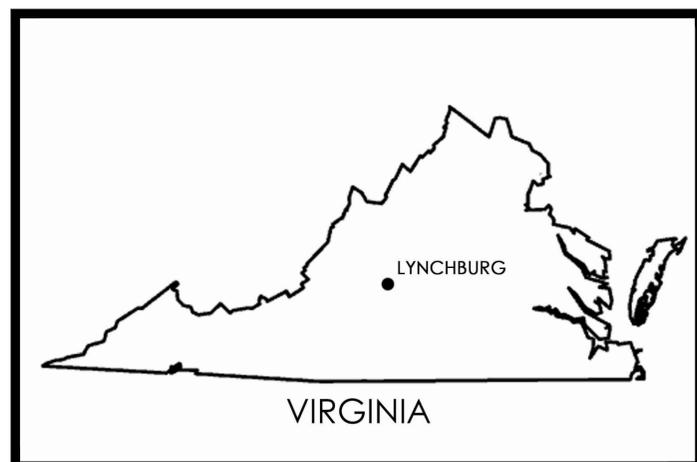


LYNCHBURG REGIONAL AIRPORT

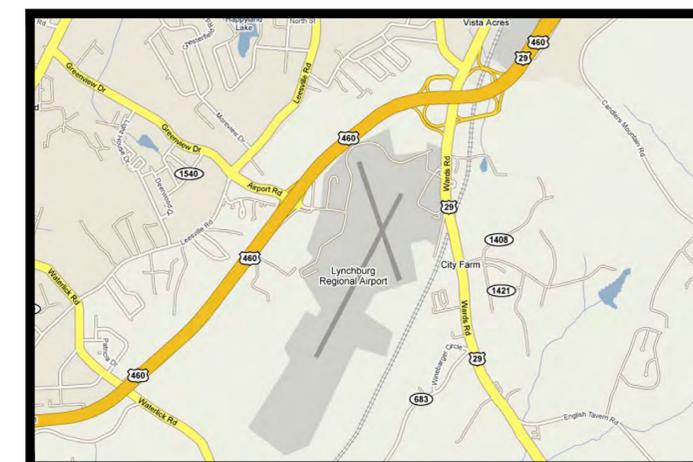
LYNCHBURG, VIRGINIA

NEW AIR TRAFFIC CONTROL TOWER



VICINITY MAP

RS&H PROJECT NUMBER: 222-0264-001
FAA AIP NUMBER: TBD



LOCATION MAP

LYNCHBURG REGIONAL AIRPORT
4308 WARDS ROAD
SUITE 100
LYNCHBURG, VA 24502
PHONE: (434) 455-6090
FAX: (434) 239-9027

AIRPORT DIRECTOR: MARK COURTNEY



Reynolds, Smith & Hills, Inc.

909 N WASHINGTON STREET, SUITE 330
ALEXANDRIA, VIRGINIA 22314
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www.rsandh.com
ALEXANDRIA VA - VIRGINIA REGISTRATION NO 0411-000594

**AUGUST 2015
BID DOCUMENTS**

**FOR BIDDING
PURPOSES ONLY**

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C002	2	INDEX OF DRAWINGS	S002	48	TYPICAL SECTIONS AND DETAILS	TE200	97	ANTENNA LAYOUT PLAN AND DETAILS
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			P102	69	THIRD AND FOURTH FLOOR PLANS - PLUMBING	C-AB-020	119	AS-BUILTS, H-3, HEATING, AIR CONDITIONING & VENTILATION
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			P501	71	PLUMBING DETAILS	C-AB-022	121	AS-BUILTS, E-2, ELECTRICAL
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4308 WARDS ROAD
 SUITE 100
 LYNCHBURG, VA 24502

AIR TRAFFIC CONTROL TOWER

REVISIONS		
NO.	DESCRIPTION	DATE

DATE ISSUED: AUGUST 12, 2015
 REVIEWED BY: NJP
 DRAWN BY: SPC
 DESIGNED BY: SPC

AEP PROJECT NUMBER
 222-0264-001

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 SHEET TITLE

INDEX OF SHEETS

SHEET NUMBER
C002

BID DOCUMENTS

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**SUMMARY OF
 QUANTITIES**

SHEET NUMBER
C003

**BID
 DOCUMENTS**

Item No.	Item Description	Unit	Quantity	Change Order				Final Quantity
Schedule A - ATC Tower								
P-001-1	AIR TRAFFIC CONTROL TOWER	LS	1					
263213-1	EMERGENCY GENERATOR SYSTEM	LS	1					
024100-1	DEMO OF EXISTING ATCT	LS	1					
Schedule B - ATCT Equipment								
275000-1	ATCT EQUIPMENT PACKAGE	LS	1					
Schedule C - Sitework and Utilities								
P-100.3-1	MOBILIZATION AND GENERAL CONDITIONS	LS	1					
P-102-10.1	SAFETY AND SECURITY	LS	1					
P-104-5.1	PROJECT SURVEY AND STAKEOUT	LS	1					
P-105-5.1	TEMPORARY CONSTRUCTION ITEMS	LS	1					
P-156-5.1a	DIVERSION DIKE	LFT	310					
P-156-5.1b	SILT FENCE	LFT	385					
P-156-5.1c	INLET / OUTLET PROTECTION	EACH	3					
024119-1	ASPHALT PAVEMENT REMOVAL	SYD	90					
033000-1	30" CURB & GUTTER (CG-6)	LF	110					
033000-1	SIDEWALK	SYD	160					
034100-1	CONCRETE WHEEL STOP	EA	2					
334100-1	12-INCH CLASS III RCP	LFT	70					
334100-2	24-INCH CLASS III RCP	LFT	120					
334100-3	8-INCH HDPE STORM PIPE	LFT	20					
334100-4	6-INCH HDPE STORM PIPE	LFT	85					
334100-5	INLET	EA	2					
334100-6	STORM MANHOLE	EA	1					
334100-7	STORM PIPE CLEAN-OUT STACK	EA	2					
334100-8	CONVERT MANHOLE TO INVERT	EA	1					
321723-1	HANDICAP PARKING SIGN	EACH	2					
321723-2	PAINTED PAVEMENT MARKING (WHITE OR BLUE)	SFT	200					
321723-3	ROADWAY DELINEATOR	EACH	1					
321723-4	24" SOLID WHITE THERMOPLASTIC	LFT	32					
329300-1	LANDSCAPE	LS	1					
331113-1	6-INCH DI CLASS 50 WATER LINE & FITTINGS	LFT	350					
331113-2	3-INCH RPZ BACKFLOW PREVENTER	EACH	1					
331113-3	6 X 6 TAPPING SLEEVE AND VALVE	EACH	1					
333000-1	4-INCH SANITARY SEWER LINE	LFT	100					
F-162-5.1	8-FOOT HIGH CHAIN LINK FENCE WITH BARBED WIRE	LFT	580					
F-162-5.2a	VEHICLE GATE	EACH	1					
F-162-5.2b	PEDESTRIAN GATE	EACH	1					
F-162-5.3	CHAIN LINK FENCE REMOVAL	LFT	210					
T-901-5.1	SEEDING	ACRE	1.2					
T-905-5.1	TOPSOILING, OBTAINED ON SITE OR REMOVED FROM STOCKPILE	CYD	540					
T-908-5.1	MULCHING	ACRE	1.2					
L-101-5.1	AIRPORT ROTATING BEACON	LS	1.0					
L-105-7.1	REMOVE AND RELOCATE BEACON	LS	1					
012100-1	AEP UTILITY ALLOWANCE	ALLOW	1					
012100-2	VERIZON UTILITY ALLOWANCE	ALLOW	1					
Section D - Airfield Lighting & Vault Modifications								
L-108-5.1	3-1/C NO. 6 AWG, L-824C CABLE, INSTALLED IN DUCT BANK OR CONDUIT	LFT	1,130					
L-108-5.2	AIRFIELD LIGHTING CONTROL CABLE, 25 PR	LFT	270					
L-110-5.1	ELECTRICAL CONDUIT 2-WAY, 2" PVC, SCHEDULE 40	LFT	180					
L-110-5.2	ELECTRICAL CONDUIT 2-WAY, 3" PVC, SCHEDULE 40	LFT	40					
L-110-5.3	ELECTRICAL CONDUIT 4-WAY, 2-3", 2-4" PVC, SCHEDULE 40	LFT	490					
L-115-5.1	ELECTRICAL HANDHOLE - VEHICLE RATED	EACH	7					
L-127-5.1	AIRFIELD LIGHTING CONTROL AND MONITORING SYSTEM MODS	LS	1					
Schedule E - Communication and Technology Package (CTP) Allowance								
012100-3	COMMUNICATION AND TECHNOLOGY PACKAGE (CTP) ALLOWANCE	ALLOW	1					
Schedule F - Alternate #1: Split Face Thin Block Finish								
012300-1	SPLIT FACE THIN BLOCK FINISH	LS	1					

