANDHOLE
⊗	ELECTRICAL JUNCTION BOX
⊗	ELECTRICAL MANHOLE
⊗	L-804 ELEVATED RUNWAY GUARD LIGHT
⊗	COUNTERPOISE GROUND ROD
⊗	DRAINAGE SUMP
⊗	EW-12 ENDWALL
---	CONDUIT & CABLE
---	FLOWED COUNTERPOISE
---	DUCT BANK - BURIED UNDER PAVEMENT (CONCRETE ENCASED)
---	DUCT BANK - BORED UNDER PAVEMENT
---	DRAINAGE CONDUIT

NOTES

- ALL STATIONS AND OFFSETS GIVEN WITH RESPECT TO BASELINE "RW" (RUNWAY 4-22 CENTERLINE) UNLESS OTHERWISE NOTED.
- REFER TO "DEMOLITION PLAN" SHEETS FOR REMOVAL OF EXISTING ELECTRICAL ITEMS.
- EXTEND DUCT BANK 5 FEET PAST EDGE OF PAVEMENT.
- TIE INTO EXISTING CIRCUIT AS ORDERED BY THE ENGINEER (NO PAY ITEM).
- INSTALL PULLCAN PLAZAS 10 FEET FROM EDGE OF PAVED SHOULDER AND 15 FEET FROM EDGE OF PAVEMENT UNLESS OTHERWISE NOTED.

SCALE: 1"=30'

CIRCUIT SCHEDULE

SYM	CIRCUIT & CABLE DESCRIPTION	SIZE	NOTES
A	RW 4-22 NORTH	#8	1
B	RW 4-22 SOUTH	#8	1
C	TW WEST	#8	1
D	RW PAPI	#8	1
E	RW 17-35	#8	1
F	RW 17 REIL	#8	2
G	RW 17 PAPI	#8	2
H	RW 35 PAPI	#8	2
I	RW 21 REIL	#8	2
J	TW EAST	#8	1
Y	RW 35 REIL	#2	2
Z	RW 22 PAPI	#2	2
CC	CONTROL CABLE	-	3
JJ	RW 22 REIL	#4	2
FAA-C	FAA COMMUNICATIONS	-	5
FAA-P	FAA WIND SENSOR POWER	-	4
*	EMPTY CONDUIT	-	-
(1)	5,000 Volt U.S.E. Cable	-	-
(2)	600 Volt U.S.E. Cable	-	-
(3)	25 Pair Shield Control Cable	-	-
(4)	2-#8, 1-#10 Ground Cable	-	-
(5)	12 Pair Shield Control Cable	-	-

TRENCH, CONDUIT, & DUCT BANK SCHEDULE

SYM	DESCRIPTION
1	TRENCH - LIGHTNING PROTECTION (GRASSED SHOULDER)
2	TRENCH - DIRECT BURIED CABLE
3	CONDUIT - WITHOUT COUNTERPOISE (2" PVC)
4	CONDUIT - WITH COUNTERPOISE (2" PVC)
5	CONDUIT - BORED UNDER PAVEMENT (2" HDPE) +
6	CONDUIT - INSTALLED UNDER NEW PAVED SHOULDER * (2" PVC) (WITH COUNTERPOISE)
6A	CONDUIT - INSTALLED UNDER NEW PAVED SHOULDER * (2" PVC) (WITH COUNTERPOISE)
7	CONDUIT - TRENCHED THROUGH EX. PAVEMENT (2" PVC) *
8	CONDUIT OR DUCT BANK - EXISTING
9	DUCT BANK - INSTALLED UNDER NEW PAVEMENT (2" PVC) +
10	DUCT BANK - INSTALLED UNDER NEW PAVEMENT (4" PVC) +
11	DUCT BANK - TRENCHED THROUGH EX. PAVEMENT (2" PVC) +
12	DUCT BANK - TRENCHED THROUGH EX. PAVEMENT (4" PVC) +

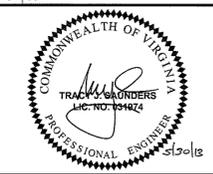
* Aircraft Rated Pavement
+ Non-Aircraft Rated Pavement

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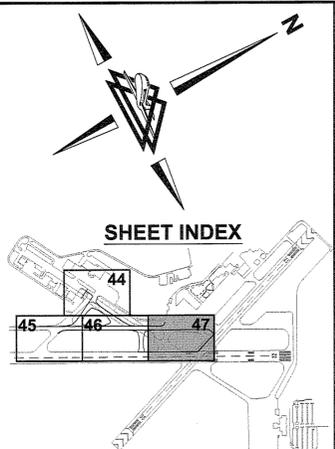
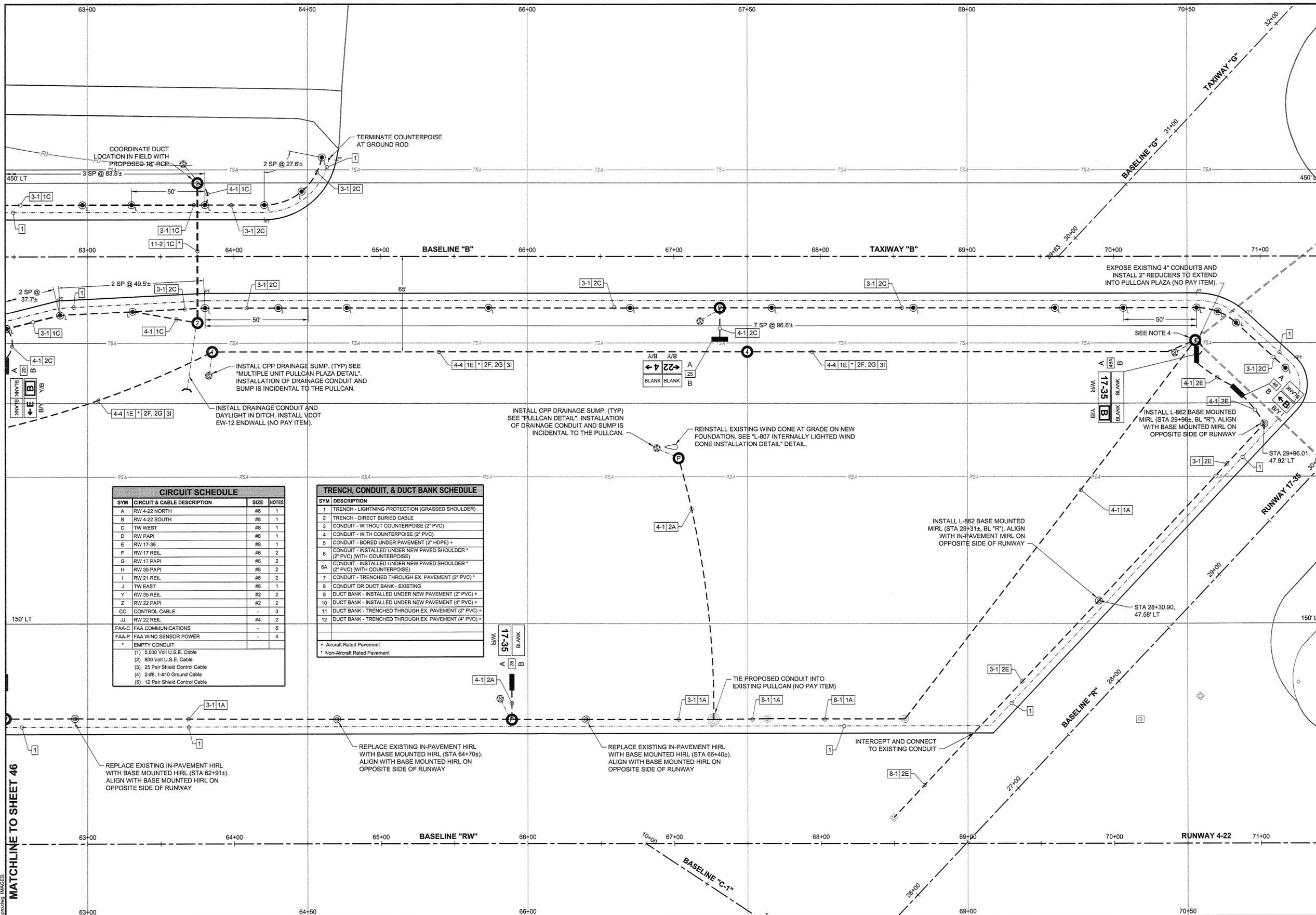
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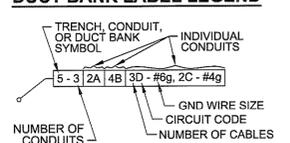
AIRFIELD PAVEMENT REHABILITATION-PHASE 2		AIP NO. 3-51-0029-036 (PENDING)	JOB NO. 12121
ELECTRICAL LAYOUT		DRAWN BY: DWS	SHEET 45
		DESIGNED BY: TJS	OF 69
LYNCHBURG REGIONAL AIRPORT		SCALE: 1" = 30'	DATE: MAY 2013

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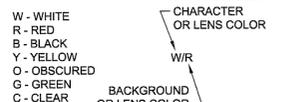
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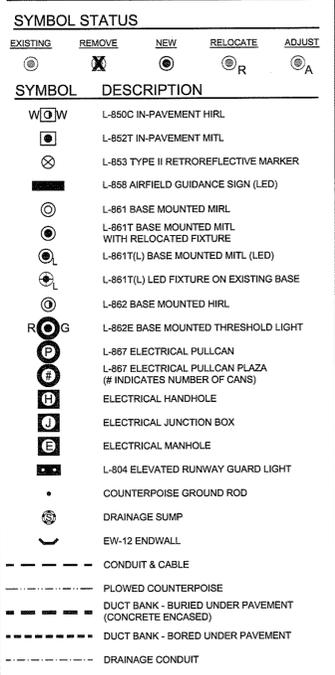
TRENCH, CONDUIT & DUCT BANK LABEL LEGEND



SIGN/LIGHT COLOR LEGEND



ELECTRICAL SYMBOL LEGEND



- NOTES**
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11	DUCT BANK - TRENCHED THROUGH EX. PAVEMENT (2" PVC) +
12	DUCT BANK - TRENCHED THROUGH EX. PAVEMENT (4" PVC) +

+ Aircraft Rated Pavement
 * Non-Aircraft Rated Pavement

MATCHLINE TO SHEET 46

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AIRFIELD PAVEMENT REHABILITATION-PHASE 2		AIP NO. 3-51-0029-036 (PENDING)	JOB NO. 12121
ELECTRICAL LAYOUT		DRAWN BY: DWS	SHEET 47
LYNCHBURG REGIONAL AIRPORT		DESIGNED BY: TJS	OF 69
SCALE: 1" = 30'	DATE: MAY 2013		

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ELECTRICAL NOTES:

UTILITIES

- THE CONTRACTOR SHALL COORDINATE (AT LEAST 48 HOURS IN ADVANCE) INTERRUPTION OF SERVICE TO ACTIVE LIGHTING CIRCUITS WITH THE OWNER. ANY DAMAGE TO EXISTING AIRPORT CIRCUITS CAUSED BY THE CONTRACTOR'S EQUIPMENT OR PERSONNEL SHALL BE PROMPTLY REPAIRED, BY THE CONTRACTOR, AT THE CONTRACTOR'S EXPENSE. ALL ACTIVE LIGHTING SYSTEMS FOR OPEN AIRCRAFT OPERATIONAL AREAS SHALL REMAIN READY FOR OPERATION DURING IFR WEATHER CONDITIONS AND DUSK TO DAWN.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR LOCATING ALL EXISTING UTILITIES PRIOR TO THE BEGINNING OF CONSTRUCTION.
- THE CONTRACTOR SHALL LOCATE ALL UTILITIES DURING CONSTRUCTION, AND HAND DIG WHEN WITHIN THREE (3) FEET OF ANY KNOWN OR SUSPECTED UNDERGROUND UTILITY.
- THE LOCATION OF ALL UTILITIES SHOWN IS APPROXIMATE ONLY AND SHALL BE FIELD VERIFIED BY THE CONTRACTOR BEFORE BEGINNING CONSTRUCTION. NOT ALL UTILITIES ARE NECESSARILY SHOWN.
- THE CONTRACTOR SHALL HAVE A TONE GENERATOR TYPE CABLE TRACER, TO LOCATE EXISTING CABLES, ON SITE AT ALL TIMES.
- THE CONTRACTOR SHALL MANUALLY LOCK OUT EACH CIRCUIT AT THE VAULT WHEN WORK IS BEING PERFORMED ON THE CIRCUIT. THE CIRCUIT SHALL BE TAGGED AND THE CONTRACTOR'S NAME SHALL BE CLEARLY IDENTIFIED ON EACH TAG. THE CONTRACTOR SHALL HAVE A LOCK OUT KIT ON SITE AT ALL TIMES. THE OWNER'S REPRESENTATIVE SHALL BE NOTIFIED EACH TIME A CIRCUIT IS SECURED AND EACH TIME THE CIRCUIT IS RETURNED TO OPERATIONAL STATUS. THE CONTRACTOR MUST BE CERTIFIED AND TRAINED BY THE OWNER PRIOR TO PERFORMING WORK OR LOCK-OUT PROCEDURES IN THE ELECTRICAL VAULT.

PRIOR TO CONSTRUCTION

- PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL INVENTORY ALL LIGHTS, FIXTURES, SIGNS, ETC. WITHIN THE PROJECT LIMITS. THE CONTRACTOR SHALL ADVISE THE ENGINEER, IN WRITING, OF ANY DAMAGED LIGHT FIXTURES, SIGNS, OR UTILITIES PRIOR TO THE BEGINNING OF CONSTRUCTION.
- THE CONTRACTOR SHALL MEGGER THE EXISTING LIGHTING SYSTEMS PRIOR TO BEGINNING WORK ON THE EXISTING LIGHTING SYSTEMS. THIS WORK SHALL BE PERFORMED IN THE PRESENCE OF THE OWNER'S REPRESENTATIVE, AND LOGGED IN THE DAILY REPORT.
- PRIOR TO BEGINNING WORK ON THE PROJECT, THE CONTRACTOR SHALL FURNISH THE ENGINEER WITH TWO (2) COPIES OF ALL MANUFACTURERS' INSTALLATION INSTRUCTIONS FOR ALL EQUIPMENT (NO SEPARATE PAY ITEM).

GENERAL

- ALL ELECTRICAL WORK SHALL BE COMPLETED IN ACCORDANCE WITH LOCAL CODE AND CURRENT NEC HANDBOOK.
- THE CONTRACTOR SHALL MAINTAIN THE EXISTING TAXIWAY LIGHTING CIRCUITS FOR OPEN AIRCRAFT OPERATIONAL AREAS. THE USE OF TEMPORARY CABLES SHALL BE ANTICIPATED DUE TO CONSTRUCTION PHASING (NO PAY ITEM).
- RUNWAY AND/OR TAXIWAY LIGHTS SHALL BE DE-ENERGIZED OR PROPERLY COVERED ON ALL CLOSED SEGMENTS OF RUNWAYS(S) AND TAXIWAY(S). (NO SEPARATE PAY ITEM). REMOVING THE LAMP FROM FIXTURES SHALL NOT BE THE METHOD SELECTED.
- ALL EXISTING ELECTRICAL EQUIPMENT REMOVED SHALL REMAIN THE PROPERTY OF THE OWNER AND STORED AT THE AIRPORT AT A LOCATION AS ORDERED BY THE ENGINEER AND/OR OWNER UNLESS OTHERWISE NOTED.
- THE CONTRACTOR SHALL BE REQUIRED TO CONNECT THE PROPOSED CIRCUITS TO THE EXISTING CIRCUITS AS ORDERED BY THE ENGINEER. ALL WORK REQUIRED TO MAKE THE CONNECTIONS TO THE EXISTING CIRCUITS SHALL BE INCIDENTAL TO THE PROJECT (I.E., PUNCHING INTO EXISTING MANHOLES FOR CONDUITS).
- WHERE PROPOSED CONDUIT IS TO BE CONNECTED TO EXISTING CONDUIT, THE CONTRACTOR SHALL MAKE THE CONNECTION USING MANUFACTURED COUPLINGS (NO SEPARATE PAY ITEM).
- EXISTING CIRCUITS SHALL BE REPLACED FROM HANDHOLE TO HANDHOLE OR PULLCAN OR NEAREST LIGHT FIXTURE.
- ALL CABLES SHALL BE TAGGED AT EACH CONNECTION AND AT EACH ENTRANCE TO ALL DUCTS AND HANDHOLES AND PULL CANS.
CABLE MARKERS SHALL BE ALMETEK "MINI-TAGS" KIT WITH BLACK STAMPED YELLOW POLYETHYLENE LETTERS OR APPROVED EQUIVALENT. ATTACH MARKERS WITH CABLE TIES.
- THE CONTRACTOR SHALL LOCATE EXISTING DUCTS WHICH WILL BE UTILIZED FOR THIS PROJECT. APPROXIMATE LOCATIONS ARE SHOWN.
- THE CONTRACTOR SHALL CLEAN/SWAB OUT EXISTING DUCTS BEING USED. THE USE OF SEWER TAPE TO CLEAR OBSTRUCTIONS WITHIN CONCRETE ENCASED DUCTS SHOULD BE ANTICIPATED (NO PAY ITEM).
- THE LOCATIONS OF ALL P.T.'S SHOWN ON THE PLANS ARE APPROXIMATE AND SHALL BE FIELD VERIFIED BY THE ENGINEER.
- THE CONTRACTOR SHALL FIELD STAKE ALL LIGHTS AND AIRFIELD SIGNS, PRIOR TO INSTALLATION. ANY DISCREPANCIES IN ALIGNMENT OR LOCATION SHOULD BE RESOLVED PRIOR TO INSTALLATION. THE LOCATION OF ALL HOLD LINES SHALL BE CONFIRMED PRIOR TO INSTALLATION OF THE SIGNS. ANY SIGNS WHICH ARE STAKED IN EXISTING DRAINAGE SWALES OR DITCHES SHALL BE BROUGHT TO THE ENGINEER'S ATTENTION PRIOR TO INSTALLATION OF THE SIGN.
- ALL SIGNS SHALL BE INSTALLED PERPENDICULAR TO THE RUNWAY OR TAXIWAY CENTERLINE.
- ALL PROPOSED ELECTRICAL HANDHOLES SHALL BE PLACED AS SHOWN ON THE PLANS. THE CONTRACTOR SHALL FIELD STAKE ALL HANDHOLES PRIOR TO INSTALLATION. ANY DISCREPANCIES IN ALIGNMENT OR LOCATION SHALL BE RESOLVED PRIOR TO INSTALLATION.
- WHERE HANDHOLES ARE TO BE PLACED AT EXISTING DUCT OR CONDUIT CROSSINGS, THE DUCT SHALL BE EXTENDED TO THE NEW STRUCTURE. EXISTING CIRCUITS SHALL BE REROUTED THROUGH THE HANDHOLES.
- THE EXISTING FIXTURE I.D. NUMBERS SHALL BE RE-USED AND SHALL BE ASSIGNED BY THE ENGINEER.

MATERIALS

- ALL GROUND RODS AND OTHER UNDERGROUND GROUNDING CONNECTIONS SHALL BE EXOTHERMICALLY WELDED. EXOTHERMIC CONNECTIONS SHALL BE IN ACCORDANCE WITH MANUFACTURER'S GUIDELINES. THE PROPOSED COUNTERPOISE SYSTEM SHALL BE CONNECTED WITH THE EXISTING SYSTEM AT ALL CROSSING POINTS.
- ALL UNDERGROUND CONDUITS SHALL BE PVC, SCHEDULE 40, UNLESS OTHERWISE NOTED. ALL UNDERGROUND CONDUITS SHALL BE UL APPROVED. ALL UNDERGROUND CONDUIT BENDS SHALL BE LONG RADIUS ANGLES.

- EACH L-830 ISOLATION TRANSFORMER, L-823 CONNECTOR KIT AND 5KV, TYPE "C" CABLE INSTALLED ON THIS PROJECT AS A COMPLETE OR PARTIAL CIRCUIT SHALL BE TESTED AS AN OPERABLE CIRCUIT SYSTEM AND SHALL MEET THE REQUIREMENTS OF SECTION L-108-3.10 OF THE SPECIFICATIONS. THE INSULATION LEAKAGE OF THE L-830 ISOLATION TRANSFORMERS SHALL NOT EXCEED 0.2 MICROAMPS WHEN TESTED AT 15,000 VOLTS D.C. IN ACCORDANCE WITH FAA AC 150/5345-47. THE CONTRACTOR SHALL PROVIDE A CERTIFIED COPY OF THE MANUFACTURER'S TEST RESULTS FOR EACH LOT OF L-830 TRANSFORMERS USED ON THIS PROJECT.

- ALL PULLCANS SHALL HAVE A 3/8-INCH (MIN.) GALVANIZED STEEL COVER, UNLESS OTHERWISE NOTED.

COMPLETION OF CONSTRUCTION

- AT THE PROJECT COMPLETION, THE CONTRACTOR SHALL PROVIDE A TWO (2) HOUR TRAINING SESSION FOR AIRPORT MAINTENANCE PERSONNEL. THE SESSION SHALL COVER ALL INSTALLED EQUIPMENT (NO SEPARATE PAY ITEM).
- AT THE PROJECT COMPLETION, THE CONTRACTOR SHALL PROVIDE A COMPLETE MAINTENANCE MANUAL BOUND IN A THREE-RING NOTEBOOK (NO SEPARATE PAY ITEM). THE MANUAL SHALL CONTAIN THE FOLLOWING AS A MINIMUM:

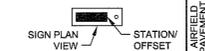
- AS-BUILT WIRING SCHEMATICS
- EQUIPMENT SHOP DRAWINGS SUBMITTALS
- MANUFACTURERS' EQUIPMENT INSTALLATION INSTRUCTIONS
- MANUFACTURERS' MAINTENANCE INSTRUCTIONS
- WARRANTIES
- SPARE PARTS LISTS

SIGN SCHEDULE													
SIGN #	PANEL "A"	PANEL "B"	LOCATION		EQUIPMENT SPECIFICATIONS					ALTERNATES		COMMENTS	
			STATION	OFFSET	# CHAR	# MODULES*	STYLE	CLASS	MODE	SIZE	1		2
4	B2 B/Y	BLANK	46+02	106' LT	3	2	3	1	1	2	X	X	NEW LED SIGN
5	BLANK	B2 B Y/B B/Y	48+65	314' LT	5	3	3	1	1	2	X	X	NEW LED SIGN
6	B B2 Y/B B/Y	BLANK	48+53	455' LT	3	2	3	1	1	2	X	X	NEW LED SIGN
6A	D B B/Y Y/B	BLANK	51+37	460' LT	3	2	3	1	1	2	X	X	NEW LED SIGN
7	BLANK	B2 B/Y	49+25	107' LT	3	2	3	1	1	2	X	X	NEW LED SIGN
8	B2 22-4 Y/B W/R	BLANK	49+52	229' LT	5	3	3	1	1	2	X	X	NEW LED SIGN
9	BLANK	B2 B B/Y Y/B	51+29	345' LT	4	2	3	1	1	2	X	X	NEW LED SIGN
9A	BLANK	B D Y/B B/Y	54+21	345' LT	3	2	3	1	1	2	X	X	NEW LED SIGN
10	D B Y/B B/Y	BLANK	53+78	500' LT	4	2	3	1	1	2	X	X	NEW LED SIGN
11	BLANK	D E B/Y Y/B	55+00	638' LT	3	2	3	1	1	2	X	X	NEW LED SIGN
12	BLANK	D E Y/B B/Y	53+60	694' LT	3	2	3	1	1	2	X	X	NEW LED SIGN
13	E D B/Y B/Y	BLANK	54+68	921' LT	4	2	3	1	1	2	X	X	NEW LED SIGN
14	E D B/Y B/Y	BLANK	54+99	908' LT	4	2	3	1	1	2	X	X	NEW LED SIGN
15	E B B/Y Y/B	BLANK	56+43	465' LT	3	2	3	1	1	2	X	X	NEW LED SIGN
16	E B Y/B B/Y	BLANK	58+74	541' LT	4	2	3	1	1	2	X	X	NEW LED SIGN
16A	B B3 Y/B B/Y	BLANK	60+57	459' LT	4	2	3	1	1	2	X	X	NEW LED SIGN
16B	BLANK	B E Y/B B/Y	60+74	458' LT	3	2	3	1	1	2	X	X	NEW LED SIGN
17	BLANK	B3 B Y/B B/Y	60+80	310' LT	5	3	3	1	1	2	X	X	NEW LED SIGN
18	B3 B/Y	BLANK	60+57	105' LT	3	2	3	1	1	2	X	X	NEW LED SIGN
20	BLANK	B3 B B/Y Y/B	62+45	331' LT	4	2	3	1	1	2	X	X	NEW LED SIGN
21	B3 22-4 Y/B W/R	BLANK	62+19	250' LT	5	3	3	1	1	2	X	X	NEW LED SIGN
25	22 4 B/Y B/Y	BLANK	67+31	345' LT	5	3	3	1	1	2	X	X	NEW LED SIGN
27	BLANK	B3 B/Y	63+44	104' LT	3	2	3	1	1	2	X	X	NEW LED SIGN
28	17-35 W/R	BLANK	65+89	104' LT	4	2	3	1	1	2	X	X	NEW LED SIGN
46	BLANK	B B/Y	70+88	304' LT	2	1	3	1	1	2	X	X	NEW LED SIGN
48A	17-35 B W/R Y/B	BLANK	70+57	339' LT	5	3	3	1	1	2	X	X	NEW LED SIGN
68	CONTACT KIT FOR TAXIWAY LIGHTS B/Y	BLANK	57+78	602' LT	-	-	3	1	1	2	X	X	NEW LED SIGN

* ESTIMATE - COORDINATE WITH SIGN MANUFACTURER ON EXACT NUMBER OF MODULES
 REFERENCE INFORMATION FROM A/C 150/5345-44J "SPECIFICATIONS FOR RUNWAY AND TAXIWAY SIGNS (28 SEP 10)"

STYLE 1 = 120 VAC
 STYLE 2 = 3 STEPS 4.8-6.6 AMPS
 STYLE 3 = 5 STEPS 2.8 - 6.6 AMPS OR 8.5 - 20.0 AMPS
 STYLE 4 = UNLIGHTED
 STYLE 5 = 1 STEP 5.5 AMPS
 CLASS 1 = OPERATING TEMPERATURE FROM -4F (-20C) TO +131F (55C)
 CLASS 2 = OPERATING TEMPERATURE FROM -40F (-40C) TO +131F (55C)
 MODE 1 = WIND LOADING TO 100 MPH
 MODE 2 = WIND LOADING TO 200 MPH
 MODE 3 = WIND LOADING TO 300 MPH
 SIZE 1 = 18" HIGH PANEL WITH 12" CHARACTER
 SIZE 2 = 24" HIGH PANEL WITH 15" CHARACTER
 SIZE 3 = 30" HIGH PANEL WITH 18" CHARACTER
 SIZE 4 = 48" HIGH PANEL WITH 40" CHARACTER
 SIZE 5 = 30" HIGH PANEL WITH 20" CHARACTER

LOCATION GUIDE



SIGN COLOR LEGEND



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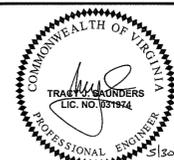
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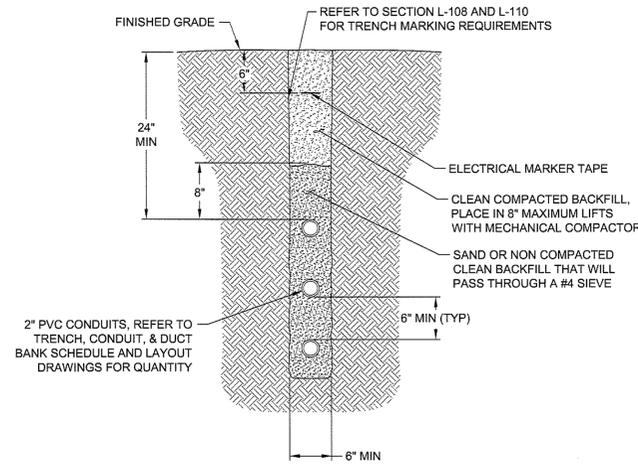
DELTA AIRPORT CONSULTANTS, INC.



AIRFIELD PAVEMENT REHABILITATION-PHASE 2		AIP NO. 3-51-0029-036 (PENDING)	JOB NO. 12121
ELECTRICAL NOTES & SIGN SCHEDULE		DRAWN BY: DWS	SHEET 48
		DESIGNED BY: TJS	OF 69
LYNCHBURG REGIONAL AIRPORT		SCALE: NONE	DATE: MAY 2013

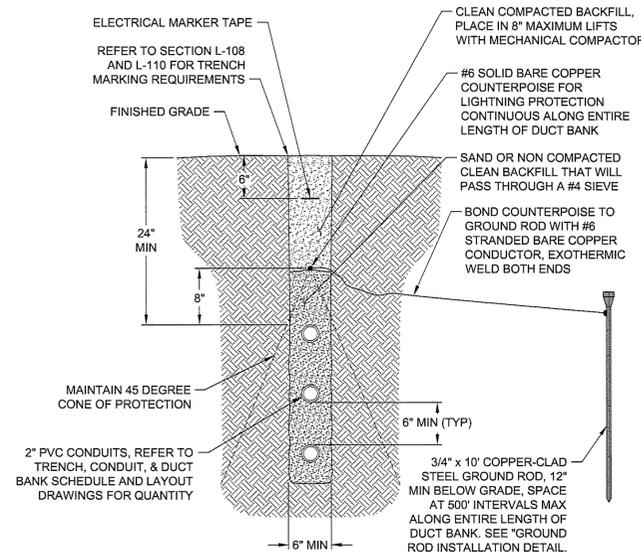
TRENCH, CONDUIT, & DUCT BANK NOTES:

1. THE #6 BARE COPPER COUNTERPOISE AND ASSOCIATED GROUND RODS AND EXOTHERMIC WELD CONNECTIONS SHOWN IN THE TRENCH, CONDUIT, AND DUCT BANK DETAILS WILL BE PAID FOR UNDER THE #6 BARE COPPER COUNTERPOISE LINE ITEM. ALL OTHER ITEMS SHOWN IN THE TRENCH, CONDUIT, AND DUCT BANK DETAILS ARE INCIDENTAL TO THE COST OF THE RESPECTIVE TRENCH, CONDUIT, OR DUCT BANK.
2. CONDUITS SHALL BE INSTALLED WITH CONDUIT SPACERS AT 5' ON CENTER.
3. ROUTE SKV CABLES IN LOWER LEVEL CONDUITS WHERE POSSIBLE. DO NOT ROUTE DIFFERENT VOLTAGE CLASSIFICATION CABLES IN THE SAME CONDUIT.
4. GROUND RODS SHALL ALSO BE USED TO TERMINATE COUNTERPOISE AT BOTH ENDS OF TRENCH, CONDUIT RUN, OR DUCT BANK (NO SEPARATE PAY ITEM).



CONDUIT DETAIL - WITHOUT COUNTERPOISE (2" PVC)
(Trench # 3)
NTS

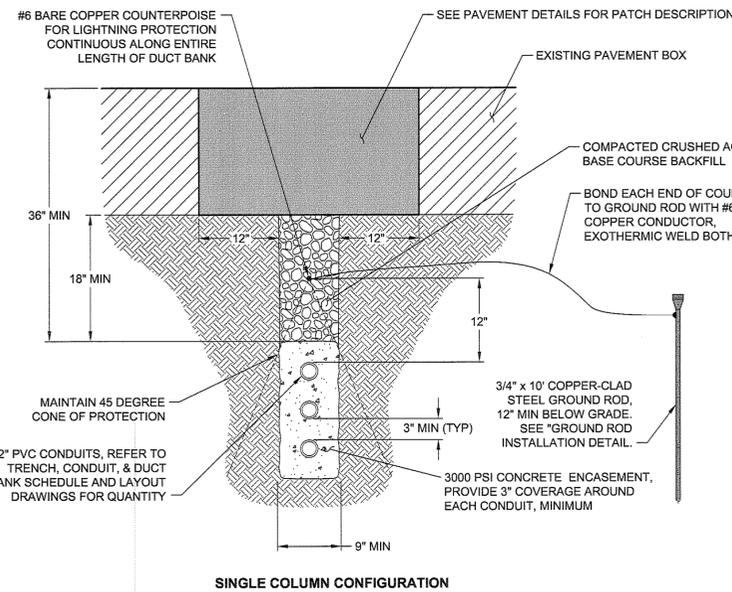
- NOTE:
1. THE ILLUSTRATION OF THE 3-WAY CONDUIT RUN SHOWN ABOVE IS FOR CONFIGURATION PURPOSES. SEE TRENCH, CONDUIT, & DUCT BANK SCHEDULE AND LAYOUT DRAWINGS FOR CONDUIT QUANTITY. SEE ELECTRICAL NOTES SHEET FOR TRENCH, CONDUIT, & DUCT BANK NOTES.



CONDUIT DETAIL - WITH COUNTERPOISE (2" PVC)
(Trench # 4)
NTS

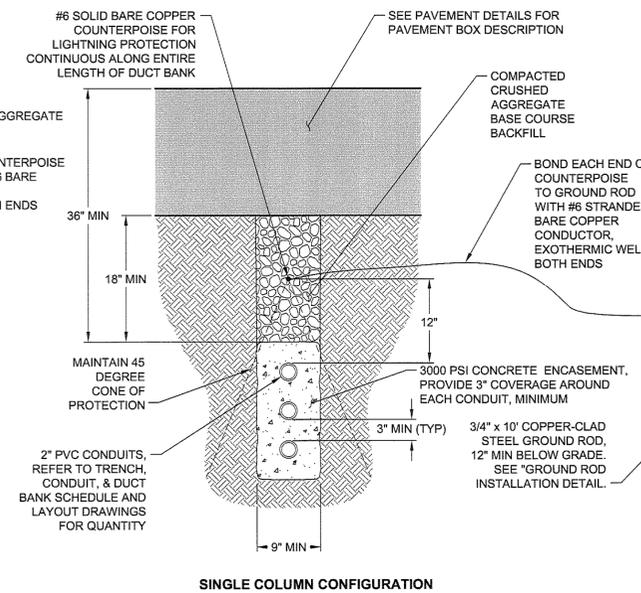
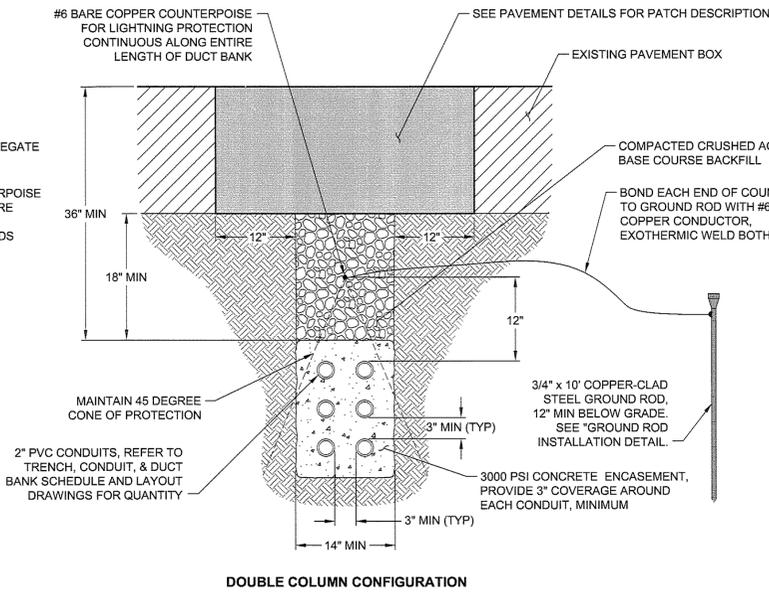
CONDUIT DETAIL - WITH COUNTERPOISE (2" PVC)
(Trench # 4)
NTS

- NOTE:
1. THE ILLUSTRATIONS SHOWN ABOVE OF THE 3-WAY SINGLE COLUMN OR THE 6-WAY DOUBLE COLUMN CONDUIT RUNS ARE FOR CONFIGURATION PURPOSES ONLY. SEE TRENCH, CONDUIT, & DUCT BANK SCHEDULE AND LAYOUT DRAWINGS FOR CONDUIT QUANTITY. SEE ELECTRICAL NOTES SHEET FOR TRENCH, CONDUIT, & DUCT BANK NOTES.



DUCT BANK DETAIL - TRENCHED THROUGH EXISTING PAVEMENT (2" PVC)
Aircraft Rated Pavement
(Trench # 11)
NTS

- NOTE:
1. THE ILLUSTRATIONS SHOWN ABOVE OF THE 3-WAY SINGLE COLUMN OR THE 6-WAY DOUBLE COLUMN CONDUIT RUNS ARE FOR CONFIGURATION PURPOSES ONLY. SEE TRENCH, CONDUIT, & DUCT BANK SCHEDULE AND LAYOUT DRAWINGS FOR CONDUIT QUANTITY. SEE ELECTRICAL NOTES SHEET FOR TRENCH, CONDUIT, & DUCT BANK NOTES.



DUCT BANK DETAIL - INSTALLED UNDER NEW PAVEMENT (2" PVC)
Aircraft Rated Pavement
(Trench # 9)
NTS

- NOTE:
1. THE ILLUSTRATIONS SHOWN ABOVE OF THE 3-WAY SINGLE COLUMN OR THE 6-WAY DOUBLE COLUMN CONDUIT RUNS ARE FOR CONFIGURATION PURPOSES ONLY. SEE TRENCH, CONDUIT, & DUCT BANK SCHEDULE AND LAYOUT DRAWINGS FOR CONDUIT QUANTITY. SEE ELECTRICAL NOTES SHEET FOR TRENCH, CONDUIT, & DUCT BANK NOTES.

NO.	REVISIONS	BY	APP.	DATE

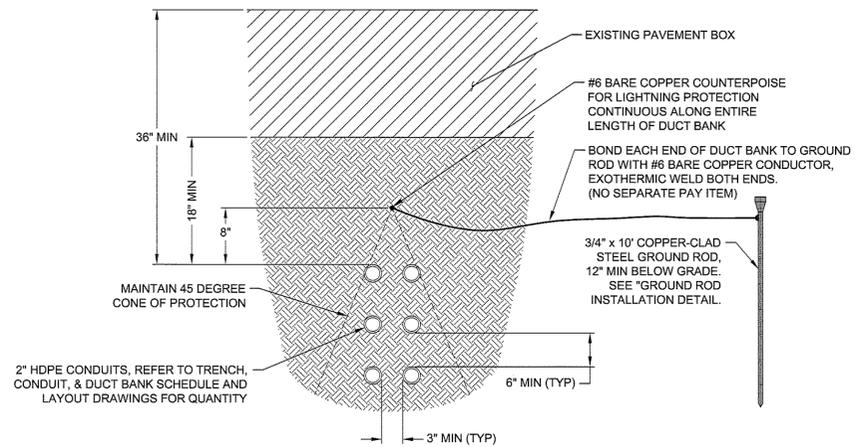
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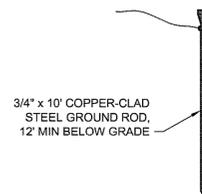
AIRFIELD PAVEMENT REHABILITATION-PHASE 2		AIP NO. 3-51-0029-036 (PENDING)	JOB NO. 12121
DUCT BANK & TRENCH DETAILS		DRAWN BY: DWS	SHEET 49
		DESIGNED BY: TJS	OF 69
LYNCHBURG REGIONAL AIRPORT		SCALE: NONE	DATE: MAY 2013



CONDUIT DETAIL - BORED UNDER PAVEMENT (2" HDPE)
Aircraft Rated Pavement
 (Trench # 5)
 NTS

NOTE:

1. THE ILLUSTRATION OF THE 6-WAY CONDUIT RUN SHOWN ABOVE IS FOR CONFIGURATION PURPOSES. SEE TRENCH, CONDUIT, & DUCT BANK SCHEDULE AND LAYOUT DRAWINGS FOR CONDUIT QUANTITY. SEE ELECTRICAL NOTES SHEET FOR TRENCH, CONDUIT, & DUCT BANK NOTES.
2. THE LINEAR FOOT PRICE FOR BORING SHALL INCLUDE ALL VERTICAL DEFLECTION AND PREPARATION/BORING TO THE LOCATIONS SHOWN ON THE PLANS.

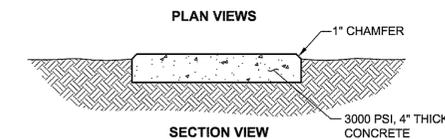
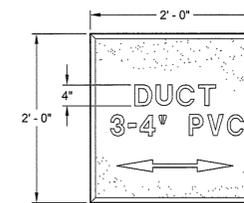
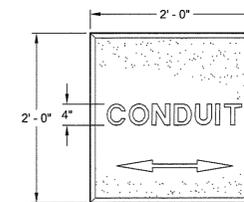
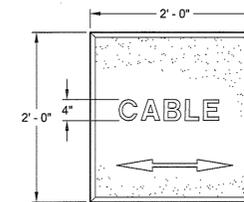


GROUND ROD INSTALLATION DETAIL
 NTS

NOTES:

1. THE IMPEDANCE TO GROUND OF EACH GROUND ROD SHALL NOT EXCEED 25 OHMS. SHOULD THE IMPEDANCE EXCEED 25 OHMS THE CONTRACTOR SHALL INSTALL A SECOND GROUND ROD AT LEAST 6 FEET AWAY IN ACCORDANCE WITH THE NATIONAL ELECTRICAL CODE (NEC). RETEST AFTER THE SECOND GROUND ROD IS INSTALLED AND NOTIFY ENGINEER IF IMPEDANCE EXCEEDS 25 OHMS.
2. GROUND ELECTRODES SHALL BE INSTALLED IN ACCORDANCE WITH NEC 250.53. WHERE ROCK PREVENTS VERTICALLY DRIVEN RODS, OBLIQUELY DRIVEN (UP TO 45 DEGREES FROM VERTICAL) OR TRENCHED HORIZONTAL GROUND RODS SHALL BE INSTALLED IN ACCORDANCE WITH NEC 250.53(G).