FOR BIDDING ONLY-NOT FOR CONSTRUCTION

SAFETY

- THE CONTRACTOR SHALL ACQUAINT HIS SUPERVISORS AND EMPLOYEES OF THE AIRPORT ACTIVITY AND OPERATIONS THAT ARE INHERENT TO LYNCHBURG REGIONAL AIRPORT AND SHALL CONDUCT THE CONSTRUCTION ACTIVITIES TO CONFORM TO ALL ROUTINE AND EMERGENCY AIR TRAFFIC REQUIREMENTS AND GUIDELINES ON SAFETY AS SPECIFIED IN SAFETY NOTE 11. CONTRACTOR SHALL SUBMIT A SAFETY PLAN TO THE AIRPORT WHICH WILL BE MONITORED ACCORDINGLY. CONSTRUCTION SHALL NOT COMMENCE UNTIL THIS PLAN IS APPROVED BY THE AIRPORT. THE SAFETY PLAN SHALL BE SUBMITTED PRIOR TO THE NTP.
- ALL CONTRACTOR VEHICLES THAT ARE AUTHORIZED TO OPERATE ON THE AIRPORT IN OR NEAR THE ACTIVE AIRCRAFT OPERATIONS AREA (AOA) SHALL BE REQUIRED TO HAVE THEIR COMPANY LOGO ON BOTH SIDES OF THE VEHICLE AND SHALL DISPLAY IN FULL VIEW A FLASHING AMBER (YELLOW) DOME-TYPE LIGHT OR ABOVE THE VEHICLE A 3' X 3' OR LARGER, ORANGE AND WHITE CHECKERBOARD FLAG, EACH CHECKERBOARD COLOR BEING 1-FOOT SQUARE. (SEE CONSTRUCTION SAFETY FLAG DETAIL, THIS SHEET), AND ESCORTED UNDER THE CONTROL OF ONE CONTRACTOR MOBILE (TWO-WAY) RADIO OPERATOR ON THE JOB AT ALL TIMES. ANY VEHICLE OPERATING IN OR NEAR THE ACTIVE AOA DURING THE HOURS OF DARKNESS SHALL BE EQUIPPED WITH A FLASHING AMBER (YELLOW) DOME LIGHT, MOUNTED ON TOP OF THE VEHICLE AND BE OF SUCH INTENSITY TO CONFORM TO LOCAL CODES FOR MAINTENANCE AND EMERGENCY VEHICLES. ALL RADIO EQUIPPED VEHICLES SHALL COORDINATE ATCT CALL SIGNS WITH AIRPORT OPERATIONS PRIOR TO OPERATING ONSITE.
- ALL NON-RADIO EQUIPPED CONTRACTOR VEHICLES THAT ARE REQUIRED TO OPERATE ON, NEAR OR ACROSS ACTIVE RUNWAYS, TAXIWAYS, APRONS, AND RUNWAY APPROACH AND PROTECTION ZONES SHALL DO SO UNDER THE DIRECT CONTROL OF A RADIO EQUIPPED ESCORT VEHICLE. ESCORT VEHICLES, OPERATORS, AND RADIO EQUIPMENT SHALL BE APPROVED BY THE AIRPORT. ESCORT VEHICLES SHALL BE MARKED AND LIGHTED AS DESCRIBED IN PARAGRAPH NO. 2 ABOVE AND SHALL BE EQUIPPED WITH AN APPROVED AVIATION BAND RADIO. RADIO EQUIPPED VEHICLES SHALL CONTINUOUSLY MONITOR GROUND CONTROL FREQUENCY 121.9 MHz. ALL AIRCRAFT TRAFFIC ON RUNWAYS, TAXIWAYS AND APRONS SHALL HAVE PRIORITY OVER CONTRACTOR'S TRAFFIC. RADIOS SHALL BE PROVIDED BY THE CONTRACTOR.
- NO RUNWAY, TAXIWAY, APRON OR AIRPORT ROADWAY SHALL BE CLOSED WITHOUT WRITTEN APPROVAL OF AIRPORT OPERATIONS STAFF. TO ENABLE NECESSARY "NOTICES TO AIRMEN" (NOTAM) OR ADVISORIES TO AIRPORT SERVICES OR TENANTS, A MINIMUM OF [48] HOURS WRITTEN NOTICE REQUESTING CLOSING SHALL BE DIRECTED TO THE RESIDENT PROJECT REPRESENTATIVE (RPR) AND/OR ENGINEER WHO WILL COORDINATE THE REQUEST WITH AIRPORT OPERATIONS.
- ANY CONSTRUCTION ACTIVITY WITHIN 250 FEET OF AN ACTIVE RUNWAY CENTERLINE OR ACTIVE GROUP III TAXIWAY EDGE, OR OPEN EXCAVATIONS IN EXCESS OF THREE INCHES DEEP WITHIN THE ABOVE AREAS, WILL REQUIRE CLOSURE OF THE AFFECTED RUNWAY OR TAXIWAY, UNLESS OTHERWISE APPROVED BY AIRPORT OPERATIONS. CLOSURE REQUIRES THE SAME PROVISIONS AS PARAGRAPH NO. 4 ABOVE.
- OPEN FLAME WELDING OR TORCH CUTTING OPERATIONS ARE PROHIBITED UNLESS APPROVED BY THE RPR AND/OR AIRPORT OPERATIONS STAFF. IF APPROVED, ADEQUATE FIRE AND SAFETY PRECAUTIONS MUST BE TAKEN.
- STOCKPILED MATERIAL SHALL BE CONSTRAINED IN A MANNER TO PREVENT MOVEMENT RESULTING FROM AIRCRAFT JET BLAST OR WIND CONDITIONS IN EXCESS OF TWENTY MILES PER HOUR. STOCKPILE HEIGHT SHALL BE LESS THAN 15 FEET AND OUTSIDE THE RUNWAY AND TAXIWAY OBJECT FREE AREAS. IN ADDITION, STOCKPILED MATERIAL SHALL HAVE SILT FENCE LOCATED AROUND THE MATERIAL TO PREVENT FOD FROM MOVING ONTO THE AIRFIELD PAVEMENTS OR POLLUTING WATERCOURSES. STOCKPILED MATERIAL SHALL NOT PENETRATE THE 7:1 TRANSITIONAL SLOPE OF THE RUNWAYS.
- OPEN TRENCHES, EXCAVATIONS AND STOCKPILED MATERIAL LOCATED IN OR NEAR THE AOA SHALL BE PROMINENTLY MARKED WITH APPROVED FLAGS AND LOW-PROFILE BARRICADES AND LIGHTED BY APPROVED LIGHT UNITS DURING HOURS OF RESTRICTED VISIBILITY AND DARKNESS.
- DEBRIS, WASTE AND LOOSE MATERIAL CAPABLE OF CAUSING DAMAGE TO AIRCRAFT LANDING GEARS, PROPELLERS OR BEING INGESTED IN JET ENGINES SHALL NOT BE ALLOWED ON OR NEAR ACTIVE AIRCRAFT MOVEMENT AREAS. IF THESE MATERIALS ARE OBSERVED TO BE ON OR NEAR ACTIVE AIRCRAFT MOVEMENT AREAS, THEY WILL BE REMOVED IMMEDIATELY AND CONTINUOUSLY DURING CONSTRUCTION. IF THESE MATERIALS ARE OBSERVED AT ANYTIME, THE CONTRACTOR SHALL REMOVE THEM IMMEDIATELY.
- THE ENGINEER WILL ARRANGE WITH AIRPORT OPERATIONS FOR INSPECTION PRIOR TO OPENING FOR AIRCRAFT USE ANY AIRFIELD PAVEMENT THAT HAS BEEN CLOSED FOR WORK ON, OR ADJACENT THERETO, OR THAT HAS BEEN USED AS A CROSSING POINT OR HAUL ROUTE BY THE CONTRACTOR. THE CONTRACTOR SHALL NOTIFY THE ENGINEER ONCE WORK HAS BEEN COMPLETED AND THE AREA READY FOR AIRCRAFT TRAFFIC SO THE ENGINEER CAN COORDINATE THE INSPECTION WITH AIRPORT OPERATIONS.
- THE CONTRACTOR IS DIRECTED TO COMPLY WITH AND ACQUAINT HIS/HER EMPLOYEES WITH THE FOLLOWING SAFETY GUIDELINES, RELATED MATERIALS AND FAA ADVISORY CIRCULARS (CURRENT VERSION):
 - 150/5200-18 "AIRPORT SAFETY SELF-INSPECTION"
 - 150/5210-5 "PAINTING, MARKING & LIGHTING OF VEHICLES USED ON AIRPORTS"
 - 150/5370-2 "OPERATIONAL SAFETY ON AIRPORTS DURING CONSTRUCTION"