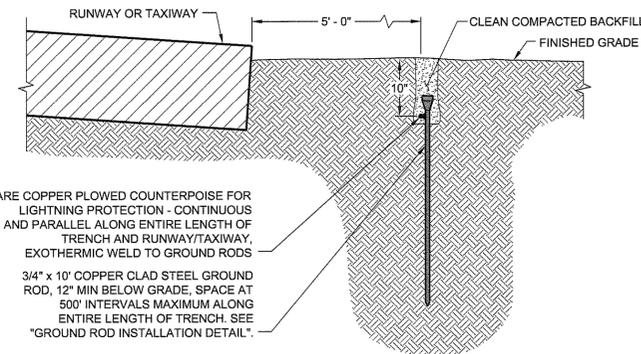
WHERE INSTALLING TRENCHED HORIZONTAL GROUND RODS, INSTALL 30" DEEP OR BELOW FROST LINE WHICHEVER IS DEEPER. ALSO PLACE GROUND ENHANCEMENT MATERIAL (GEM) IN ACCORDANCE WITH MANUFACTURER'S INSTRUCTIONS WITH A MINIMUM 1" TOP AND BOTTOM COVER. GEM MUST BE PERMANENT AND MAINTENANCE-FREE AND MAINTAIN ITS EARTH RESISTANCE WITH TIME. GEM MUST SETUP FIRMLY AND NOT DISSOLVE OR DECOMPOSE, OR OTHERWISE POLLUTE THE SOIL OR THE LOCAL WATER TABLE. THE GEM MUST ALSO BE SUITABLE FOR INSTALLATION IN A SLURRY FORM. GEM SHALL NOT DEPEND ON THE CONTINUOUS PRESENCE OF WATER TO MAINTAIN ITS CONDUCTIVITY.



TRENCH, CONDUIT, & DUCT BANK MARKER DETAIL
 NTS

NOTES:

1. LETTERS SHALL BE 4" HIGH, 1/4" DEEP WITH A 1/2" STROKE WIDTH. FREE HAND LETTERING IS NOT PERMITTED. MARKERS SHALL BE PAINTED INTERNATIONAL ORANGE. CONSTRUCTION AND INSTALLATION SHALL BE IN ACCORDANCE WITH STANDARD SPECIFICATION L-108 AND L-110. (NO SEPARATE PAY ITEM).
2. AS A MINIMUM, MARKERS ARE REQUIRED AT THE FOLLOWING:
 - A. ALL ENDS OF DIRECT BURIED CABLE TRENCHES, CONDUIT RUNS, AND DUCT BANKS
 - B. ALL CHANGES IN DIRECTION OF DIRECT BURIED CABLE TRENCHES, CONDUIT RUNS, AND DUCT BANKS
 - C. EVERY 200' FOR DIRECT BURIED CABLE TRENCHES
 - D. ALL NON-FIXTURE RELATED CABLE CONNECTION SPLICE POINTS



TRENCH DETAIL - LIGHTNING PROTECTION (Grassed Shoulder)
 (Trench # 1)
 NTS

NOTES:

1. THE #6 BARE COPPER COUNTERPOISE AND ASSOCIATED GROUND RODS AND EXOTHERMIC WELD CONNECTIONS SHOWN IN THE DETAIL ABOVE SHALL BE PAID FOR SEPARATELY UNDER THE #6 BARE COPPER COUNTERPOISE LINE ITEM. ALL OTHER ITEMS SHOWN IN THE DETAIL ABOVE ARE INCIDENTAL TO THE COST OF THE TRENCH.
2. GROUND RODS SHALL ALSO BE USED TO TERMINATE COUNTERPOISE AT BOTH ENDS OF TRENCH (NO SEPARATE PAY ITEM).

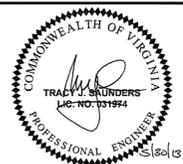
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NO.	REVISIONS	BY	APP.	DATE

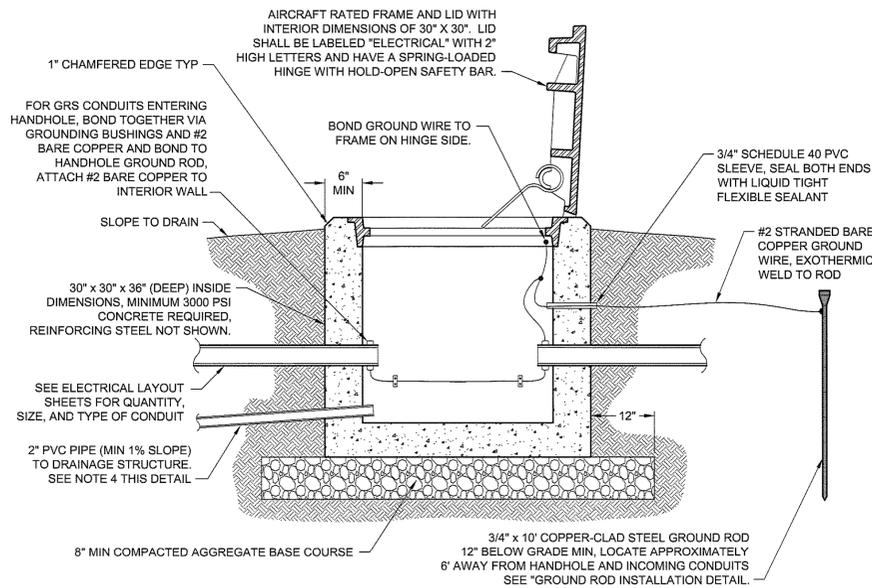
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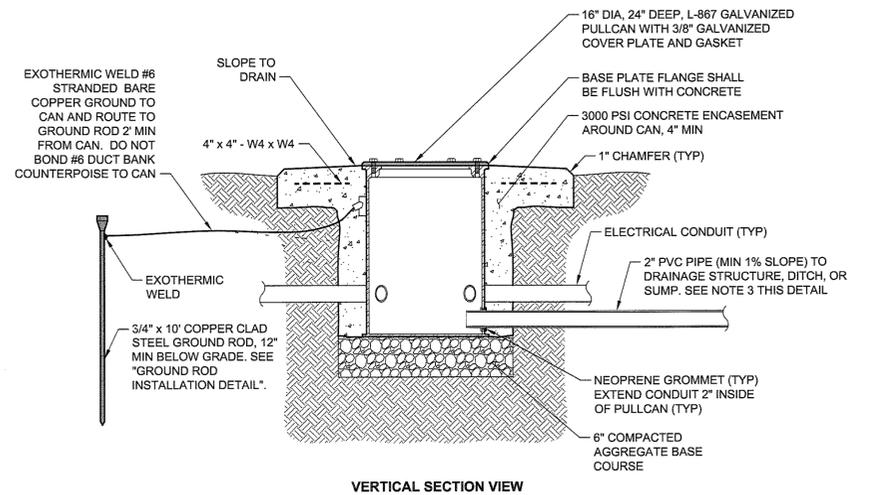
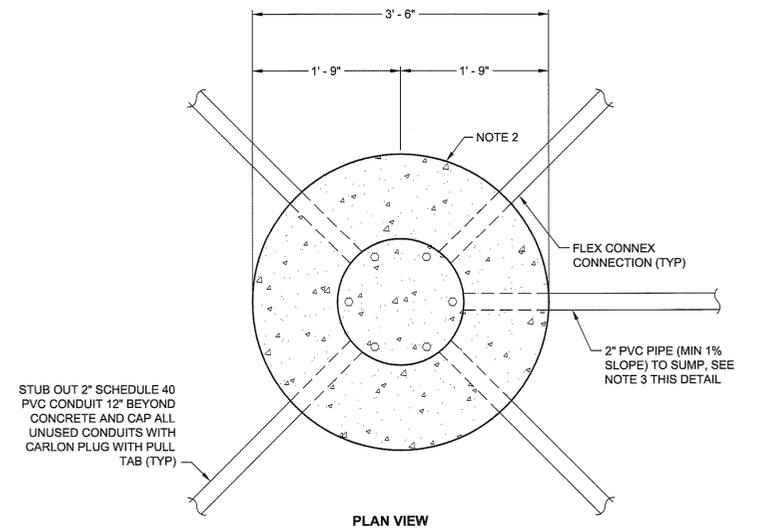
AIRFIELD PAVEMENT REHABILITATION-PHASE 2		AIP NO. 3-51-0029-036 (PENDING)	JOB NO. 12121
DUCT BANK & TRENCH DETAILS		DRAWN BY: DWS	SHEET 50
		DESIGNED BY: TJS	OF 69
LYNCHBURG REGIONAL AIRPORT		SCALE: NONE	DATE: MAY 2013



AIRCRAFT RATED REINFORCED CONCRETE HANDHOLE DETAIL
NTS

NOTES:

1. FRAME AND LID SHALL CONSIST OF A GRAY IRON FRAME WITH DUCTILE IRON LID AND SHALL BE PROOF LOADED TESTED TO 200,000 POUNDS.
2. PROVIDE AND INSTALL TWO SADDLE RACKS ON EACH WALL OF HANDHOLE. ALL CABLES SHALL BE PROPERLY TAGGED AND LABELED IN HANDHOLE. PROVIDE & INSTALL BELL ENDS ON EACH DUCT AND GROUT DUCT AT HANDHOLE WITH AN APPROVED NON-SHRINK GROUT.
3. STRUCTURE SHALL BE DESIGNED TO WITHSTAND AIRCRAFT STRUT LOADING OF 185,000 POUNDS ON DUAL GEAR WITH 150 PSI TIRE PRESSURE (B727-200). CONTRACTOR SHALL PROVIDE DESIGN CALCULATIONS AND STRUCTURAL DRAWINGS OF FOUNDATION BED, HANDHOLE OR MANHOLE STRUCTURE, FRAME, AND LID SEALED BY A LICENSED PROFESSIONAL ENGINEER IN THE STATE OF INSTALLATION.
4. IF DRAINAGE SUMP IS ORDERED, INSTALL DRAINAGE SUMP 10' FROM HANDHOLE AS SHOWN ON THE PLANS. SEE "CPP DRAINAGE SUMP DETAIL". TOP OF SUMP SHALL BE BELOW FINISHED ELEVATION OF PULLCAN BOTTOM. (COST OF SUMP, PVC PIPE, AND ALL INSTALLATION LABOR IS INCIDENTAL TO THE PULLCAN.)



PULLCAN DETAIL
NTS

NOTES:

1. PULLCANS, CONCRETE, CONDUIT, W4 x W4, AGGREGATE, GROUND ROD, COUNTERPOISE, EXCAVATION, AND BACKFILL ARE INCIDENTAL TO THE COST OF THE PULLCAN.
2. A 3'-6" SQUARE PAD MAY ALTERNATELY BE USED IN TURFED AREAS ONLY.
3. IF DRAINAGE SUMP IS ORDERED, INSTALL DRAINAGE SUMP 10' FROM PULLCAN AS SHOWN ON THE PLANS. SEE "CPP DRAINAGE SUMP DETAIL". TOP OF SUMP SHALL BE BELOW FINISHED ELEVATION OF PULLCAN BOTTOM. (COST OF SUMP, PVC PIPE, AND ALL INSTALLATION LABOR IS INCIDENTAL TO THE PULLCAN.)

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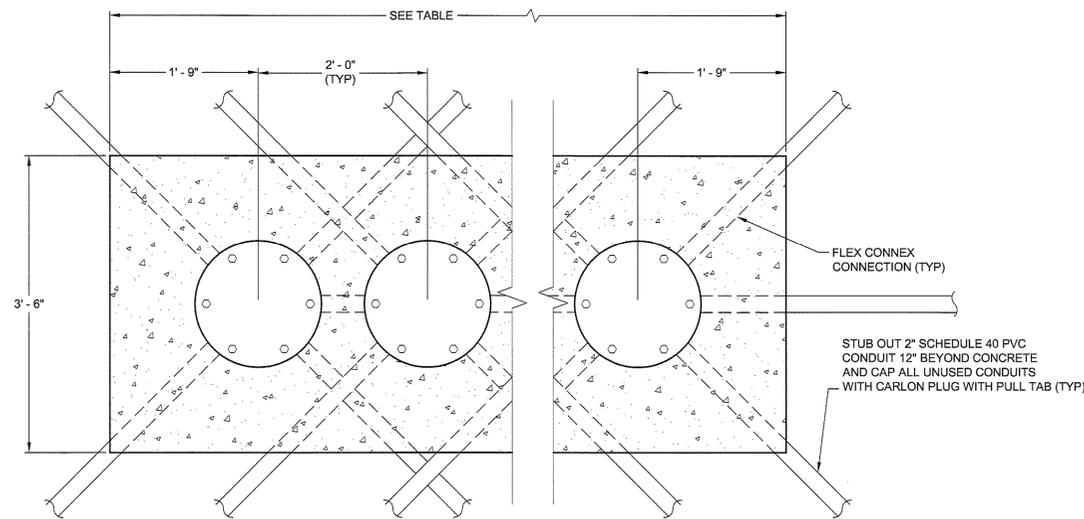
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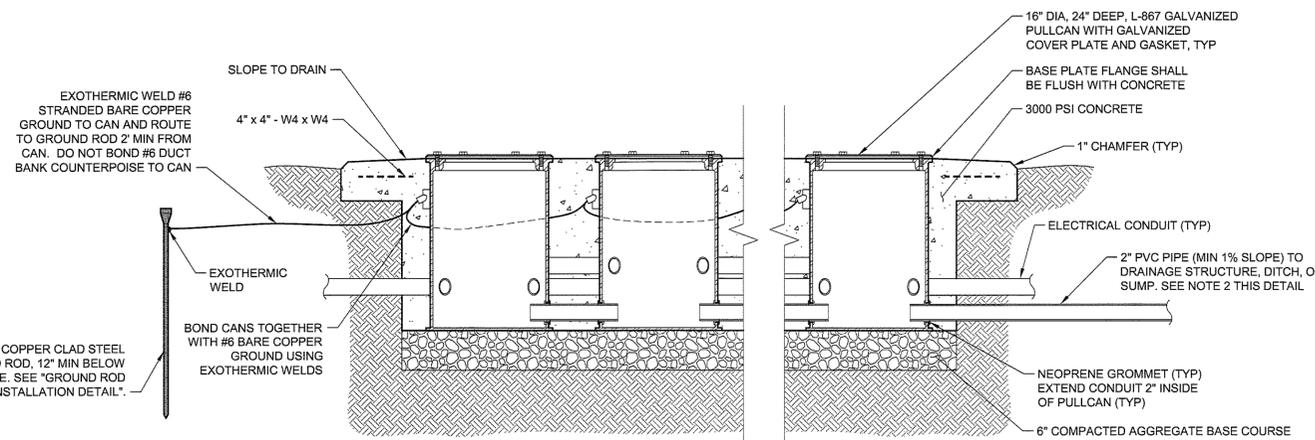
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AIRFIELD PAVEMENT REHABILITATION-PHASE 2		AIP NO. 3-51-0029-036 (PENDING)	JOB NO. 12121
PULLCAN & HANDHOLE DETAILS		DRAWN BY: DWS	SHEET 51 OF 69
		DESIGNED BY: TJS	
LYNCHBURG REGIONAL AIRPORT		SCALE: NONE	DATE: MAY 2013



PLAN VIEW



VERTICAL SECTION VIEW

MULTIPLE UNIT PULLCAN PLAZA DETAIL

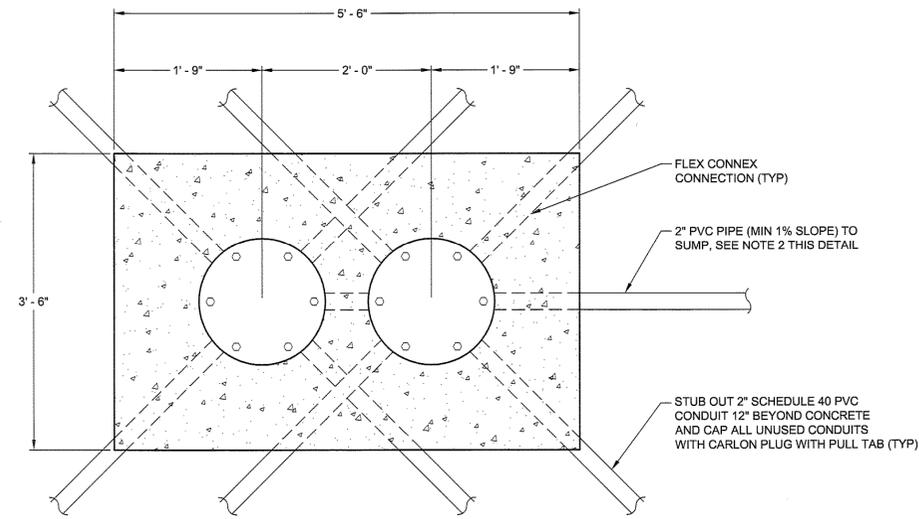
NTS

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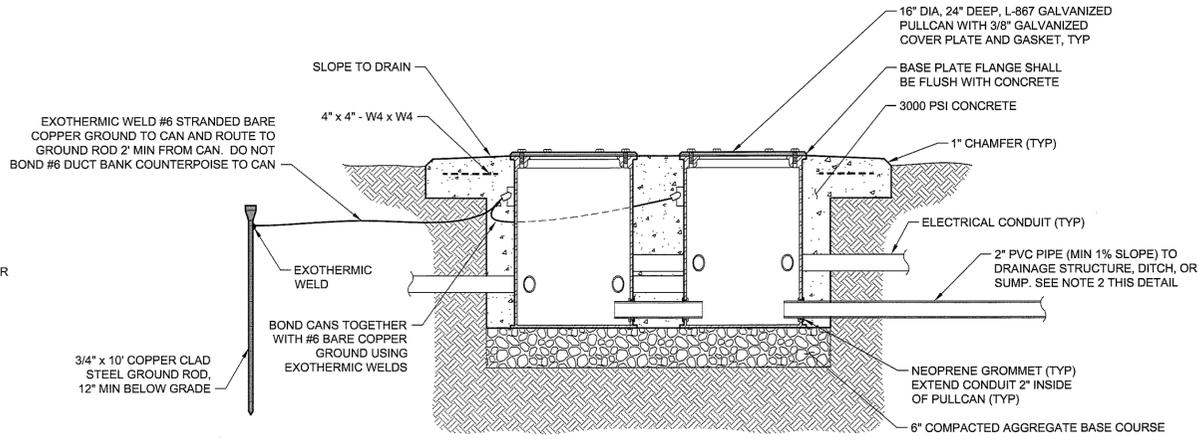
- PULLCANS, CONCRETE, CONDUIT, W4 x W4, AGGREGATE, GROUND ROD, COUNTERPOISE, EXCAVATION, AND BACKFILL ARE INCIDENTAL TO THE COST OF THE PULLCAN PLAZA.
- IF DRAINAGE SUMP IS ORDERED, INSTALL DRAINAGE SUMP 10' FROM PULLCAN AS SHOWN ON THE PLANS. SEE "CPP DRAINAGE SUMP DETAIL". TOP OF SUMP SHALL BE BELOW FINISHED ELEVATION OF PULLCAN BOTTOM. (COST OF SUMP, PVC PIPE, AND ALL INSTALLATION LABOR IS INCIDENTAL TO THE PULLCAN.)

PLAZA WIDTH TABLE

# OF UNITS	OVERALL WIDTH
3	7' - 6"
4	9' - 6"
5	11' - 6"
6	13' - 6"
7	15' - 6"
8	17' - 6"
9	19' - 6"
10	21' - 6"



PLAN VIEW



VERTICAL SECTION VIEW

2 UNIT PULLCAN PLAZA DETAIL

NTS

NOTES:

- PULLCANS, CONCRETE, CONDUIT, W4 x W4, AGGREGATE, GROUND ROD, COUNTERPOISE, EXCAVATION, AND BACKFILL ARE INCIDENTAL TO THE COST OF THE PULLCAN PLAZA.
- IF DRAINAGE SUMP IS ORDERED, INSTALL DRAINAGE SUMP 10' FROM PULLCAN AS SHOWN ON THE PLANS. SEE "CPP DRAINAGE SUMP DETAIL". TOP OF SUMP SHALL BE BELOW FINISHED ELEVATION OF PULLCAN BOTTOM. (COST OF SUMP, PVC PIPE, AND ALL INSTALLATION LABOR IS INCIDENTAL TO THE PULLCAN.)

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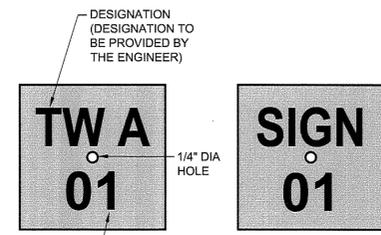


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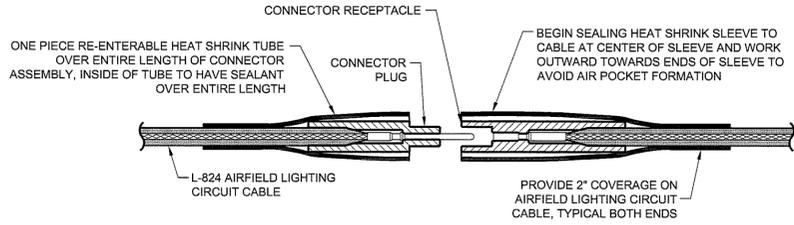
AIRFIELD PAVEMENT REHABILITATION-PHASE 2		AIP NO. 3-51-0029-036 (PENDING)	JOB NO. 12121
PULLCAN & HANDHOLE DETAILS		DRAWN BY: DWS	SHEET 52
		DESIGNED BY: TJS	OF 69
LYNCHBURG REGIONAL AIRPORT		SCALE: NONE	DATE: MAY 2013

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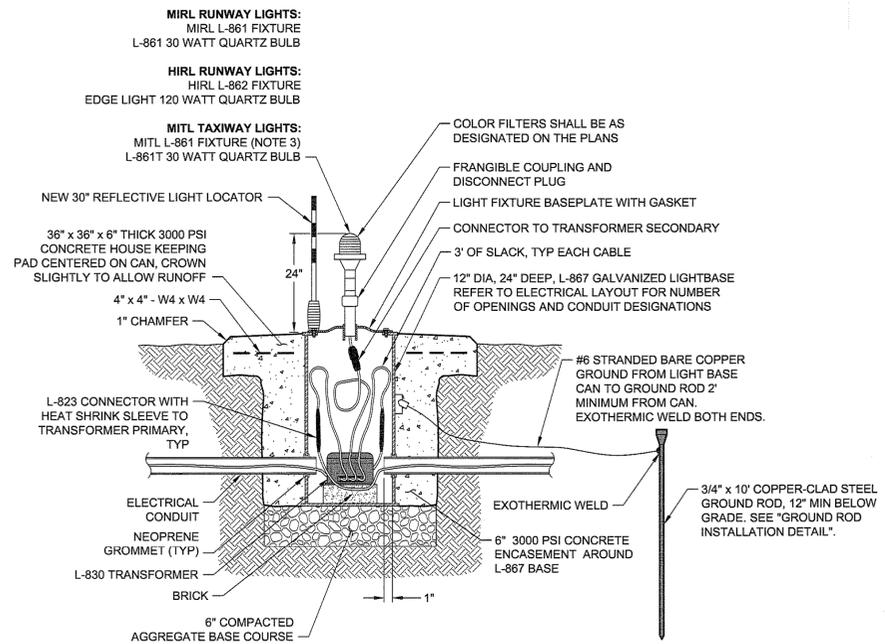


LIGHT/SIGN FIXTURE IDENTIFICATION TAG DETAIL
NTS

- NOTES:
- 1" MIN CHARACTER HEIGHT VISIBLE FROM EDGE OF PAVEMENT.
 - CHARACTERS ENGRAVED IN BLACK ON 1/8" THICK WHITE PLASTIC LAMINATE. ATTACH TO GUIDANCE SIGN SIDE PANEL WITH SELF TAPPING SCREW SEALED WITH SILICONE SEALANT. ATTACH TO LIGHT FIXTURE STEM WITH CONDUIT HANGER OR CLIP.
 - FIXTURE IDENTIFICATION TAGS SHALL BE PROVIDED FOR ALL RUNWAY / TAXIWAY LIGHTS AND SIGNS. (NO SEPARATE PAY ITEM)

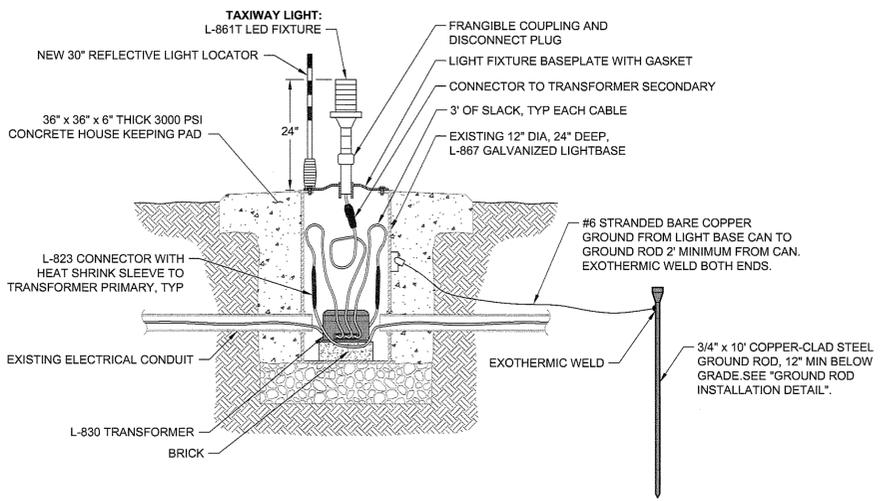


L-823 AIRFIELD LIGHTING CABLE CONNECTOR DETAIL
NTS



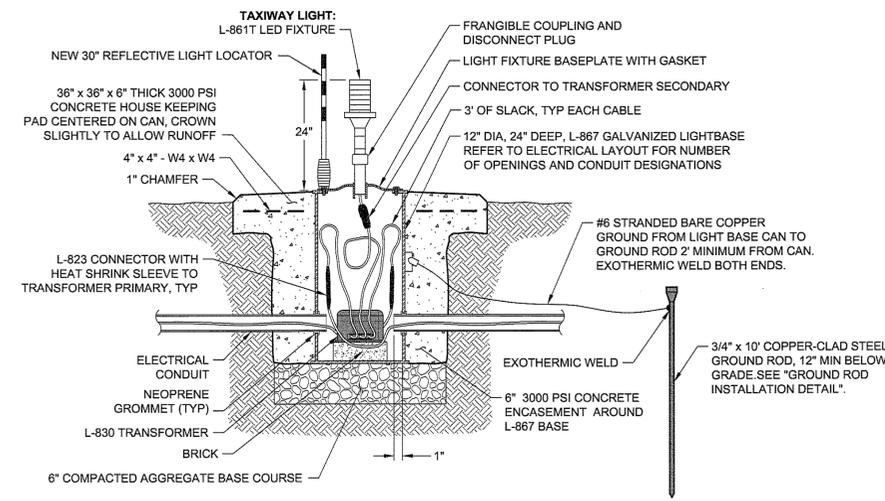
BASE MOUNTED RUNWAY & TAXIWAY EDGE LIGHT DETAIL
NTS

- NOTE:
- THE #6 STRANDED BARE COPPER GROUND AND THE GROUND ROD IS INCIDENTAL TO THE COST OF THE RUNWAY/TAXIWAY EDGE LIGHT.
 - TAXIWAY LIGHT MARKER SHALL BE ORANGE FOR RUNWAY EDGE LIGHTS AND BLUE FOR TAXIWAY EDGE LIGHTS.
 - THE CONTRACTOR SHALL UTILIZE RELOCATED FIXTURES FOR THE NON-LED TAXIWAY EDGE LIGHTS. ALL OTHER COMPONENTS (BASEPLATE, GASKET, TRANSFORMER, REFLECTIVE LIGHT LOCATOR, BULB, ETC.) SHALL BE NEW AND PROVIDED BY THE CONTRACTOR.



BASE MOUNTED L.E.D. TAXIWAY EDGE LIGHT ON EXISTING BASE DETAIL
NTS

- NOTE:
- THE #6 STRANDED BARE COPPER GROUND AND THE GROUND ROD IS INCIDENTAL TO THE COST OF THE TAXIWAY EDGE LIGHT.
 - TAXIWAY LIGHT MARKER SHALL BE BLUE FOR TAXIWAY EDGE LIGHTS.



BASE MOUNTED L.E.D. TAXIWAY EDGE LIGHT DETAIL
NTS

- NOTE:
- THE #6 STRANDED BARE COPPER GROUND AND THE GROUND ROD IS INCIDENTAL TO THE COST OF THE TAXIWAY EDGE LIGHT.
 - TAXIWAY LIGHT MARKER SHALL BE SHALL BE BLUE FOR TAXIWAY EDGE LIGHTS.

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LIGHT FIXTURE DETAILS		DRAWN BY: DWS	SHEET 53
		DESIGNED BY: TJS	OF 69
LYNCHBURG REGIONAL AIRPORT		SCALE: NONE	DATE: MAY 2013

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 REVISIONS: WIND CONE & AIRFIELD GUIDANCE SIGN DETAILS

NO.	REVISIONS	BY	APP.	DATE

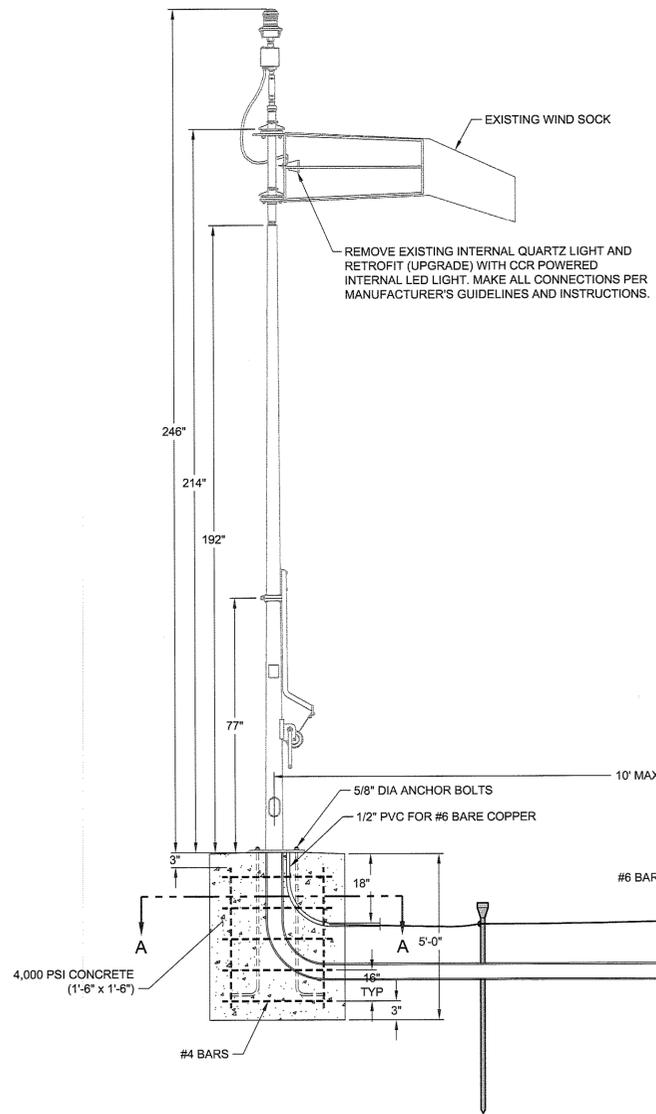
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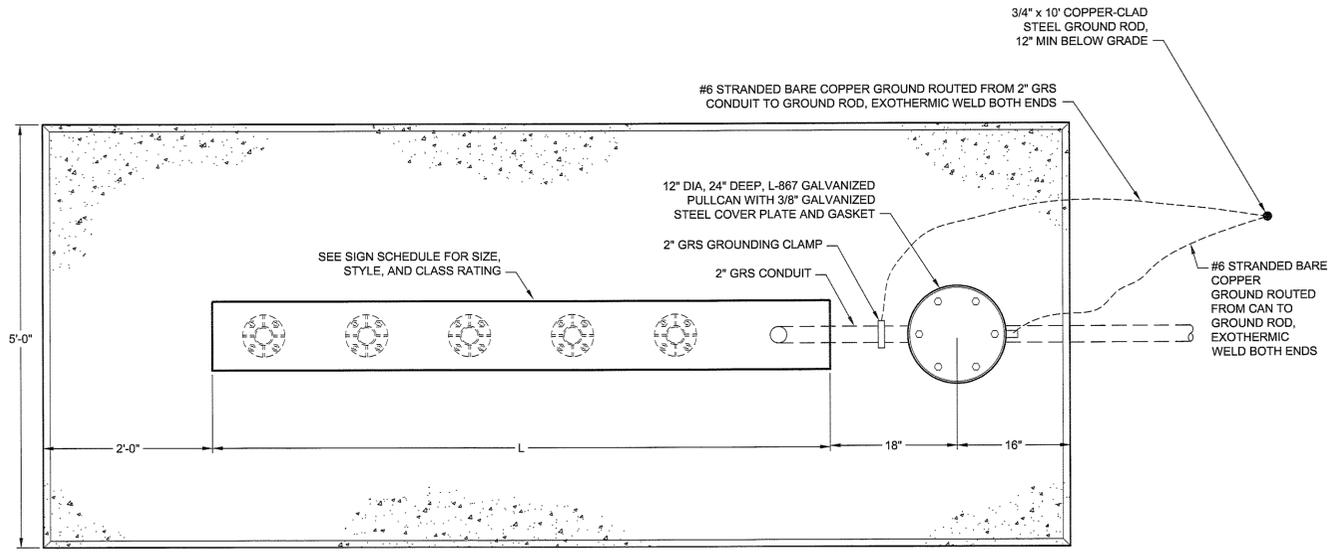
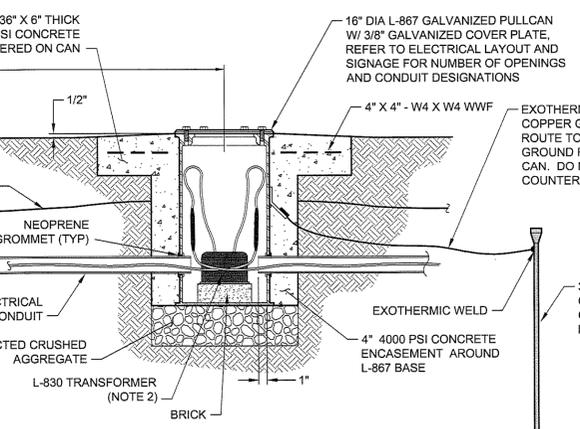
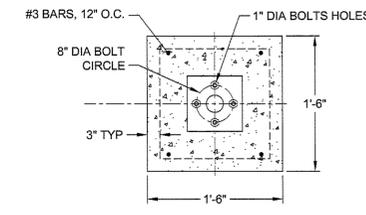


AIRFIELD PAVEMENT REHABILITATION-PHASE 2		AIP NO. 3-51-0029-036 (PENDING)	JOB NO. 12121
WIND CONE & AIRFIELD GUIDANCE SIGN DETAILS		DRAWN BY: DWS	SHEET 55
LYNCHBURG REGIONAL AIRPORT		DESIGNED BY: TJS	OF 69
SCALE: NONE	DATE: MAY 2013		

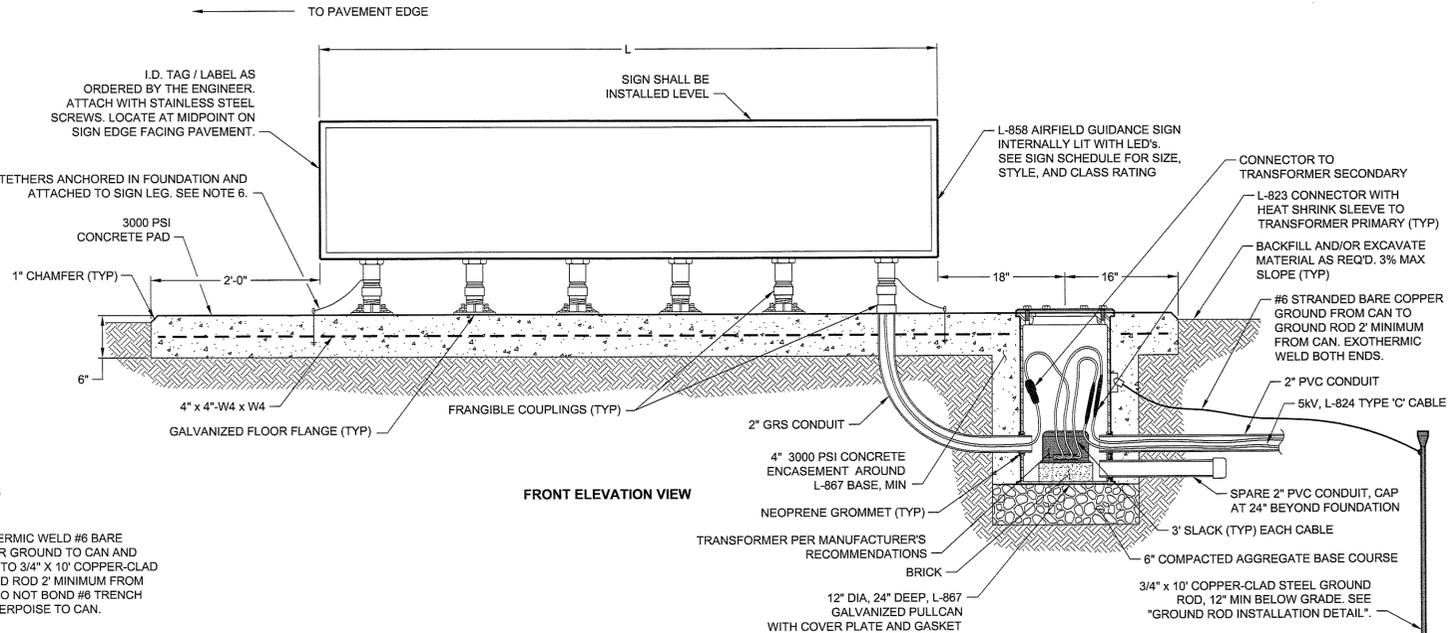


L-806 INTERNALLY LIGHTED WINDCONE INSTALLATION DETAIL
 NTS

- NOTES:
- EXISTING L-806 WINDCONE IS HALI-BRITE.
 - REMOVE EXISTING POWER ADAPTER AND TURN OVER TO OWNER FOR SALVAGE. INSTALL NEW L-830 TRANSFORMER AS REQUIRED AND ROUTE SECONDARY CONDUCTORS TO WINDCONE LED LIGHT PER MANUFACTURER'S GUIDELINES AND INSTRUCTIONS.



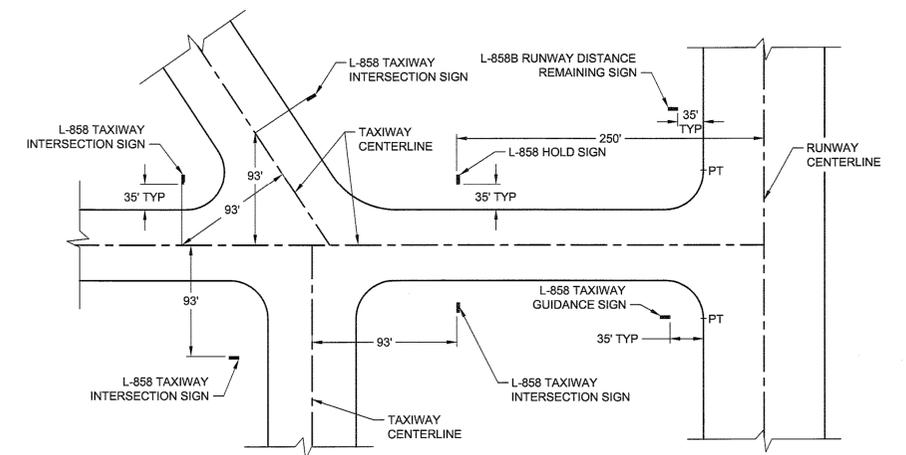
PLAN VIEW



FRONT ELEVATION VIEW

AIRFIELD GUIDANCE SIGN (L.E.D.) DETAIL
 NTS

- NOTES:
- PROVIDE A L-823 DISCONNECT PLUG AND RECEPTACLE IN THE FRANGIBLE COUPLING OF THE POWER LEG AS REQUIRED.
 - ALL CONDUIT UNDER THE CONCRETE PAD IS INCIDENTAL TO THE COST OF THE AIRFIELD GUIDANCE SIGN.
 - ORIENT THE INTERNAL POWER LUGS OF THE AIRFIELD GUIDANCE SIGN SO THAT THEY WILL BE CLOSEST TO THE LEG THROUGH WHICH THE POWER ENTERS THE SIGN HOUSING.
 - THE #6 STRANDED BARE COPPER GROUND AND THE GROUND ROD ARE INCIDENTAL TO THE COST OF THE AIRFIELD GUIDANCE SIGN.
 - CONTRACTOR TO COORDINATE DEPTH OF PAD WITH MANUFACTURER'S RECOMMENDED ANCHOR BOLTS.
 - SINGLE MODULE SIGN REQUIRES ONE TETHER. MULTIPLE MODULE SIGN IN CONTINUOUS FRAME MUST USE TETHER AT BOTH ENDS.