COPIES OF THESE DOCUMENTS ARE AVAILABLE ONLINE AT:
[HTTP://WWW.FAA.GOV/AIRPORTS/RESOURCES/ADVISORY_CIRCULARS/](http://www.faa.gov/airports/resources/advisory_circulars/)

- CONSTRUCTION DURING THE PROJECT MAY BE HALTED AT ANY TIME BY THE RESIDENT PROJECT REPRESENTATIVE, ENGINEER AND/OR AIRPORT OPERATIONS STAFF IF IT IS DETERMINED TO BE IN THE BEST INTEREST OF AIRPORT OPERATIONS OR SAFETY. THE CONTRACTOR MAY BE DIRECTED TO REMOVE EQUIPMENT AND/OR EVACUATE THE SITE IN ORDER TO ENABLE AIRCRAFT OPERATIONS. NECESSARY EXTENSIONS IN CONTRACT TIME MAY BE GRANTED OR A STOP WORK ORDER WILL BE ISSUED DUE TO THESE DELAYS. HOWEVER, THERE WILL BE NO ADJUSTMENTS IN CONTRACT PRICE DUE TO THESE DELAYS.
- THE CONTRACTOR SHALL PREPARE SAFETY PLANS SPECIFIC TO DAYTIME AND NIGHTTIME CONSTRUCTION OPERATIONS, AS WELL AS A CONTINGENCY PLAN TO ADDRESS CASES OF ABNORMAL FAILURES OR UNEXPECTED DISASTERS. THE CONTRACTOR SHALL ALSO PREPARE A DESTRUCTIVE WEATHER PLAN TO SET FORTH GENERAL GUIDANCE AND INFORMATION FOR THE CONTRACTOR TO COORDINATE PREPAREDNESS PLANS WHEN DESTRUCTIVE WEATHER THREATENS THE AIRPORT. THE DESTRUCTIVE WEATHER PLAN FORM IS PROVIDED IN THE SPECIFICATIONS.
- HAUL ROUTES ON THE AIRFIELD SHALL BE DELINEATED WITH LIGHTED LOW-PROFILE BARRICADES. OTHER MEANS TO CLEARLY MARK THE ROUTES TO THE WORK SITE MAY BE APPROVED ONLY BY THE RPR, ENGINEER AND/OR AIRPORT OPERATIONS STAFF.
- ALL COMMUNICATION WITH THE AIR TRAFFIC CONTROL TOWER OR OTHER ELEMENTS OF THE AIRPORT SHALL BE THROUGH THE RPR, ENGINEER AND/OR AIRPORT OPERATIONS STAFF.
- THE CONTRACTOR SHALL TAKE ALL PRECAUTIONS NECESSARY TO INSURE THE SAFETY OF OPERATING AIRCRAFT AS WELL AS HIS OWN EQUIPMENT AND PERSONNEL. SPECIAL CONSIDERATIONS SHOULD BE GIVEN TO FLIGHT SCHEDULES AND MISCELLANEOUS AIRCRAFT OPERATIONS. THE CONTRACTOR SHALL OBEY ALL INSTRUCTIONS AS TO ROUTES TO BE TAKEN BY EQUIPMENT TRAVELING WITHIN THE AIRPORT AREA AND KEEP SUCH VEHICLES AND EQUIPMENT MARKED WITH THE SPECIFIED AIRPORT SAFETY FLAGS. THE CONTRACTOR SHALL MAKE HIS OWN ESTIMATE OF ALL DIFFICULTIES TO BE ENCOUNTERED. EQUIPMENT NOT IN OPERATION SHALL BE KEPT CLEAR OF ALL AIRFIELD PAVEMENTS, PERSONNEL SHALL NOT ENTER AREAS OF THE AIRPORT WHERE AIRCRAFT ARE OPERATING WITHOUT SPECIFIC PERMISSION.
- THE CONTRACTOR SHALL TAKE ALL STEPS NECESSARY TO PROTECT THE EXISTING RUNWAY AND TAXIWAY LIGHTS, FAA POWER AND COMMUNICATION CABLES, AND ALL OTHER UNDERGROUND CABLES AND UTILITIES DURING CONSTRUCTION TO ASSURE CONTINUOUS OPERATION OF LIGHTS AND NAVIGATIONAL AIDS. ANY DAMAGE TO EXISTING UTILITIES AND/OR INTERRUPTION OF AIRFIELD LIGHTS AND/OR NAVIGATIONAL AIDS MUST BE REPAIRED IMMEDIATELY BY THE CONTRACTOR AT NO COST TO THE OWNER.
- THE TERM CONTRACTOR MEANS THE PRIME CONTRACTOR, HIS/HER SUBCONTRACTORS, SUPPLIERS, MATERIAL MEN, EMPLOYEES, AND OTHERS WHO WILL PERFORM SERVICES FOR OR IN CONJUNCTION WITH THE PRIME CONTRACTOR.
- MATERIALS STORED OR STOCKPILED ON THE AIRPORT SHALL BE SO PLACED AND THE WORK SHALL AT ALL TIMES BE CONDUCTED AS TO CAUSE NO GREATER OBSTRUCTION TO AIR OR GROUND TRAFFIC THAN IS CONSIDERED NECESSARY BY THE ENGINEER, NO RUNWAY, TAXIWAY, OR RAMP AREA SHALL BE CLOSED OR OPENED EXCEPT BY EXPRESSED PERMISSION OF THE AIRPORT.
- THE CONTRACTOR SHALL PROVIDE, ERECT AND MAINTAIN ALL NECESSARY BARRICADES, SIGNS, DANGER SIGNALS, AND LIGHTS FOR THE PROTECTION OF THE WORK AND THE SAFETY OF THE PUBLIC FOR BOTH AIR AND GROUND GROUND TRAFFIC IN ACCORDANCE WITH THE SPECIFICATIONS AND FAA AC 150/5370-2F.
- CLOSED RUNWAYS OR TAXIWAYS SHALL BE BARRICADED OFF AT ALL INTERSECTIONS WITH ACTIVE RUNWAYS AND/OR TAXIWAYS. THE CONTRACTOR SHALL HAVE AN EMPLOYEE ON CALL 24-HOURS PER DAY FOR EMERGENCY MAINTENANCE OF AIRPORT HAZARD LIGHTING AND BARRICADES.

CONTRACTOR'S SAFETY AND SECURITY REQUIREMENTS

- THE AIRPORT OPERATIONS SUPERVISOR, WORKING THROUGH THE ENGINEER, SHALL AT ALL TIMES HAVE COMPLETE JURISDICTION OVER THE SAFETY OF ALL AIRCRAFT OPERATIONS DURING THE WORK. WHEREVER THE SAFETY OF AIR TRAFFIC IS CONCERNED, THE DECISIONS OF THE AIRPORT OPERATIONS SUPERVISOR OR HIS DESIGNATED REPRESENTATIVE, SHALL BE FINAL AS TO METHODS, PROCEDURES AND MEASURES USED.
- FOR ANY RESTRICTIONS TO AIRCRAFT OPERATIONS, THE AIRPORT OPERATIONS SUPERVISOR SHALL GIVE PROPER NOTICE TO THE NEAREST FAA FLIGHT SERVICE STATION PRIOR TO THE START OF WORK, AND FOR ANY SUBSEQUENT CHANGES NEEDED IN THE NOTAM WHICH MAY BE ISSUED DURING THE PERIOD OF WORK, THE CONTRACTOR MUST PROVIDE NOTIFICATION FIVE (5) DAYS IN ADVANCE OF ANY WORK THAT MAY RESTRICT AIRCRAFT OPERATIONS.
- THE CONTRACTOR SHALL LOCATE ALL EXISTING UNDERGROUND UTILITIES IN THE PROPOSED CONSTRUCTION AREAS. ANY DAMAGED UTILITIES SHALL BE IMMEDIATELY REPAIRED AT THE CONTRACTORS EXPENSE AND THE AIRPORT FACILITY MANAGER AND RPR SHALL BE IMMEDIATELY NOTIFIED. THE CONTRACTOR SHALL BE RESPONSIBLE FOR HAND DIGGING TO LOCATE FACILITY UNDERGROUND UTILITIES AND FOR PROTECTION OF THOSE UTILITIES THROUGHOUT THE PROJECT. A LAND DISTURBING PERMIT SHALL BE ACQUIRED BY THE CONTRACTOR PRIOR TO THE START OF WORK.
- ANY MOVEMENT OF THE CONTRACTOR'S VEHICLES AND EQUIPMENT ON OR ACROSS AIRFIELD PAVEMENTS SHALL ONLY BE AT CROSSING POINTS APPROVED BY THE ENGINEER AND AT TIMES THE AIRPORT DIRECTS. MOVEMENT WITHIN THE AOA SHALL ONLY BE MADE AFTER THE CONTRACTOR'S REPRESENTATIVE GETS APPROVAL BY USE OF RADIO CONTACT WITH THE AIR TRAFFIC CONTROL TOWER TO GET PERMISSION FOR MOVEMENT.
- THE CONTRACTOR SHALL CONFINE HIS PERSONNEL, EQUIPMENT, OPERATIONS AND TRAVEL TO THE RE WITHIN THE DEFINED WORK LIMITS SHOWN ON THE PLANS.
- THE CONTRACTOR SHALL INFORM ALL CONSTRUCTION PERSONNEL AS TO THE PROPER ROUTES, SPEEDS AND PROCEDURES FOR TRANSPORTING EQUIPMENT AND MATERIALS TO THE CONSTRUCTION SITE; AND ALL RESTRICTIONS TO MOVEMENT OF EQUIPMENT OR PERSONNEL WITHIN THE AIRCRAFT OPERATIONS AREA. ON A DAILY BASIS, AND MORE OFTEN IF NECESSARY, ALL PERSONNEL SHALL BE ADVISED OF ANY CHANGES IN AIRPORT OPERATIONS THAT MAY FURTHER RESTRICT MOVEMENT.
- ACCESS OR HAUL ROUTES SHALL BE EXISTING ROADWAYS TO THE EXTENT THAT THEY ARE AVAILABLE. THE CONTRACTOR SHALL CORRECT ANY DAMAGE TO THE ROADS USED AND SHALL RESTORE THOSE ROADS TO THE SAME OR BETTER CONDITION AS THEY EXISTED PRIOR TO THE START OF WORK. THE CONTRACTOR MAY ESTABLISH ADDITIONAL HAUL OR ACCESS ROUTES AT HIS OWN EXPENSE AND RESPONSIBILITY IF APPROVED BY THE AIRPORT. UPON COMPLETION OF THE WORK, ANY ADDITIONAL ROADS SHALL EITHER BE LEFT OR GRADED AS DIRECTED SO THAT THEY DO NOT IMPEDE THE EXISTING DRAINAGE OR ACCESS ROUTES.
- MEASURES SHALL BE ADOPTED TO PREVENT POTENTIAL POLLUTANTS FROM ENTERING ANY DRAINAGE SYSTEM OR WATERWAY. MATERIALS AND DEBRIS SHALL NOT BE STORED IN THE WORK AREA IN A MANNER THAT WOULD ALLOW THEM TO ENTER THE DRAINAGE SYSTEM AS A RESULT OF SPILLAGE, NATURAL RUNOFF OR FLOODING. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO IMMEDIATELY NOTIFY THE ENGINEER AND THE AIRPORT SHOULD THERE BE A SPILLAGE OF MATERIAL WHICH MIGHT CONTAMINATE THE DRAINAGE SYSTEM. IT SHALL ALSO BE THE CONTRACTOR'S RESPONSIBILITY TO REMOVE AND CLEAR UP SUCH SPILLAGE IN A MANNER ACCEPTABLE TO THE AIRPORT. MATERIAL SHALL BE SECURED SO THAT IT WILL NOT BE BLOWN BY THE WIND ONTO AIRFIELD SURFACES.
- SPECIAL ATTENTION TO DUST CONTROL WILL BE REQUIRED WHEN EARTHWORK OR HAULING OPERATIONS ARE IN PROGRESS OR WHEN WIND AND WEATHER CONDITIONS CAUSE EXCESSIVE BLOWING OF DUST. IN THIS REGARD THE CONTRACTOR SHALL APPLY WATER OR CALCIUM CHLORIDE SOLUTION TO THE AFFECTED SITES AS DIRECTED.
- AT THE END OF EACH DAY, ANY RUNWAY, TAXIWAY OR APRON WHICH IS NOT CLOSED TO AIRCRAFT AND WHICH HAS BEEN USED BY THE CONTRACTOR SHALL BE CLEANED BY BROOMING OR OTHER ACCEPTABLE METHODS APPROVED BY THE AIRPORT FACILITY MANAGER. ALL EQUIPMENT SHALL BE STORED OR MOVED SO THAT IT IS NO CLOSER THAN 500 FEET FROM THE EDGE OF AN ACTIVE RUNWAY OR 100 FEET FROM THE EDGE OF AN ACTIVE TAXIWAY OR APRON.
- VEHICLES WITHIN THE SECURITY FENCE SHALL BE VISIBLY IDENTIFIABLE AS CONTRACTOR VEHICLES WHICH HAVE BEEN PROPERLY CLEARED FOR ENTRY AS SPECIFIED IN SAFETY NOTE 2 AND SECURITY NOTE 8.
- CONSTRUCTION EQUIPMENT SHALL HAVE A MAXIMUM HEIGHT OF FIFTEEN (15) FEET, EXCEPT AS AUTHORIZED BY THE ENGINEER.
- THE CONTRACTOR SHALL INSTALL ALL REQUIRED BARRICADES AT DESIGNATED PLAN LOCATIONS, HAVE ALL ACCESS GATES GUARDED AND LOCKABLE, HAVE ALL EQUIPMENT EITHER FLAGGED OR FITTED WITH FLASHING AMBER DOME-TYPE LIGHT ON TOP OF THE VEHICLES, INSTALL ALL TEMPORARY LIGHTING AND/OR SIGNS. ALL THESE ITEMS SHALL CONSIST OF THE SAFETY AND SECURITY SYSTEM. THE CONTRACTOR SHALL INSTALL THE COMPONENTS OF THE SYSTEM AT THE APPROPRIATE TIMES AS SPECIFIED IN THE CONTRACT DOCUMENTS. THE CONTRACTOR SHALL INSPECT EVERY ASPECT OF THE SAFETY AND SECURITY SYSTEM ON A DAILY BASIS AND ENSURE ALL COMPONENTS ARE FUNCTIONING PROPERLY. THE RESIDENT PROJECT REPRESENTATIVE (OWNERS REPRESENTATIVE) SHALL ALSO DAILY INSPECT THE SYSTEM AND IF ANY DEFICIENCIES ARE NOTED, THE CONTRACTOR SHALL HAVE THAT DAY'S PRORATED SAFETY AND SECURITY COST PERMANENTLY DEDUCTED FROM THE CONTRACTOR'S EARNINGS. THE SYSTEM ELEMENTS TO BE INSPECTED AND DEFICIENCIES NOTED ARE AS FOLLOWS:
 - *BARRICADES SET PROPERLY AND ALL FLASHING WARNING LIGHTS OPERATING PROPERLY.
 - *ALL CONTRACTOR PERSONNEL AND EQUIPMENT ACCESS GATES MANNED AND SECURITY PROCEDURES IN PLACE.
 - *ALL EQUIPMENT FLAGGED OR OUTFITTED WITH FLASHING AMBER DOME-TYPE LIGHTS.
 - *CONTRACTOR USE OF UNAUTHORIZED AIRPORT ACCESS GATES CHECKED.
 - *INSTALLATION OF ALL TEMPORARY LIGHTING AND/OR SIGNAGE.