TYPICAL SIGN LAYOUT DETAIL

NTS

NOTE:

1. ALL TAXIWAY TO TAXIWAY INTERSECTION SIGNS SHALL BE COLLOCATED PERPENDICULAR TO THE TAXIWAY CENTERLINE AT THE POINT 93 FEET (MINIMUM) FROM THE INTERSECTING TAXIWAY CENTERLINE, OR AS SHOWN ON THE PLANS.

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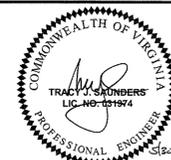
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NO.	REVISIONS	BY	APP.	DATE

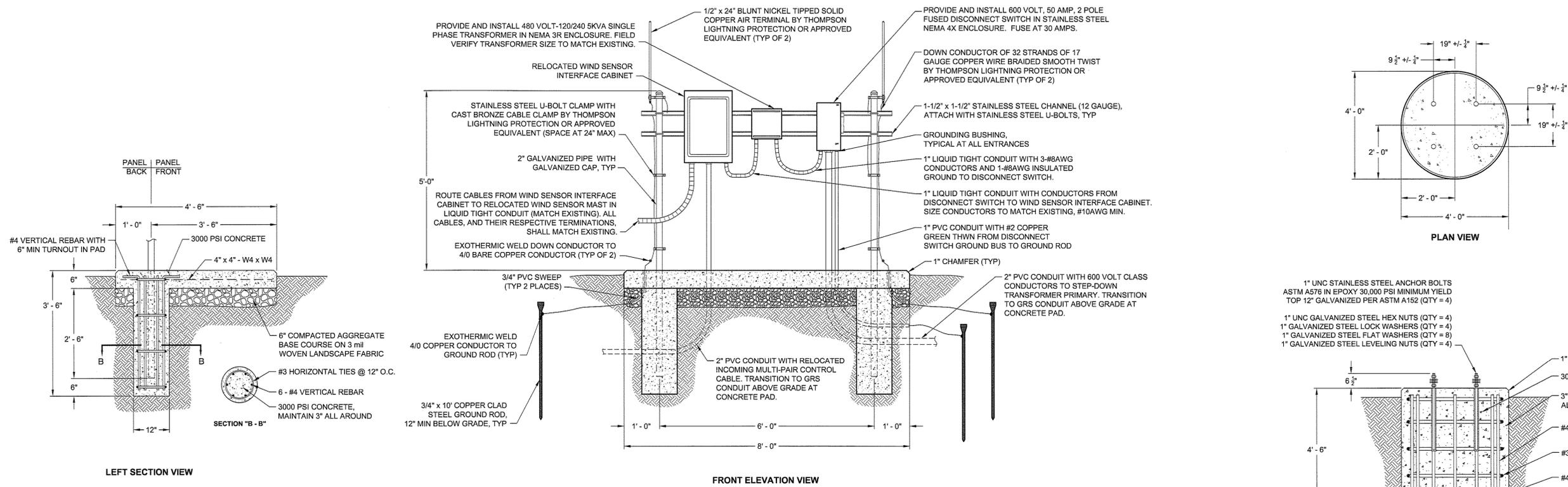
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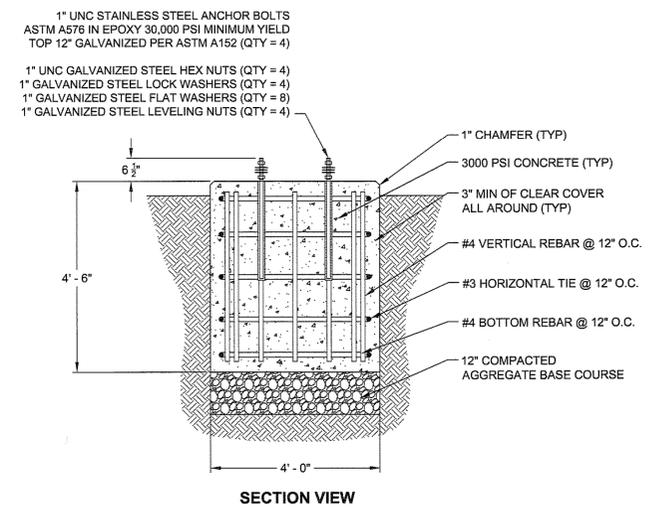
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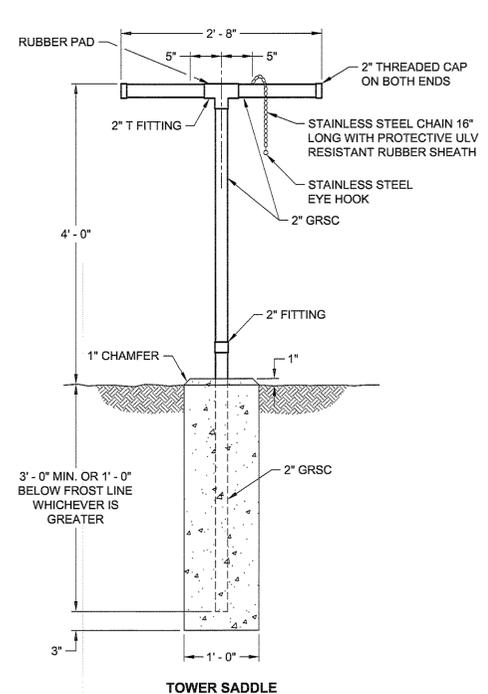
AIRFIELD PAVEMENT REHABILITATION-PHASE 2		AIP NO. 3-51-0029-036 (PENDING)	JOB NO. 12121
AIRFIELD GUIDANCE SIGN DETAILS		DRAWN BY: DWS	SHEET 56
		DESIGNED BY: TJS	OF 69
LYNCHBURG REGIONAL AIRPORT		SCALE: NONE	DATE: MAY 2013



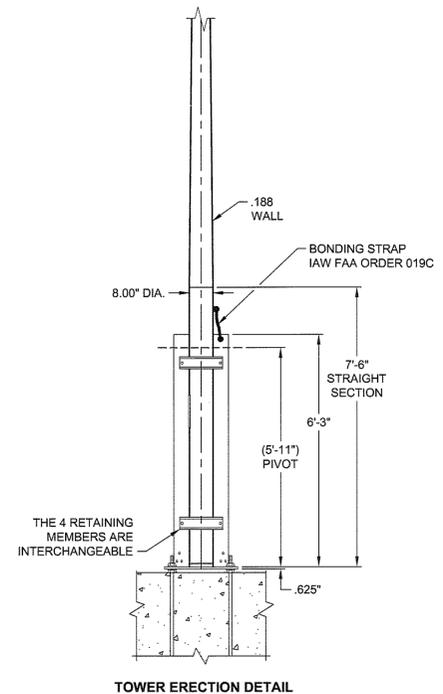
ELECTRICAL PANEL BOARD SERVICE RACK DETAIL
NTS



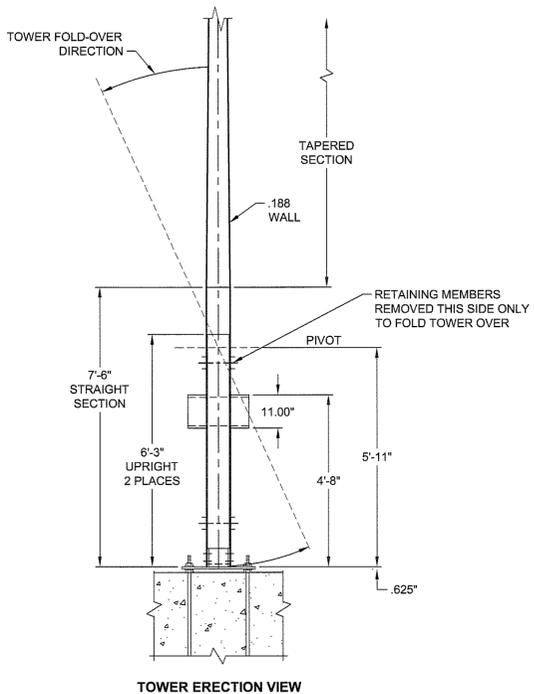
F-420 WIND SENSOR MAST FOUNDATION DETAIL
NTS



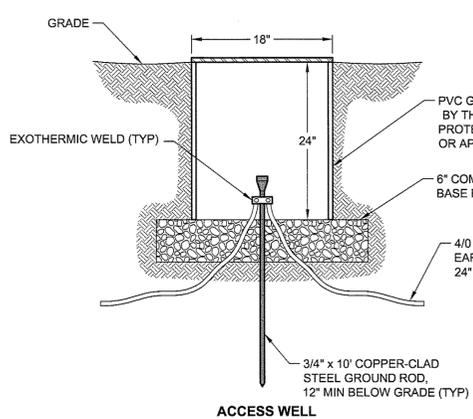
TOWER SADDLE



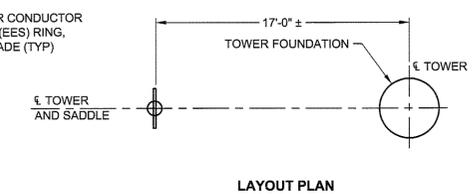
TOWER ERECTION DETAIL



TOWER ERECTION VIEW



ACCESS WELL



LAYOUT PLAN

- EQUIPMENT DETAIL**
NTS
- NOTE:
1. DETAILS PROVIDED REFLECT TYPICAL FAA INSTALLATION DETAILS. THE CONTRACTOR SHALL VERIFY THE EXISTING CONDITIONS PRIOR TO BEGINNING THE RELOCATION.
 2. CONTRACTOR TO RELOCATE THE EXISTING WIND SENSOR TO NEW FOUNDATION.
 3. CONTRACTOR TO INSTALL NEW TOWER SADDLE.

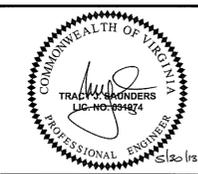
- NOTES:**
1. CONTRACTOR TO FIELD VERIFY CONFIGURATION, SIZE (DIAMETER, DEPTH), AND BOLT PATTERN/SPACING PRIOR TO BEGINNING THE RELOCATION. ANY DISCREPANCIES SHALL BE BROUGHT TO THE IMMEDIATE ATTENTION OF THE ENGINEER.
- NOTE:
- DRAWINGS ON THIS SHEET ARE FOR GENERAL INFORMATION PURPOSES. CONTRACTOR SHALL COORDINATE DRAWINGS SHOWN ON THIS SHEET WITH ACTUAL EQUIPMENT TO BE RELOCATED.

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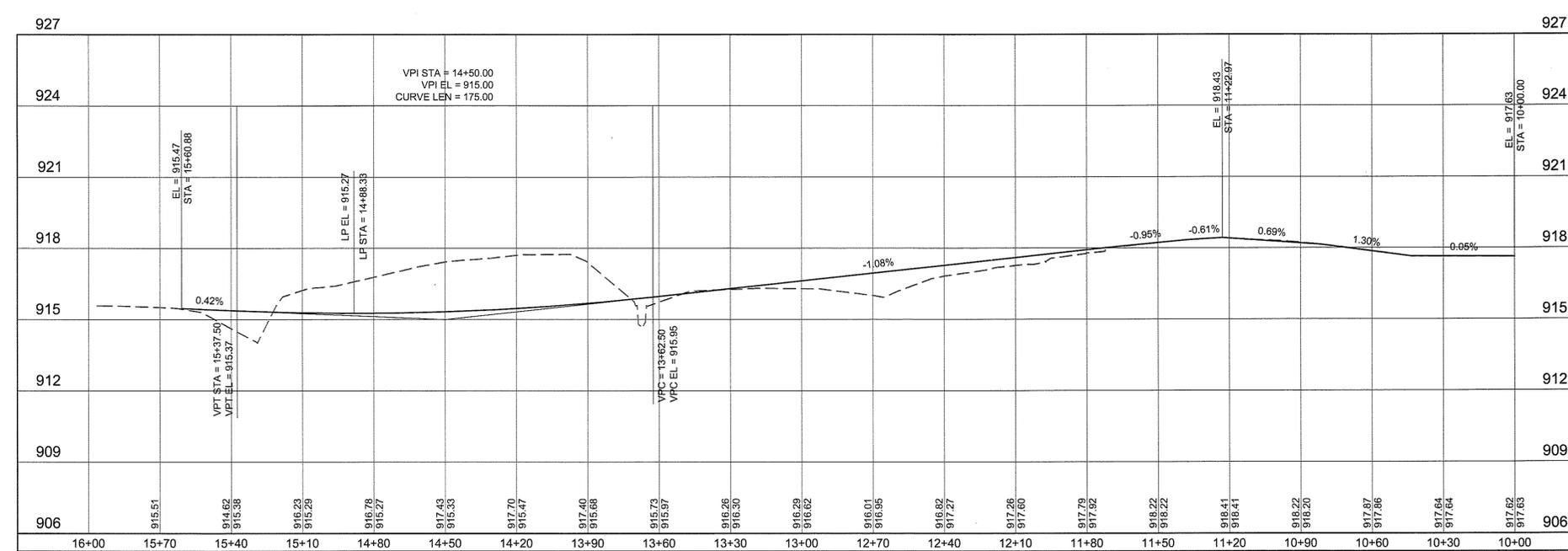
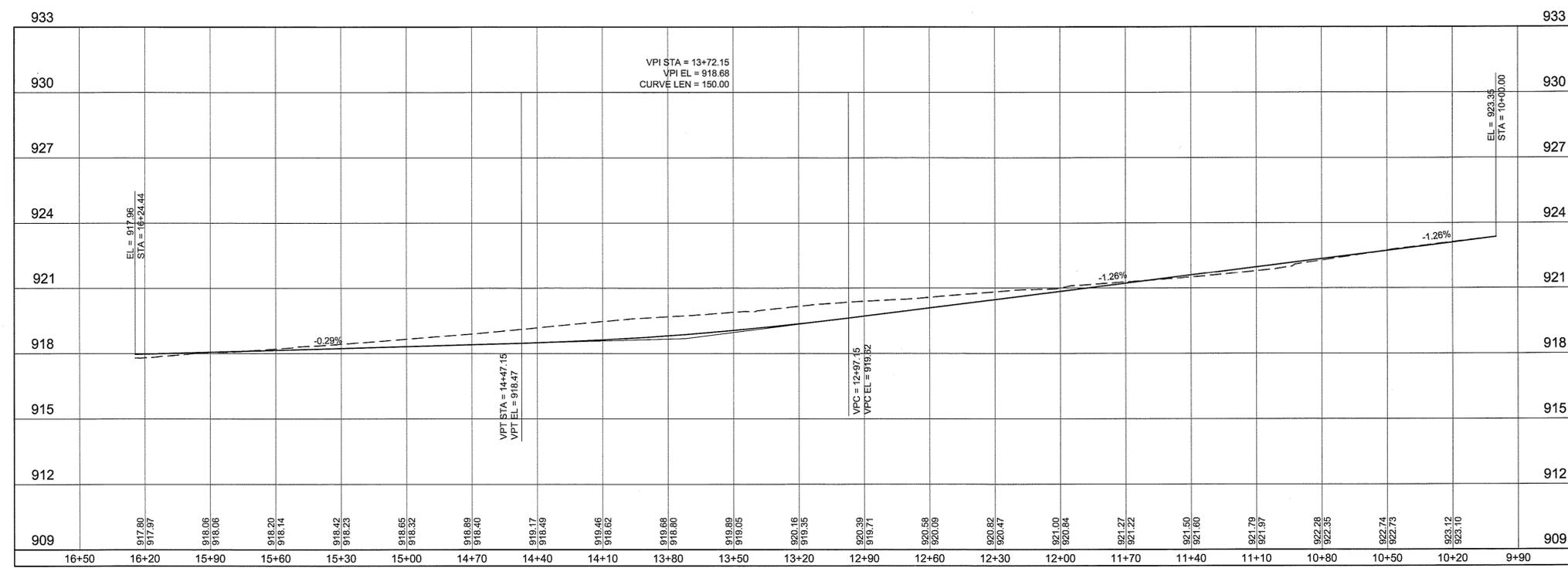
AIRFIELD PAVEMENT REHABILITATION-PHASE 2		AIP NO. 3-51-0029-036 (PENDING)	JOB NO. 12121
FAA WIND SENSOR DETAILS		DRAWN BY: DWS	SHEET 58
		DESIGNED BY: TJS	OF 69
LYNCHBURG REGIONAL AIRPORT		SCALE: 1" = 10'	DATE: MAY 2013

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DRAWING: WFS\chris.laynor\12121.dwg, LAYOUT: ETO, 5/20/2013 10:53:03 AM, dwg
XREFS: WIND_SENSOR_FOUNDATION_DETAIL.dwg, WIND_SENSOR_FOUNDATION_DETAIL.dwg, WIND_SENSOR_FOUNDATION_DETAIL.dwg, WIND_SENSOR_FOUNDATION_DETAIL.dwg, WIND_SENSOR_FOUNDATION_DETAIL.dwg

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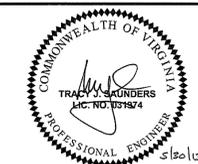
--- EXISTING GRADE
 — PROPOSED GRADE



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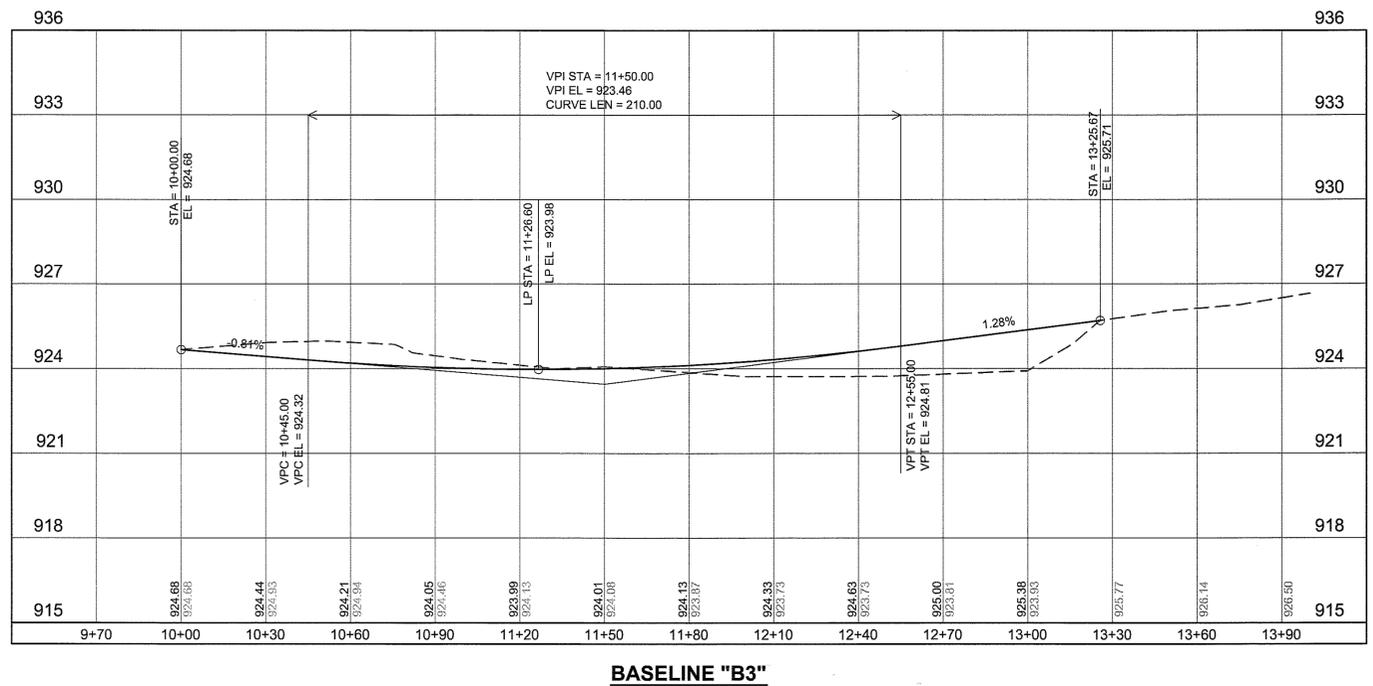
AIRFIELD PAVEMENT REHABILITATION-PHASE 2		AIP NO. 3-51-0029-036 (PENDING)	JOB NO. 12121
CENTERLINE PROFILES		DRAWN BY: DWS	SHEET 59
LYNCHBURG REGIONAL AIRPORT		DESIGNED BY: TJS	OF 69
SCALE: HORIZ: 1" = 30' VERT: 1" = 3'	DATE: MAY 2013		