ANY OF THE ABOVE SAFETY AND SECURITY ITEMS FOUND TO BE DEFICIENT AT THE BEGINNING OF THE DAY BY THE RPR WILL RESULT IN THAT DAY'S PRORATED SAFETY AND SECURITY BID ITEM LOST AND BEING DEDUCTED PERMANENTLY FROM THE CONTRACTOR'S EARNINGS. THE CONTRACTOR SHALL MAKE A CONCERTED EFFORT TO ENSURE ALL SAFETY AND SECURITY ITEMS ARE IN PROPER WORKING ORDER EACH DAY DUE TO THE HEIGHTENED SECURITY STATUS OF THE AIRPORT AND THE CONSIDERABLE LIABILITY ASSOCIATED WITH THE SAFETY AND SECURITY WORK.

SECURITY

- GENERAL INTENT:** IT IS INTENDED THAT THE CONTRACTOR SHALL COMPLY WITH ALL REQUIREMENTS OF THE AIRPORT SECURITY PLAN AND WITH THE SECURITY REQUIREMENTS SPECIFIED HEREIN BY THE AIRPORT. THE CONTRACTOR SHALL DESIGNATE TO THE RPR AND AIRPORT, IN WRITING, THE NAME OF HIS "CONTRACTOR SECURITY & SAFETY OFFICER (CSSO)." THE CSSO SHALL REPRESENT THE CONTRACTOR ON THE SECURITY REQUIREMENTS FOR THE CONTRACT. THE CSSO SHALL PROVIDE TELEPHONE NUMBERS WHICH ALLOW THE RPR, ENGINEER AND/OR AIRPORT OPERATIONS STAFF THE ABILITY TO CONTACT THE CSSO 24 HOURS A DAY.
- CONTRACTOR PERSONNEL SECURITY ORIENTATION:** THE CSSO SHALL BE RESPONSIBLE FOR BRIEFING ALL CONTRACTOR PERSONNEL ON SECURITY REQUIREMENTS. ALL NEW CONTRACTOR EMPLOYEES SHALL BE BRIEFED ON SECURITY REQUIREMENTS PRIOR TO WORKING IN THE CONSTRUCTION AREA.
- ACCESS TO THE SITE:** CONTRACTOR'S ACCESS TO THE SITE SHALL BE AS SHOWN ON THE PLANS. NO OTHER ACCESS POINTS SHALL BE ALLOWED UNLESS APPROVED BY THE AIRPORT. ALL CONTRACTOR TRAFFIC AUTHORIZED TO ENTER THE SITE SHALL BE EXPERIENCED IN THE ROUTE OR GUIDED BY AUTHORIZED CONTRACTOR PERSONNEL. THE CONTRACTOR SHALL BE RESPONSIBLE FOR TRAFFIC CONTROL TO AND FROM THE VARIOUS CONSTRUCTION AREAS ON THE SITE, AND FOR THE OPERATION AND SECURITY OF THE ACCESS GATE TO THE SITE. A CONTRACTOR'S FLAG MAN OR TRAFFIC CONTROL PERSON SHALL MONITOR AND COORDINATE ALL CONTRACTOR TRAFFIC AT THE ACCESS GATE WITH SECURITY. THE CONTRACTOR SHALL NOT PERMIT ANY UNAUTHORIZED CONSTRUCTION PERSONNEL OR TRAFFIC ON THE SITE. ACCESS GATES TO THE SITE SHALL BE LOCKED AND SECURED AT ALL TIMES WHEN NOT ATTENDED BY THE CONTRACTOR. IF THE CONTRACTOR CHOOSES TO LEAVE ANY ACCESS GATE OPEN, THEY MUST HAVE APPROVAL FROM THE AIRPORT AND IT SHALL BE ATTENDED BY CONTRACTOR PERSONNEL WHO ARE FAMILIAR WITH THE REQUIREMENTS OF THE AIRPORT OPERATIONS SECURITY PROGRAM. THE CONTRACTOR IS RESPONSIBLE FOR THE IMMEDIATE CLEANUP OF ANY DEBRIS DEPOSITED ALONG THE ACCESS ROUTE AS A RESULT OF HIS CONSTRUCTION TRAFFIC. DIRECTIONAL SIGNING FROM THE ACCESS GATE ALONG THE DELIVERY ROUTE TO THE STORAGE AREA, PLANT SITE OR WORK SITE SHALL BE AS DIRECTED BY THE AIRPORT. THE CONTRACTOR SHALL DAISY CHAIN ALL PERMANENT AND TEMPORARY ACCESS GATES TO THE PROJECT SITE AND STAGING AREAS.
- MATERIALS DELIVERY TO THE SITE:** ALL CONTRACTOR'S MATERIAL ORDERS FOR DELIVERY TO THE WORK SITE WILL USE AS A DELIVERY ADDRESS, THE STREET NAME ASSIGNED TO THE ACCESS POINT AT THE CONTRACTOR'S STAGING SITE AT THE AIRPORT. THE NAME "LYNCHBURG REGIONAL AIRPORT" SHALL NOT BE USED IN THE DELIVERY ADDRESS AT ANY TIME. THIS WILL PRECLUDE DELIVERY TRUCKS FROM ENTERING INTO THE TERMINAL COMPLEX, OR TAKING SHORT CUTS THROUGH THE PERIMETER GATES AND ENTERING INTO AIRCRAFT OPERATIONS AREAS INADVERTENTLY.

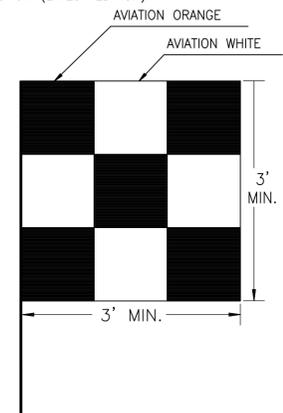