NO.	REVISIONS	BY	APP.	DATE

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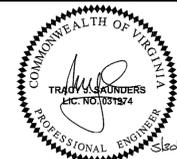
--- EXISTING GRADE
 — PROPOSED GRADE



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AIRFIELD PAVEMENT REHABILITATION-PHASE 2		AIP NO. 3-51-0029-036 (PENDING)	JOB NO. 12121
CENTERLINE PROFILES		DRAWN BY: DWS	SHEET 60
		DESIGNED BY: TJZ	OF 69
LYNCHBURG REGIONAL AIRPORT		SCALE: HORIZ: 1" = 30' VERT: 1" = 3'	DATE: MAY 2013

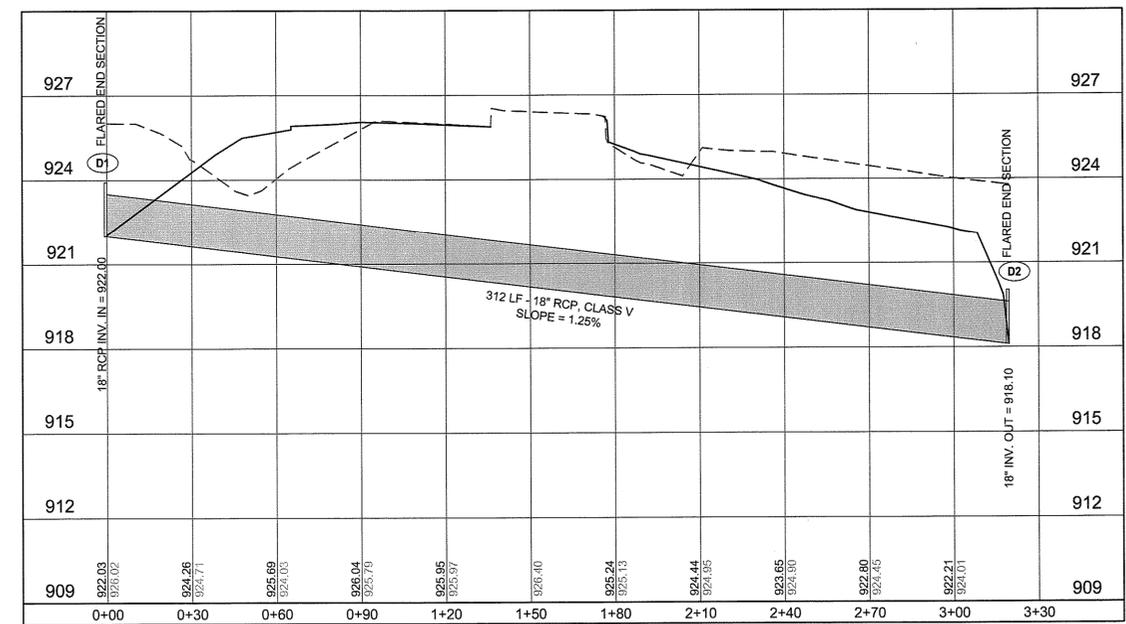
NO.	REVISIONS	BY	APP.	DATE

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LEGEND

--- EXISTING GRADE
 — PROPOSED GRADE



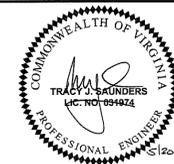
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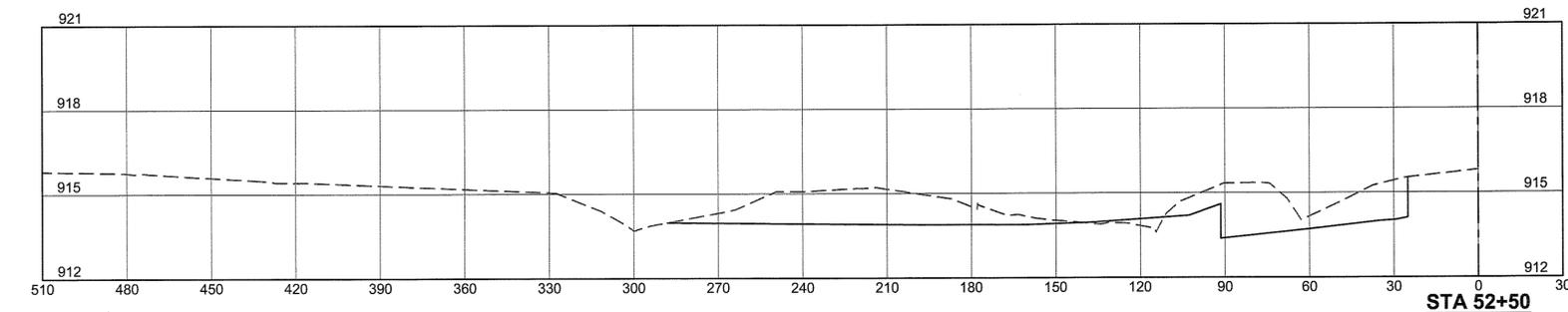
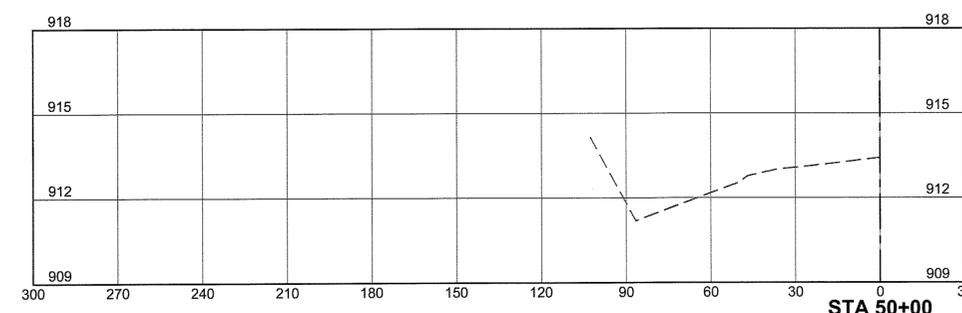
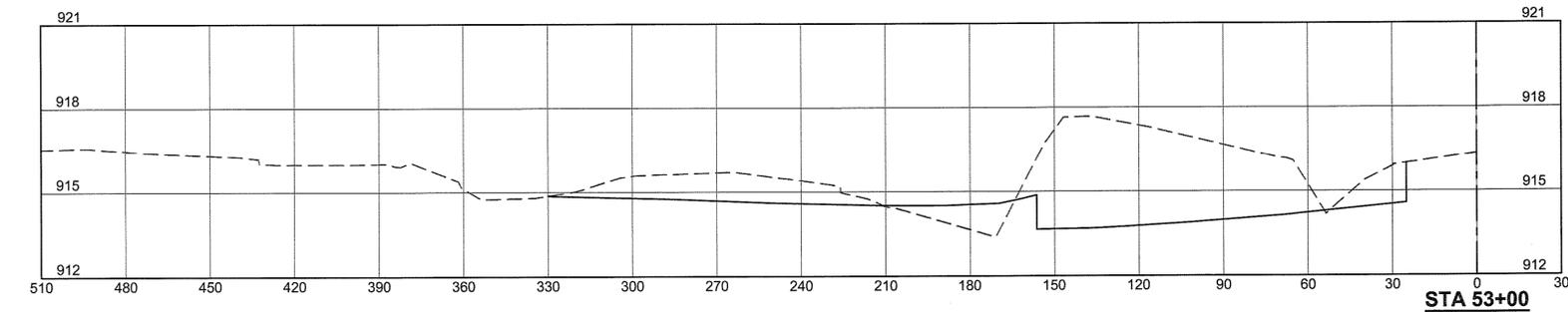
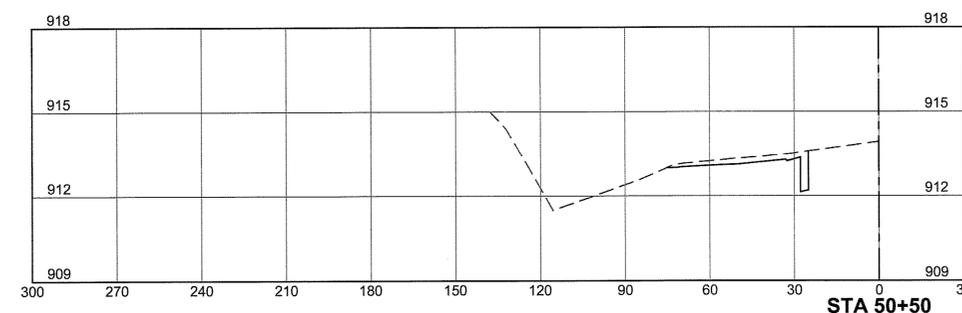
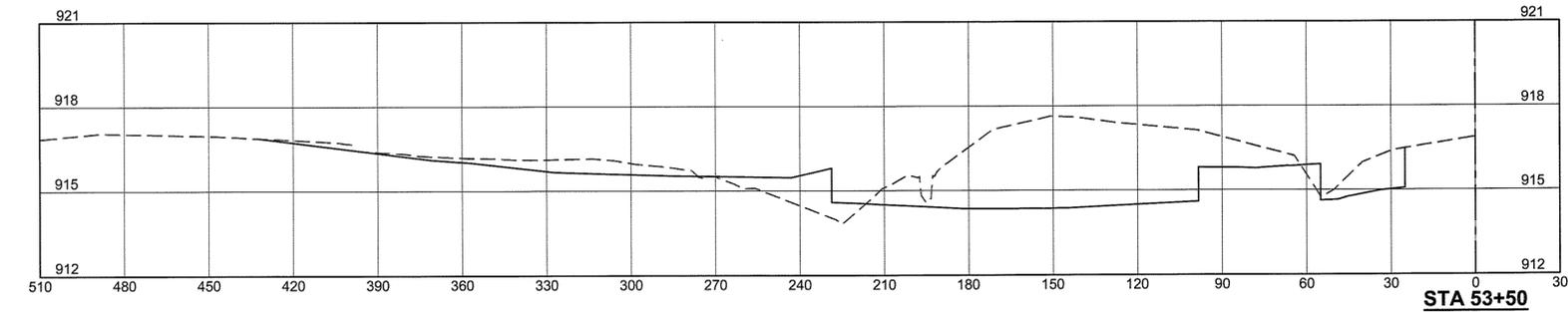
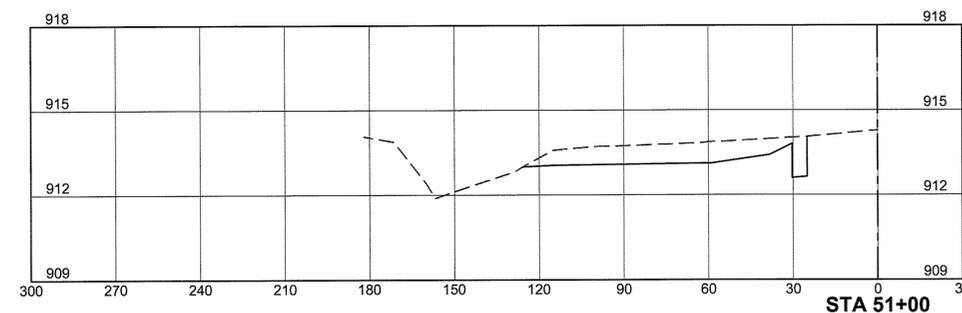
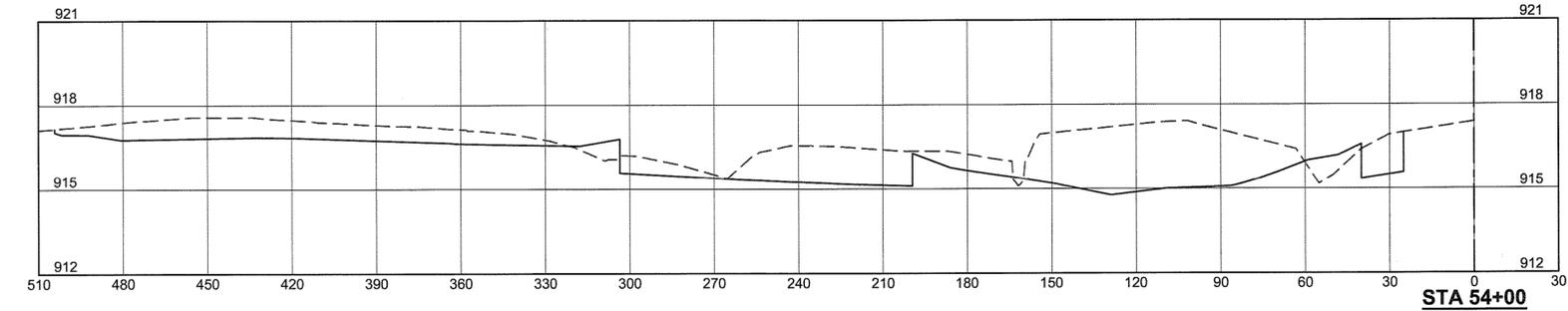
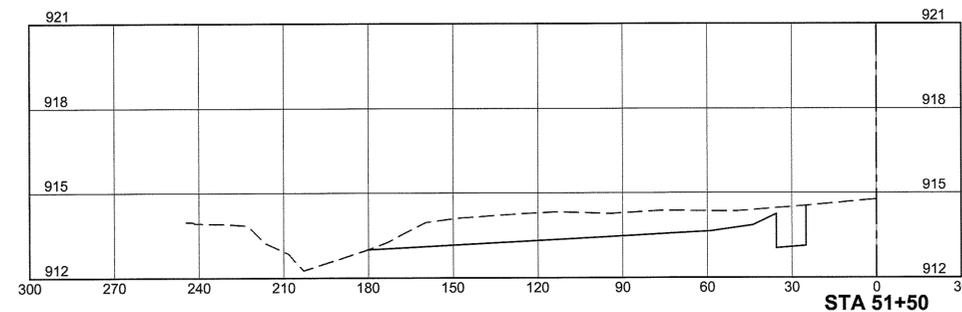
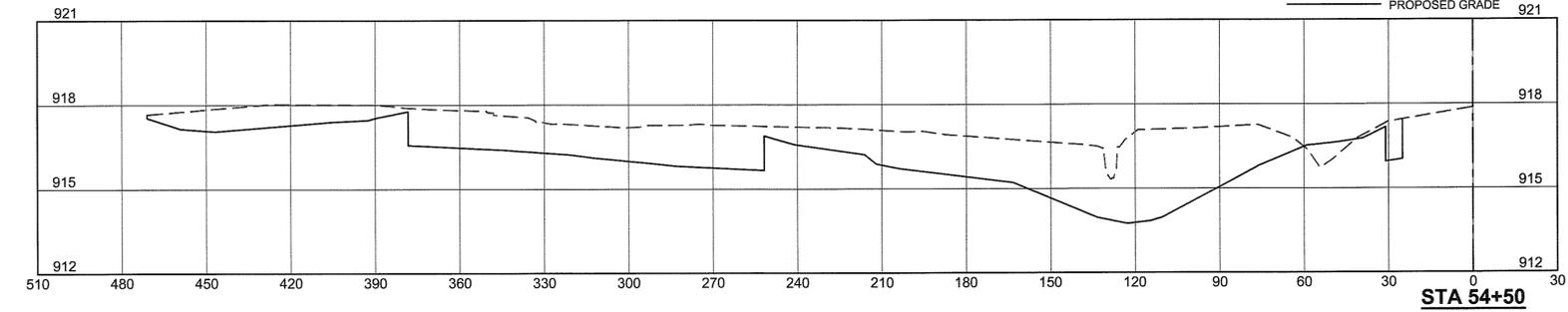
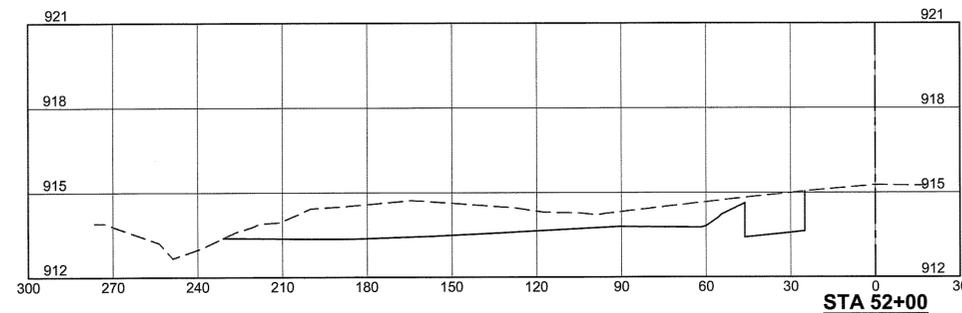


AIRFIELD PAVEMENT REHABILITATION-PHASE 2		AIP NO. 3-51-0029-036 (PENDING)	JOB NO. 12121
DRAINAGE PROFILES		DRAWN BY: DWS	SHEET 61 OF 69
		DESIGNED BY: TJZ	
LYNCHBURG REGIONAL AIRPORT		SCALE: HORIZ: 1" = 30' VERT: 1" = 3'	DATE: MAY 2013

LEGEND

NOTE: CROSS SECTIONS REFLECT BITUMINOUS PAVEMENT SECTIONS (SCHEDULE 1).

--- EXISTING GRADE
 — PROPOSED GRADE

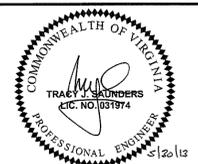


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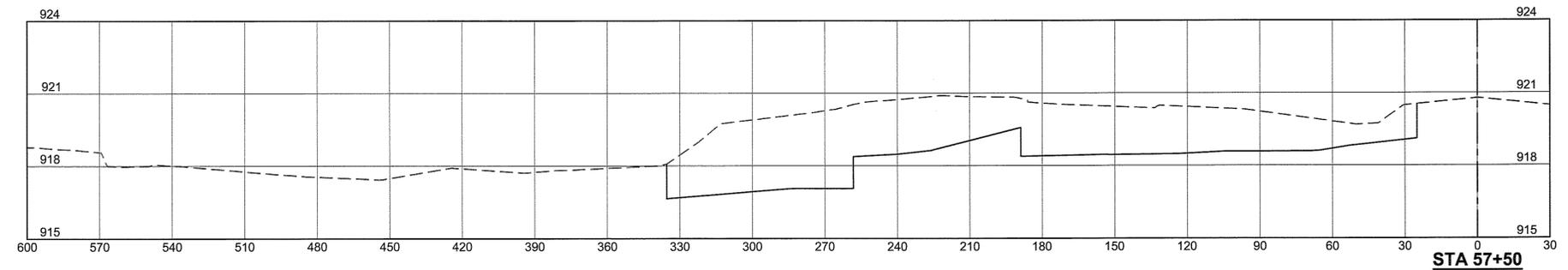
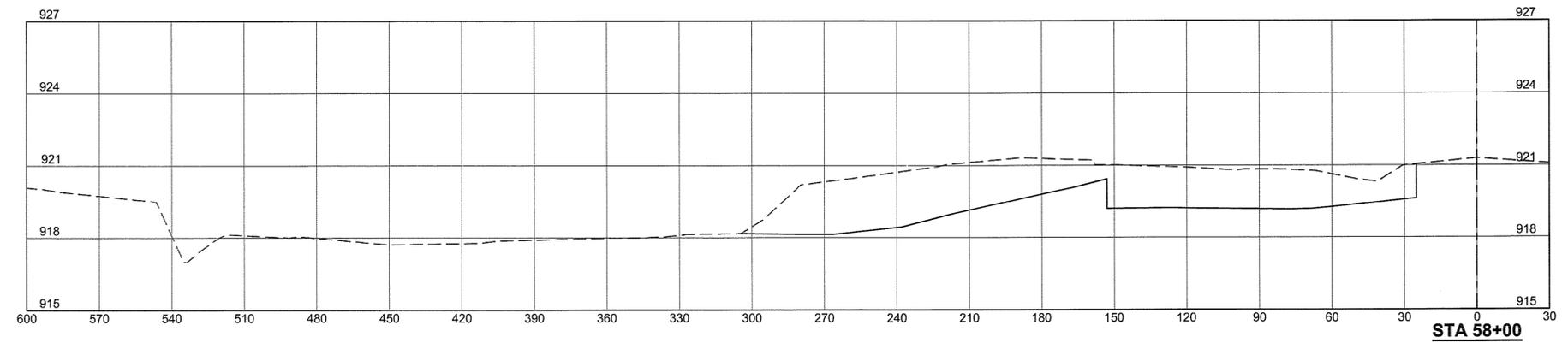
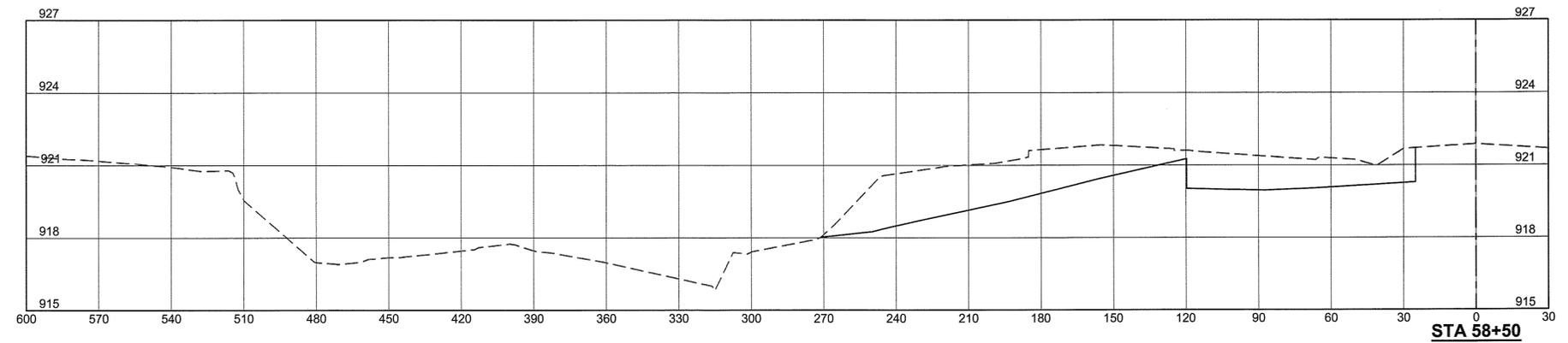
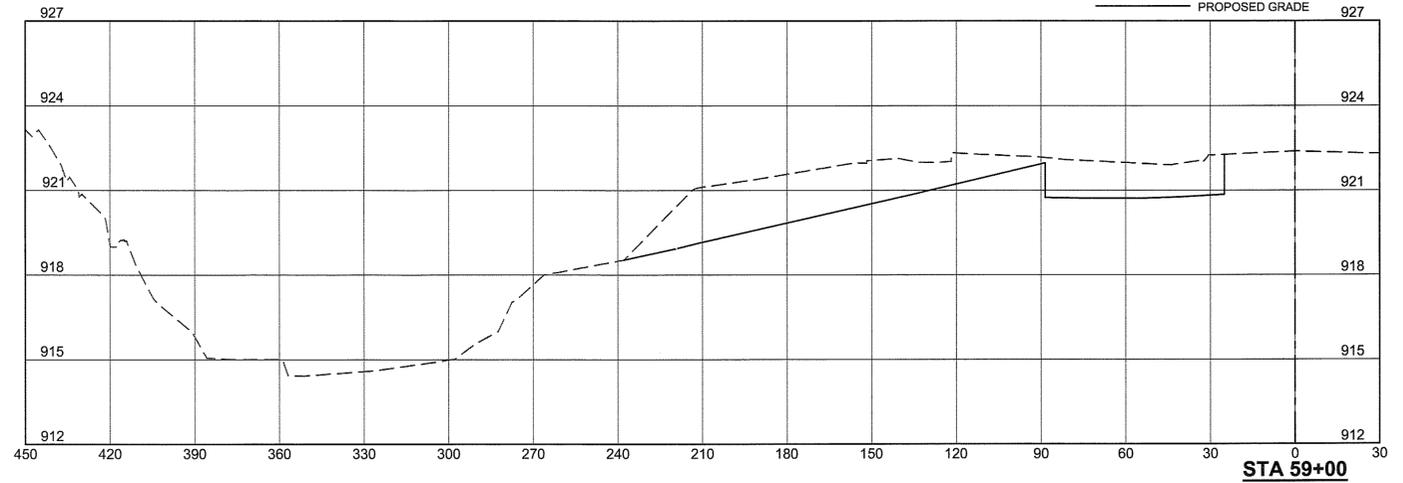
AIRFIELD PAVEMENT REHABILITATION-PHASE 2		AIP NO. 3-51-0029-036 (PENDING)	JOB NO. 12121
BL 'B' CROSS SECTIONS - STA 50+00 TO STA 54+50		DRAWN BY: DWS	SHEET 62
LYNCHBURG REGIONAL AIRPORT		DESIGNED BY: TJS	OF 69
SCALE: HORIZ: 1" = 30' VERT: 1" = 3'	DATE: MAY 2013		

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LEGEND

NOTE: CROSS SECTIONS REFLECT BITUMINOUS PAVEMENT SECTIONS (SCHEDULE 1).

--- EXISTING GRADE
 — PROPOSED GRADE

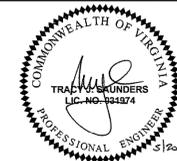


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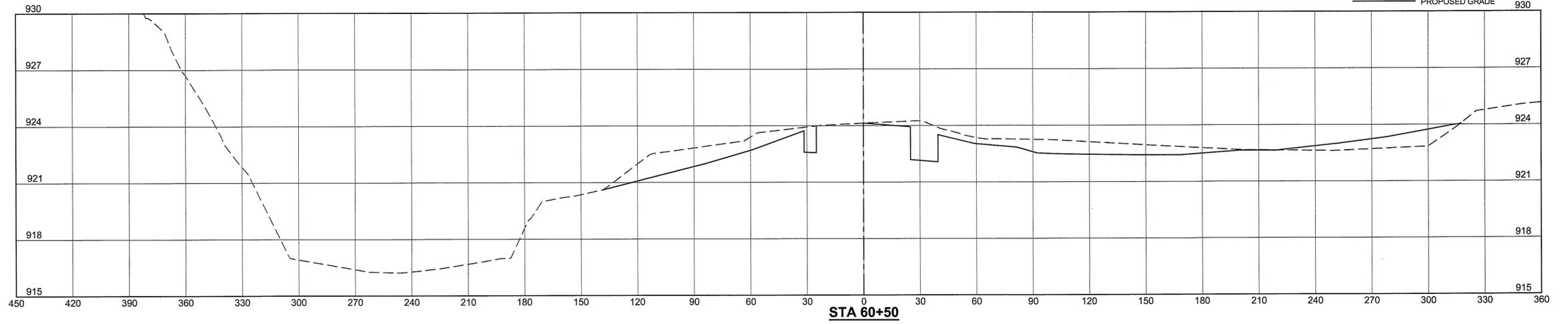
AIRFIELD PAVEMENT REHABILITATION-PHASE 2		AIP NO. 3-51-0029-036 (PENDING)	JOB NO. 12121
BL 'B' CROSS SECTIONS - STA 57+50 TO STA 59+00		DRAWN BY: DWS	SHEET 64
LYNCHBURG REGIONAL AIRPORT		DESIGNED BY: TJS	OF 69
SCALE: HORIZ: 1" = 30' VERT: 1" = 3'	DATE: MAY 2013		

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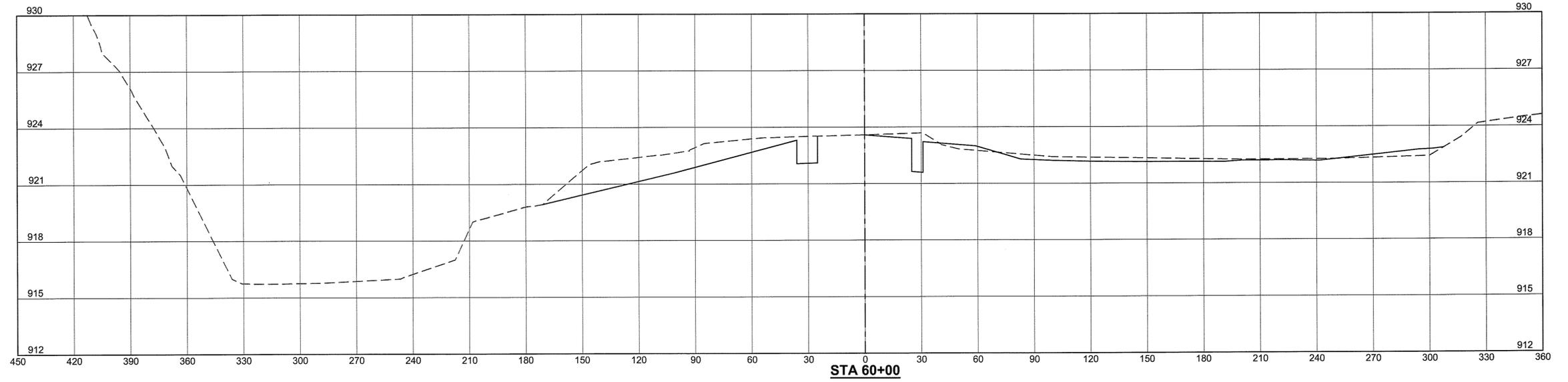
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NOTE: CROSS SECTIONS REFLECT BITUMINOUS PAVEMENT SECTIONS (SCHEDULE 1).

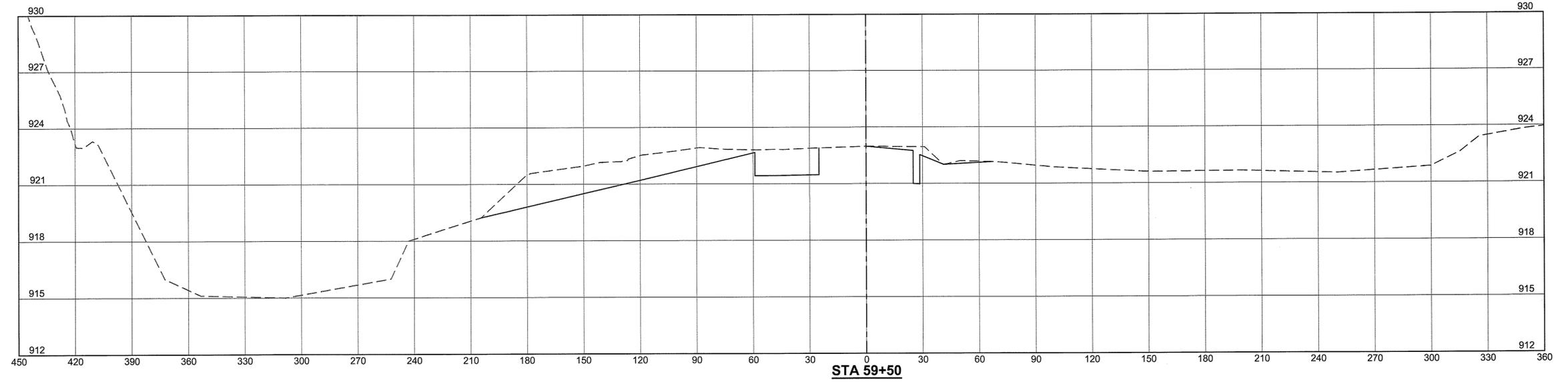
--- EXISTING GRADE
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STA 60+50



STA 60+00



STA 59+50

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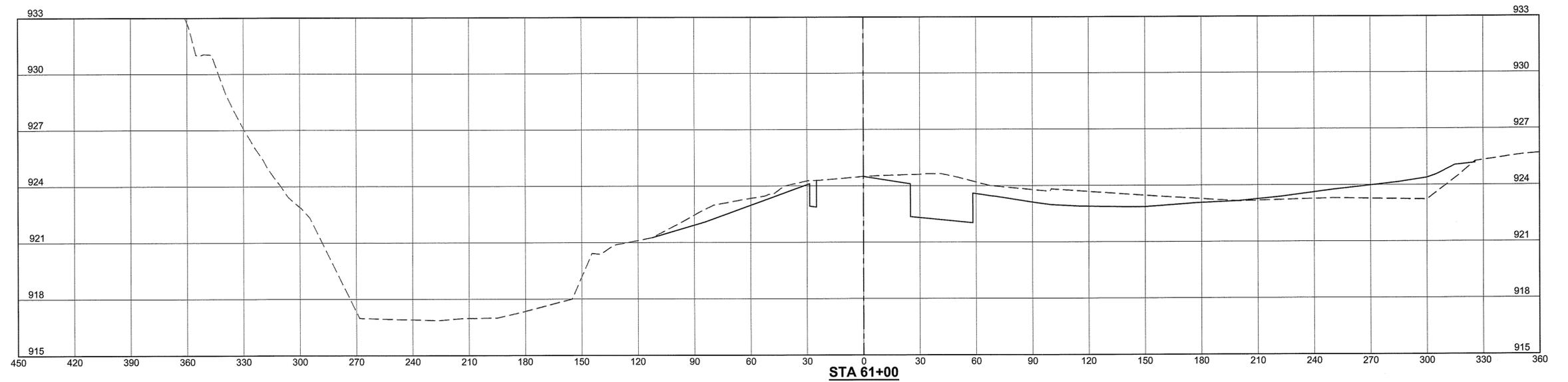
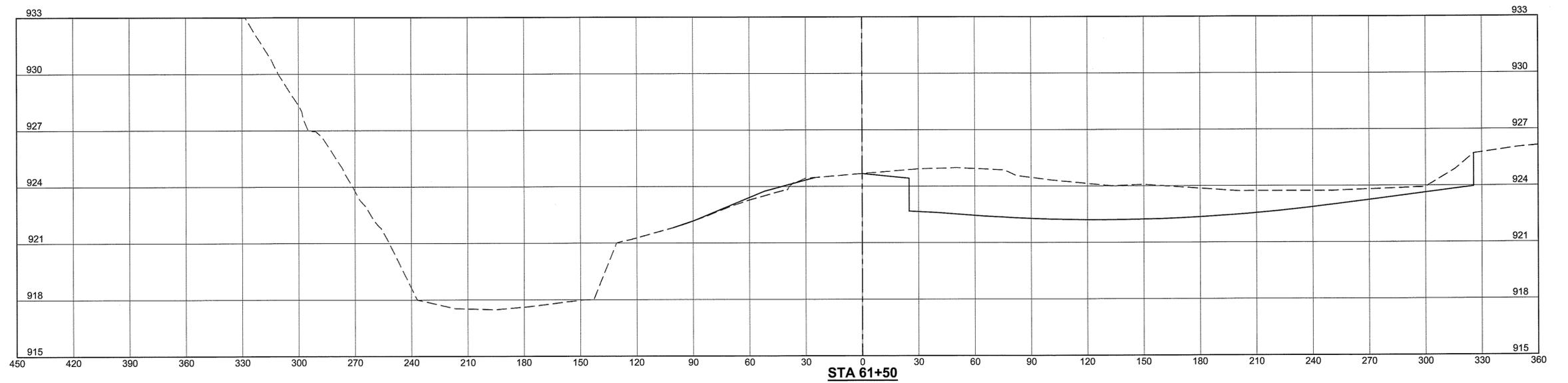
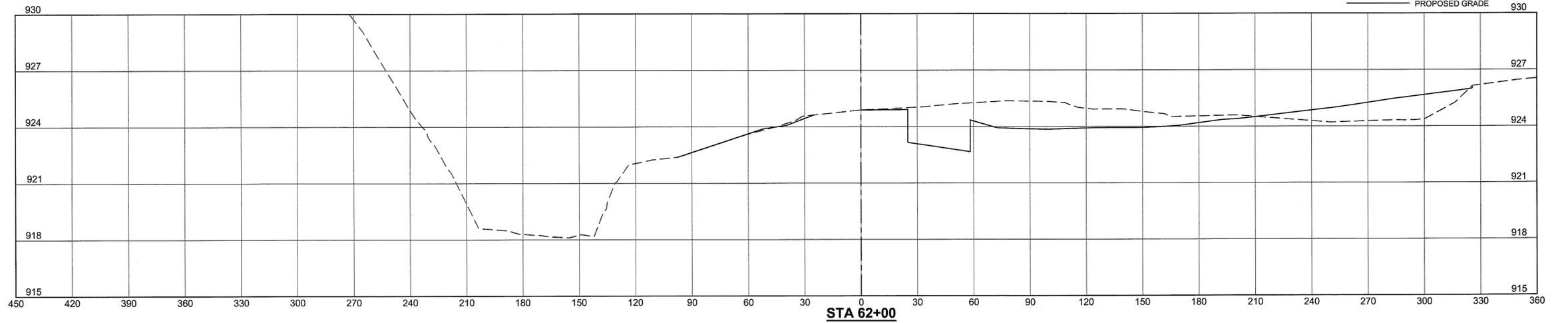
AIRFIELD PAVEMENT REHABILITATION-PHASE 2		AIP NO. 3-51-0029-036 (PENDING)	JOB NO. 12121
BL 'B' CROSS SECTIONS - STA 59+50 TO STA 60+50		DRAWN BY: DWS	SHEET 65
LYNCHBURG REGIONAL AIRPORT		DESIGNED BY: TJS	OF 69
SCALE: HORIZ: 1" = 30' VERT: 1" = 3'	DATE: MAY 2013		

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LEGEND

NOTE: CROSS SECTIONS REFLECT BITUMINOUS PAVEMENT SECTIONS (SCHEDULE 1).