SECURITY (CONT.)

- CONSTRUCTION AREA LIMITS:** THE LIMITS OF CONSTRUCTION, MATERIAL STORAGE AREAS, PLANT SITE, EQUIPMENT STORAGE AREA, PARKING AREA AND OTHER AREAS DEFINED AS REQUIRED FOR THE CONTRACTOR'S EXCLUSIVE USE DURING CONSTRUCTION SHALL BE MARKED BY THE CONTRACTOR. THE CONTRACTOR SHALL ERECT AND MAINTAIN AROUND THE PERIMETER OF THESE AREAS SUITABLE FENCING, MARKING AND WARNING DEVICES VISIBLE FOR DAY/NIGHT USE. TEMPORARY BARRICADES, FLAGGING AND FLASHING WARNING LIGHTS WILL BE REQUIRED AT CRITICAL ACCESS POINTS. TYPE OF MARKING AND WARNING DEVICES SHALL BE APPROVED BY THE AIRPORT.
- IDENTIFICATION --- PERSONNEL:** ALL EMPLOYEES, AGENTS, VENDORS, INVITEES, ETC. OF THE CONTRACTOR OR SUBCONTRACTOR REQUIRING ACCESS TO THE CONSTRUCTION SITE SHALL BE REQUIRED TO DISPLAY CONTRACTOR ISSUED IDENTIFICATION AT ALL TIMES. IDENTIFIABLE HARD HATS OR OTHER IDENTIFICATION SHALL ALSO BE WORN AT ALL TIMES IF REQUIRED BY THE AIRPORT. THE CONTRACTOR AND HIS STAFF ARE RESPONSIBLE FOR ATTENDING A TRAINING PROGRAM WHICH WILL INCLUDE AIR/GROUND RADIO, TAXIWAY AND AIRPORT FAMILIARIZATION AND A RAMP DRIVING CLASS. ESTIMATED TIME FOR COMPLETION IS 1 HOUR. LYNCHBURG REGIONAL AIRPORT RESERVES THE RIGHT TO REQUIRE AIRPORT BADGING FOR CONTRACTOR'S EMPLOYEES DURING THE COURSE OF THIS CONSTRUCTION PROJECT. NO BADGE WILL BE ISSUED TO ANY PERSON UNTIL A REVIEW OF THE FAA REQUIRED PAPERWORK HAS BEEN COMPLETED BY AIRPORT SECURITY AND ALL REQUIREMENTS ARE MET. PAPERWORK SHALL BE SUBMITTED AS DIRECTED BY AIRPORT OPERATIONS STAFF BEFORE ISSUANCE OF ANY BADGE. AIRPORT OPERATIONS SHOULD BE NOTIFIED IMMEDIATELY IF A BADGE IS LOST. REPLACEMENT BADGES WILL REQUIRE A FEE.
- FINES:** PAYMENT OF ALL FINES ASSESSED TO LYNCHBURG REGIONAL AIRPORT DUE TO VIOLATIONS BY THE CONTRACTOR OF FAA REGULATIONS OR SAFETY REQUIREMENTS SHALL BE THE SOLE RESPONSIBILITY OF THE CONTRACTOR. IF THE RESTRICTED AREA GATE IS FOUND TO BE OPEN OR UNLOCKED AND UNATTENDED, AIRPORT SECURITY POLICE MY ISSUE THE CONTRACTOR A CITATION. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL COURT COSTS AND IMPOSED FINES. IN ADDITION, A \$1,000.00 CHARGE MAY BE LEVIED BY THE AIRPORT FOR EACH VIOLATION SO DOCUMENTED AND UPON FINAL PAYMENT THE TOTAL OF ANY SUCH CHARGES WILL BE DEDUCTED FROM MONEYS DUE THE CONTRACTOR.
- VEHICLE IDENTIFICATION:** THE CONTRACTOR, THROUGH THE CSSO, SHALL ESTABLISH AND MAINTAIN A LIST OF CONTRACTOR AND SUBCONTRACTOR VEHICLES AUTHORIZED TO OPERATE ON THE SITE. CONTRACTOR EMPLOYEE VEHICLES SHALL BE RESTRICTED TO THE CONTRACTOR'S EMPLOYEE PARKING AREA AND ARE NOT ALLOWED ON THE AOA AT ANY TIME. IN ADDITION, EACH CONTRACTOR VEHICLE ENTERING THE PROJECT SITE SHALL BE REQUIRED TO DISPLAY, ON BOTH SIDES OF THE VEHICLE EITHER PERMANENT OR MAGNETIC SIGNS THAT IDENTIFY THE NAME OF THE CONTRACTOR AND A FLASHING AMBER LIGHT OR A 3' X 3' ORANGE AND WHITE CHECKERBOARD FLAG, EACH CHECKERBOARD COLOR BEING 1-FOOT SQUARE.
- VEHICLE PARKING:** CONTRACTOR EMPLOYEE VEHICLES SHALL BE RESTRICTED TO THE CONTRACTOR'S EMPLOYEE PARKING AREA AND ARE NOT ALLOWED ON THE AOA AT ANY TIME.
- A MINIMUM OF 48 HOURS IN ADVANCE OF ANY EXCAVATION OR BORINGS, THE CONTRACTOR SHALL CONTACT THE AIRPORT FACILITY MANAGER TO VERIFY ALL UNDERGROUND CABLE LOCATION IN THE VICINITY OF THE PROPOSED WORK.

SAFETY PLAN NARRATIVE

GENERAL: THE SAFETY PLAN SHALL CONSIST OF SHEET C004, ALONG WITH REQUIREMENTS ESTABLISHED IN THE CONTRACT DOCUMENTS AND AS SET FORTH IN AC 150/5370-2, OPERATIONAL SAFETY ON AIRPORTS DURING CONSTRUCTION (LATEST EDITION).

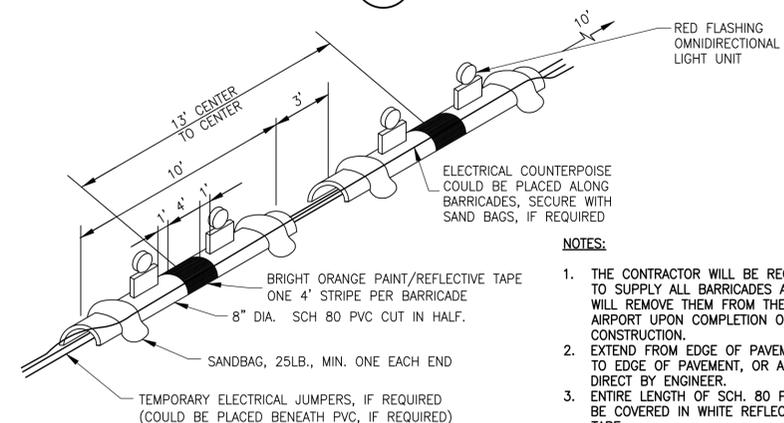
- THE CONTRACTOR SHALL FOLLOW ALL GUIDELINES AND REQUIREMENTS ESTABLISHED BY AUTHORITIES HAVING JURISDICTION (EITHER LOCAL, STATE OR FEDERAL) RELATING TO HAZARDOUS MATERIALS (HAZMAT) DURING THE LIFE OF THE CONSTRUCTION.
- THIS PROJECT INCLUDES THE CONSTRUCTION OF A NEW AIR TRAFFIC CONTROL TOWER, UTILITY AND PEDESTRIAN ACCESS CONNECTIONS TO THE NEW TOWER, AND DEMOLITION OF THE EXISTING AIR TRAFFIC CONTROL TOWER.
- AIRCRAFT OPERATIONS WILL CONTINUE DURING CONSTRUCTION. TYPE OF AIRCRAFT OPERATIONS DURING CONSTRUCTION: DESIGN GROUP I, II, III, AND IV, AND APPROACH CATEGORY A, B, C, AND D.
- BASIS FOR TEMPORARY DISPLACED THRESHOLDS: NOT APPLICABLE.
- BASIS FOR DEVIATING FROM STANDARDS: NOT APPLICABLE.
- TYPE AND HEIGHT (NOT-TO-EXCEED) OF CONSTRUCTION EQUIPMENT:
 1. TRUCKS (DUMP, FLATBED, PANEL, PICKUP, CONCRETE) - 15 FEET
 2. FRONT END LOADERS - 15 FEET
 3. DOZERS - 15 FEET
 4. CRANE - 100 FEET *
 5. ROLLERS AND COMPACTORS - 15 FEET
 * CONSTRUCTION EQUIPMENT LOCATIONS SHALL NOT VIOLATE RUNWAY 7 TO 1 TRANSITIONAL SURFACES AND RUNWAY APPROACH ZONE HEIGHT LIMITATIONS EXCEPT UNDER SPECIAL WAIVER CONDITIONS. APPROPRIATE WAIVERS MUST BE OBTAINED BY THE OWNER FROM FAA. CONTRACTOR SHALL NOTIFY AIRPORT OPERATIONS 48 HOURS PRIOR TO USING CRANE.
- DATES FOR START AND COMPLETION:
 - ESTIMATED START: OCTOBER 15, 2015
 - ESTIMATED COMPLETE: MAY 22, 2016
 - CONSTRUCTION TIME: 220 CALENDAR DAYS



NOTE: SAFETY FLAG SHALL BE PROMINENTLY DISPLAYED ON ALL CONSTRUCTION EQUIPMENT. (SEE NOTE 2 UNDER SAFETY)

1 C004 CONSTRUCTION SAFETY FLAG

SCALE: N.T.S.



2 C004 TYPE 1 LOW-PROFILE BARRICADE

SCALE: N.T.S.

THE ENGINEER AND/OR SURVEYOR TAKES NO RESPONSIBILITY FOR THE LOCATION OR ACCURACY OF THE UTILITIES AS SHOWN HEREON OR ANY UTILITIES WITHIN THE PROJECT THAT MAY NOT BE SHOWN HEREON. THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTACTING THE UTILITY COMPANIES TO SEE IF ANY UTILITIES EXIST WITHIN THE AREA OF THE PROJECT BEFORE ANY CONSTRUCTION BEGINS. ANY COST INCURRED BY DAMAGING ANY UTILITY WITHIN THE PROJECT SHALL BE AT THE EXPENSE OF THE CONTRACTOR.

48 WORKING HOURS PRIOR TO STARTING THE WORK, THE CONTRACTOR SHALL CALL MISS UTILITY AT PHONE NUMBER 811 AND ADVISE THE NATURE AND LOCATION OF THE WORK.