--- EXISTING GRADE
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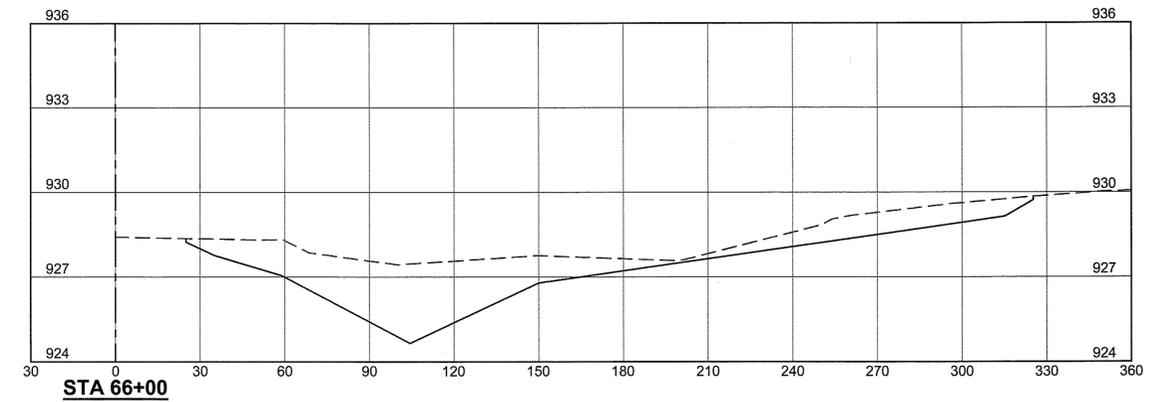
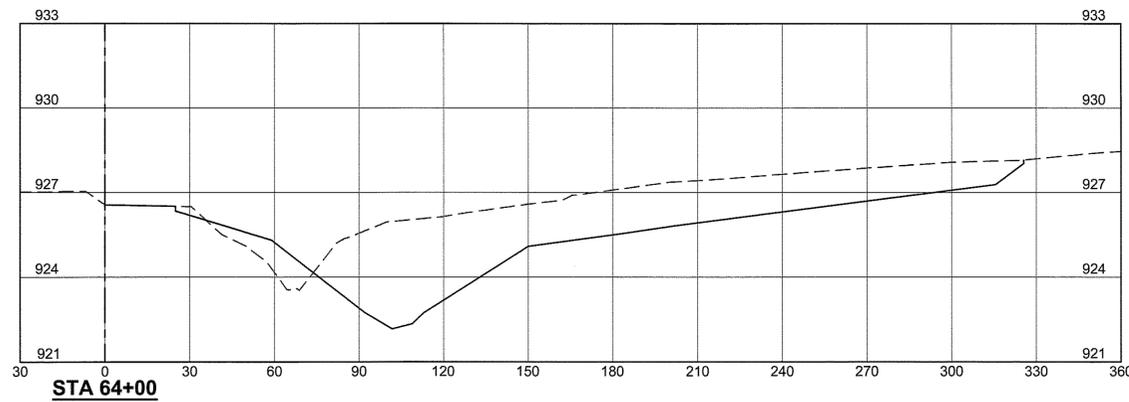
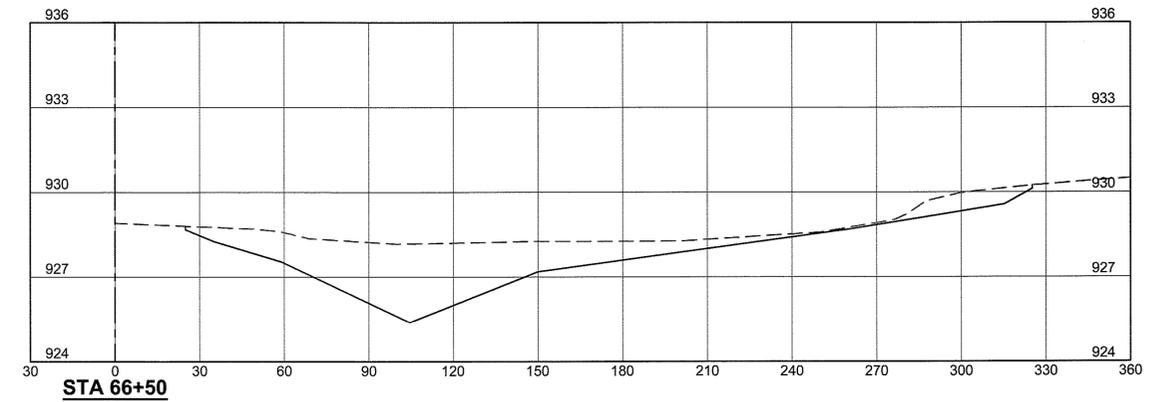
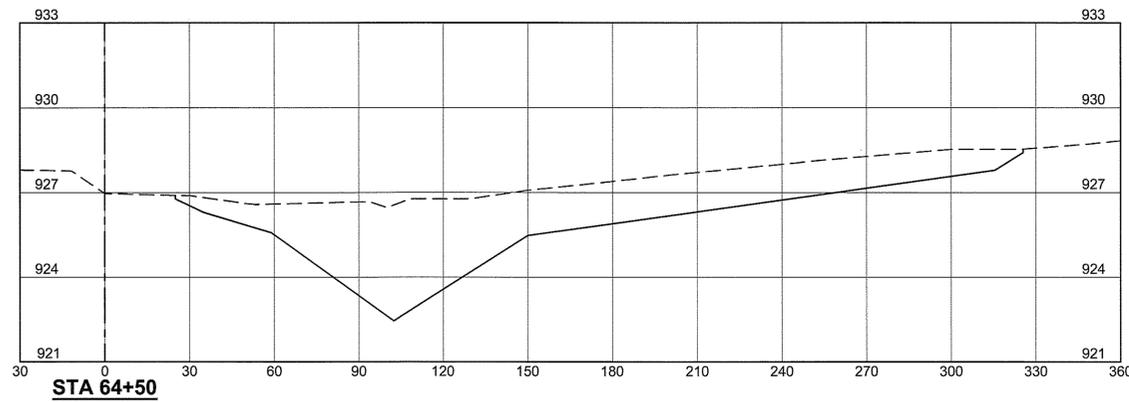
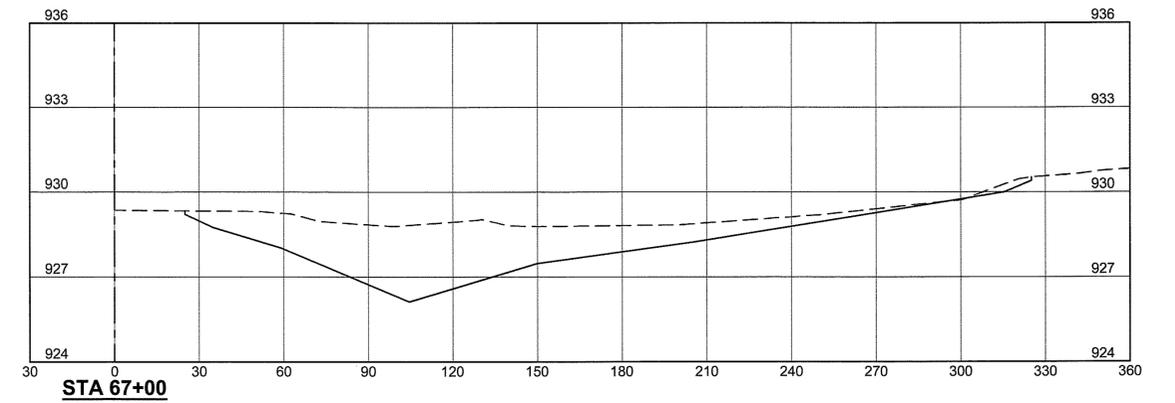
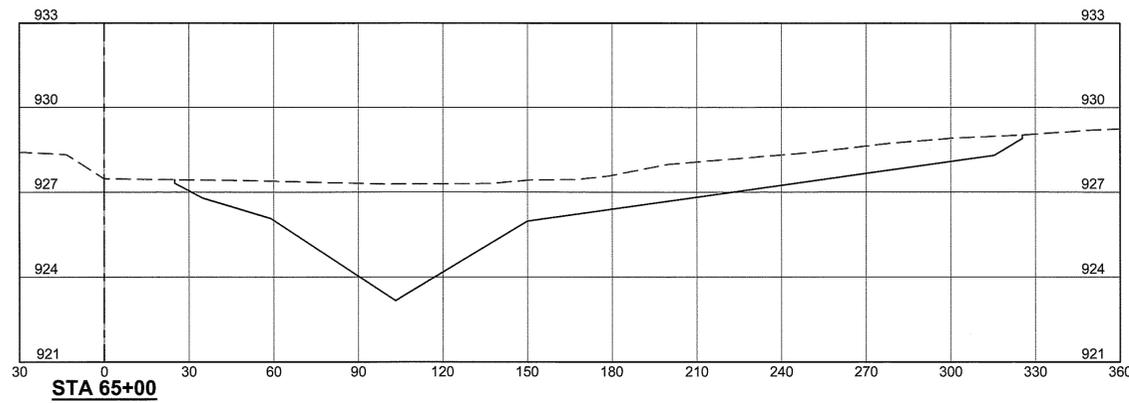
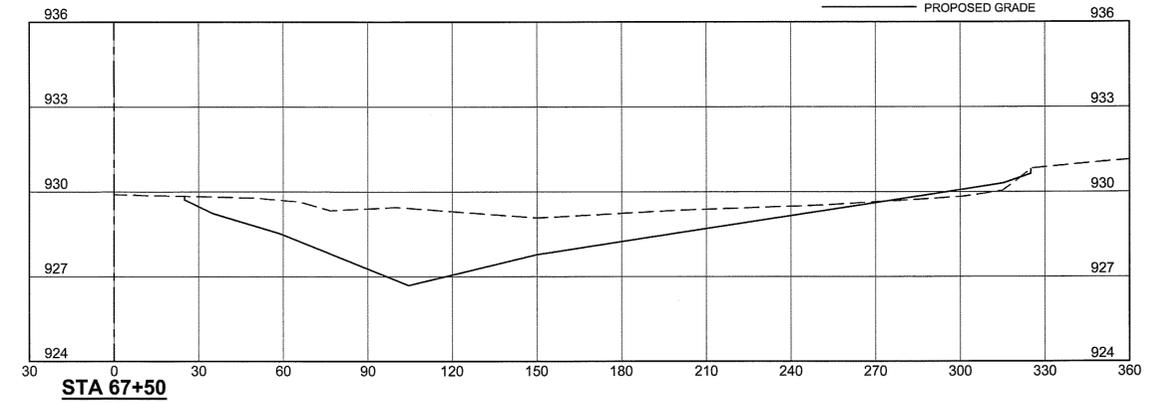
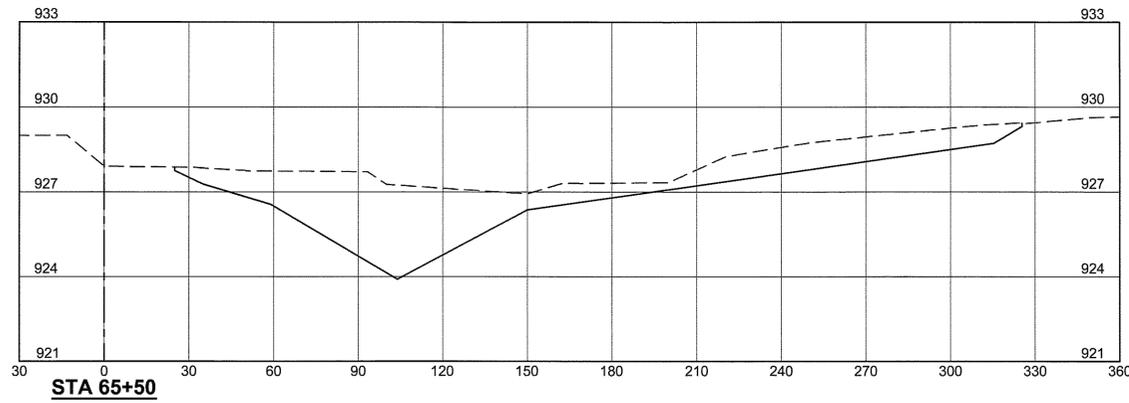
AIRFIELD PAVEMENT REHABILITATION-PHASE 2		AIP NO. 3-51-0029-036 (PENDING)	JOB NO. 12121
BL 'B' CROSS SECTIONS - STA 61+00 TO STA 62+00		DRAWN BY: DWS	SHEET 66
LYNCHBURG REGIONAL AIRPORT		DESIGNED BY: TJS	OF 69
SCALE: HORIZ: 1" = 30' VERT: 1" = 3'	DATE: MAY 2013		

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LEGEND

NOTE: CROSS SECTIONS REFLECT BITUMINOUS PAVEMENT SECTIONS (SCHEDULE 1).

--- EXISTING GRADE
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AIRFIELD PAVEMENT REHABILITATION-PHASE 2		AIP NO. 3-51-0029-036 (PENDING)	JOB NO. 12121
BL 'B' CROSS SECTIONS - STA 64+00 TO STA 67+50		DRAWN BY: DWS	SHEET 68
LYNCHBURG REGIONAL AIRPORT		DESIGNED BY: TJS	OF 69
SCALE: HORIZ: 1" = 30' VERT: 1" = 3'	DATE: MAY 2013		

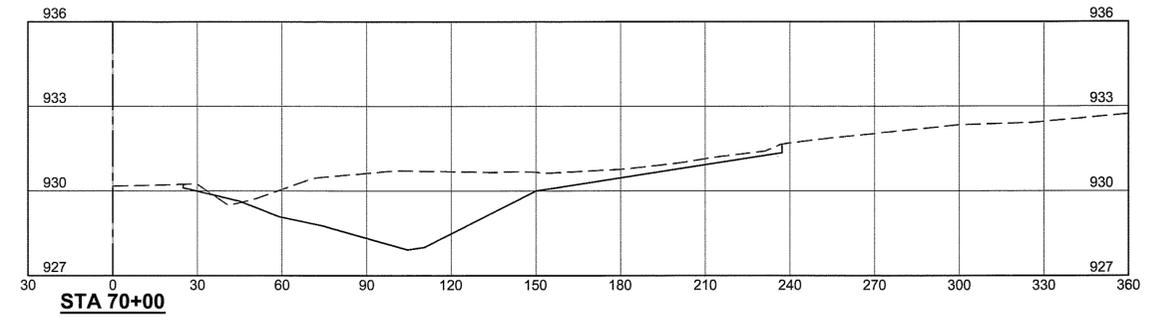
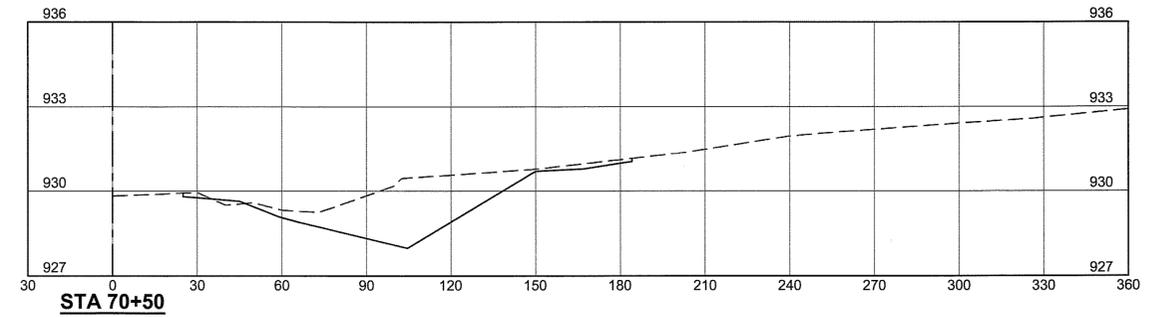
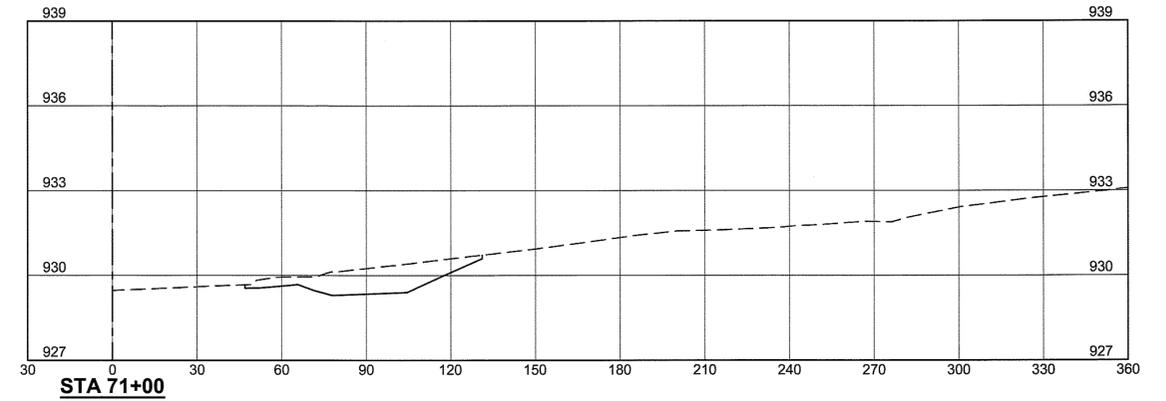
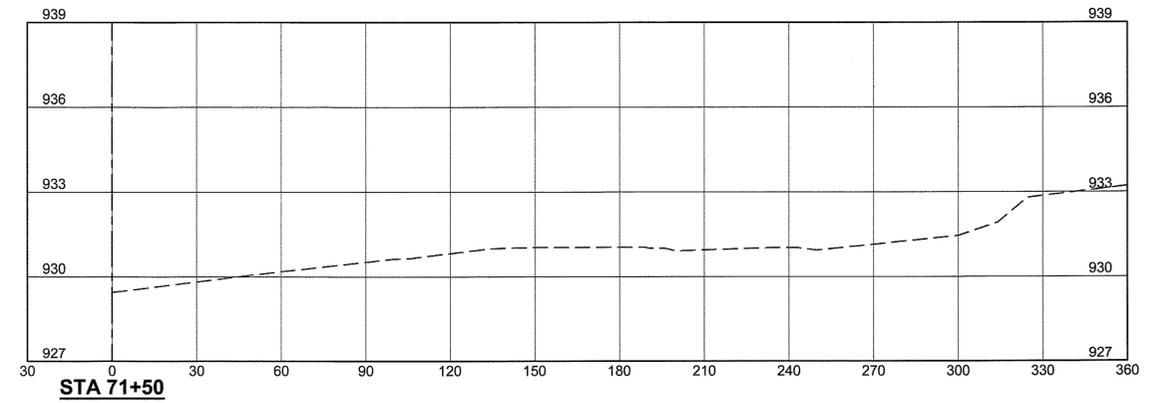
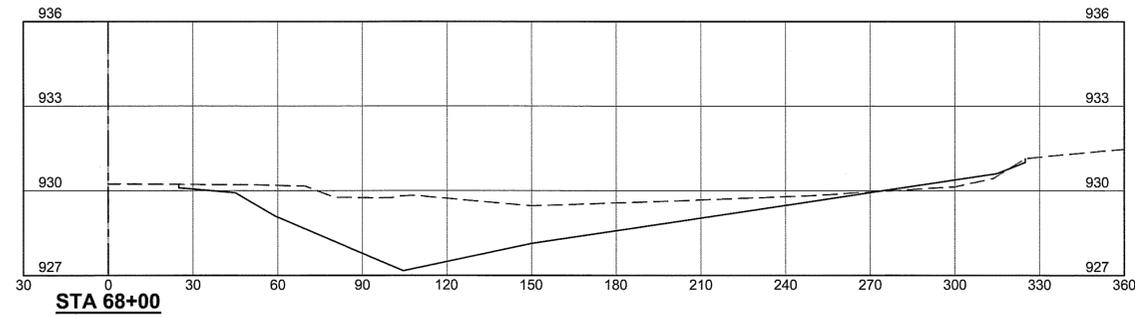
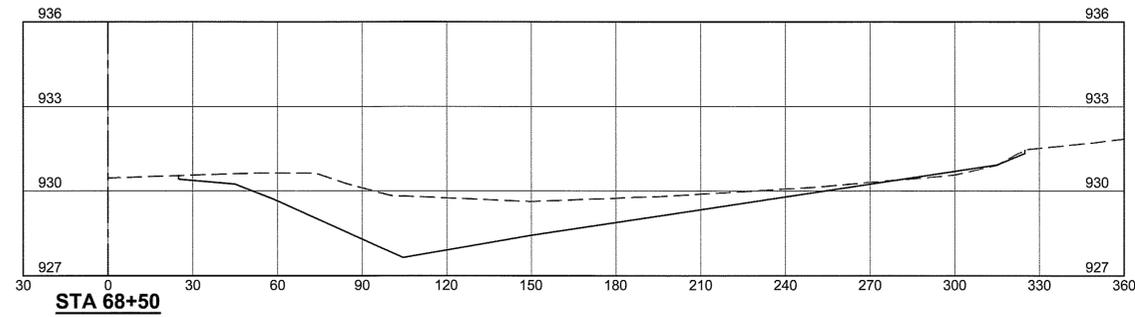
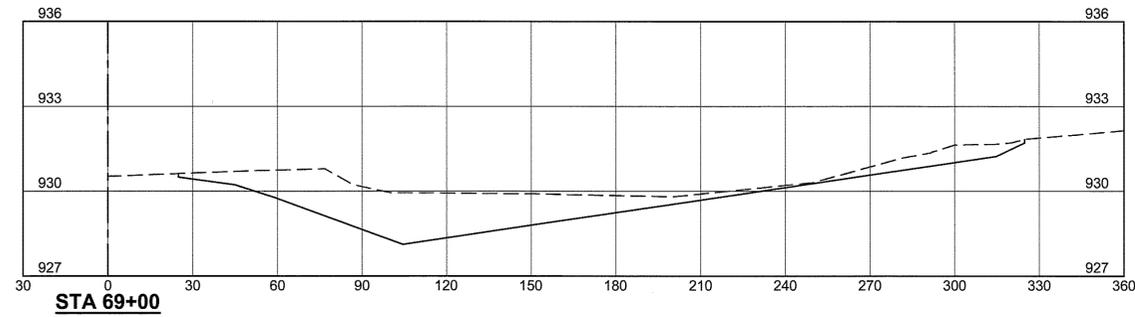
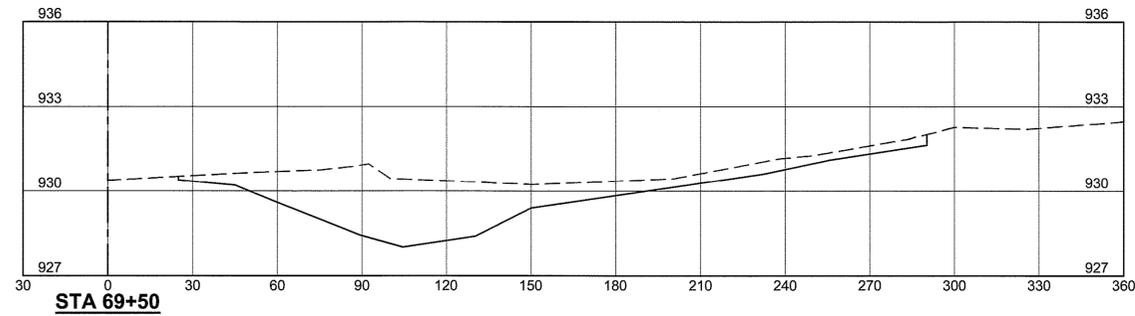
NO.	REVISIONS	BY	APP.	DATE

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LEGEND

NOTE: CROSS SECTIONS REFLECT BITUMINOUS PAVEMENT SECTIONS (SCHEDULE 1).

--- EXISTING GRADE
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AIRFIELD PAVEMENT REHABILITATION-PHASE 2		AIP NO. 3-51-0029-036 (PENDING)	JOB NO. 12121
BL 'B' CROSS SECTIONS - STA 68+00 TO STA 71+50		DRAWN BY: DWS	SHEET 69
LYNCHBURG REGIONAL AIRPORT		DESIGNED BY: TJS	OF 69
SCALE: HORIZ: 1" = 30' VERT: 1" = 3'	DATE: MAY 2013		

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