FOR BIDDING ONLY-NOT FOR CONSTRUCTION

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LYNCHBURG REGIONAL AIRPORT

4308 WARDS ROAD
 SUITE 100
 LYNCHBURG, VA 24502

AIR TRAFFIC CONTROL TOWER

REVISIONS		
NO.	DESCRIPTION	DATE

DATE ISSUED: AUGUST 12, 2015
 REVIEWED BY: NJP
 DRAWN BY: SPC
 DESIGNED BY: SPC

AEP PROJECT NUMBER
222-0264-001

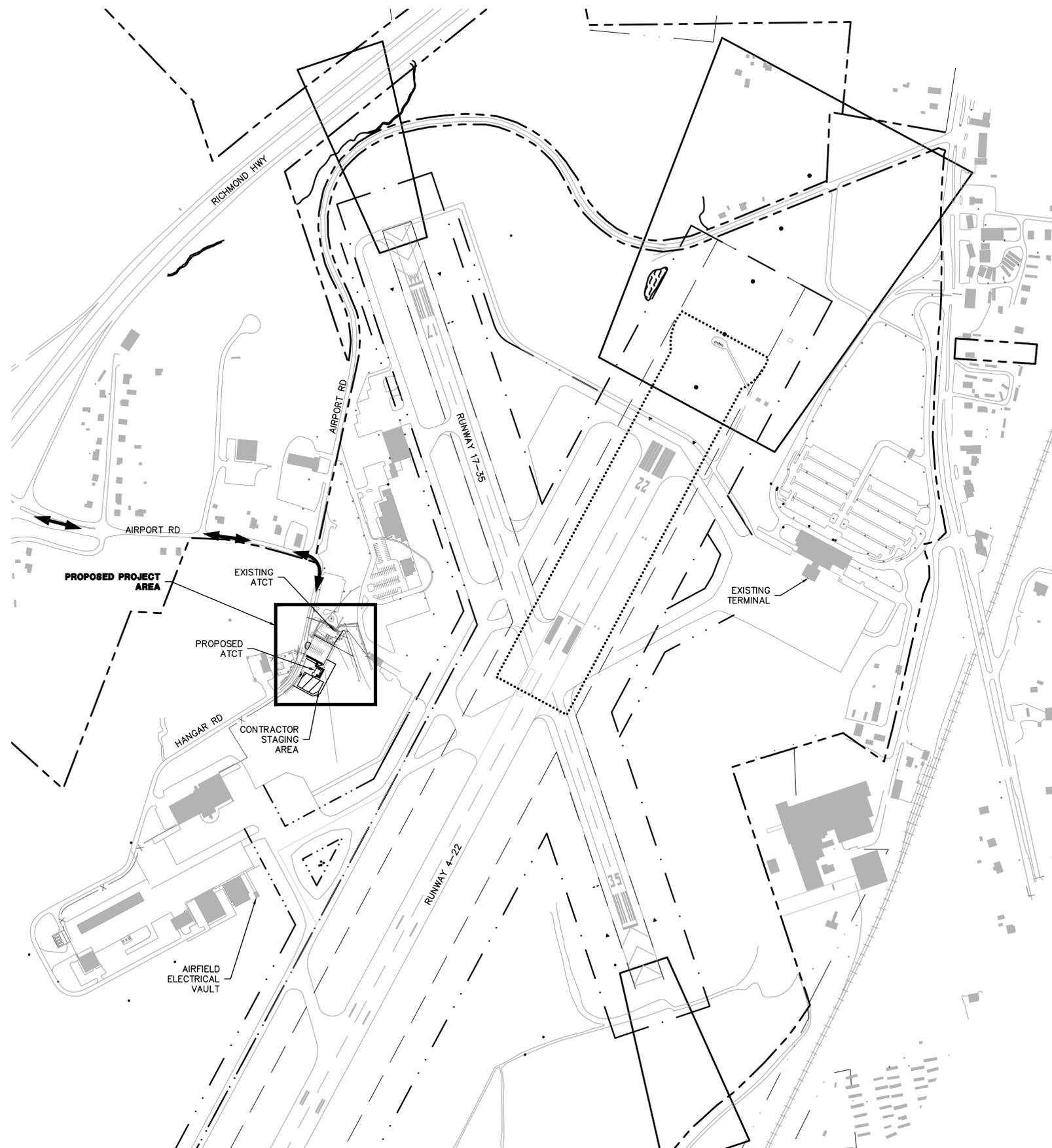
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 SHEET TITLE

SAFETY AND SECURITY NOTES AND DETAILS

SHEET NUMBER

C004

BID DOCUMENTS



GENERAL NOTES

- HAUL ROUTES:** LOCATION OF HAUL ROUTES ON THE AIRPORT SITE SHALL BE AS SPECIFIED ON THE PLANS OR AS APPROVED BY THE ENGINEER AND/OR AIRPORT OPERATIONS. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO COORDINATE OFF-SITE HAUL ROUTES (STATE HIGHWAYS, COUNTY ROADS OR CITY STREETS) WITH THE APPROPRIATE OWNER WHO HAS JURISDICTION OVER THE AFFECTED ROUTE. ON-SITE HAUL ROUTES SHALL BE MAINTAINED BY THE CONTRACTOR AND SHALL BE RESTORED TO THEIR ORIGINALLY CONSTRUCTED CONDITION UPON COMPLETION OF THE PROJECT. THE BEFORE AND AFTER CONDITION OF ON-SITE HAUL ROUTES SHALL BE JOINTLY INSPECTED AND DETERMINED BY THE CONTRACTOR, THE ENGINEER AND AIRPORT REPRESENTATIVES. FENCING, DRAINAGE, GRADING, DRAINAGE PIPE AND OTHER MISCELLANEOUS CONSTRUCTION REQUIRED TO CONSTRUCT TEMPORARY HAUL ROUTES OR ACCESS POINTS ON THE AIRPORT WILL BE THE CONTRACTOR'S TOTAL RESPONSIBILITY AND SHALL BE APPROVED BY AIRPORT OPERATIONS PRIOR TO THE WORK. ALL ON-SITE FAA ACCESS ROADS TO FAA FACILITIES SHALL REMAIN OPEN AND MAINTAINED AT ALL TIMES. PHOTOGRAPHS AND A VIDEO OF THE HAUL ROUTES, INCLUDING APRON AREAS, SPECIFIED BY THE PLANS MUST BE PROVIDED BY THE CONTRACTOR BEFORE AND AFTER CONSTRUCTION TO AIRPORT OPERATIONS. THE CONTRACTOR IS RESPONSIBLE FOR REPAIRING ANY DAMAGE TO HAUL ROUTES RESULTING FROM CONSTRUCTION TRAFFIC. ANY SERVICE, ACCESS OR FAA ROADWAY CROSSED BY CONSTRUCTION TRAFFIC SHALL BE PROTECTED AGAINST DAMAGE AND ALL DAMAGE OCCURRING WILL BE REPAIRED WITH NEW MATERIAL AT THE CONTRACTOR'S EXPENSE WITH NO ADDITIONAL COMPENSATION. THE CONTRACTOR SHALL DETERMINE THE DEPTH OF THE ASPHALT PAVEMENT LOCATIONS WHERE THE CONTRACTOR MUST CROSS TO GET TO THE CONSTRUCTION SITE. ANY EXISTING AIRFIELD CONCRETE PAVEMENTS DAMAGED BY CROSSING CONSTRUCTION EQUIPMENT SHALL BE REMOVED AND REPLACED TO AT LEAST 10 FEET ON EACH SIDE OF THE MOST EXTREME OUTER TIRE MARKS TO ENSURE THAT ALL DAMAGED CONCRETE OR ASPHALT PAVEMENTS TRAVERSED BY THE CONSTRUCTION EQUIPMENT IS REMOVED AND REPLACED. THE REMOVAL AND REPLACEMENT OF EXISTING CONCRETE OR ASPHALT PAVEMENT SHALL BE AT NO ADDITIONAL COST TO THE OWNER. ANY DAMAGED PAVEMENT WILL BE REPLACED WITH MATCHING PAVEMENT. CONSTRUCTION AND MAINTENANCE OF ON SITE HAUL ROUTES ARE INCLUDED UNDER ITEM P-105 TEMPORARY CONSTRUCTION ITEMS.
- WASTE DISPOSAL AND BORROW AREAS:** ALL CONSTRUCTION DEBRIS AND UNSUITABLE EXCAVATION WASTE MATERIAL REMOVED FROM THE CONSTRUCTION AREA SHALL BE LEGALLY DISPOSED OF OFF THE AIRPORT PROPERTY. NO MATERIAL SHALL BE WASTED ON THE AIRPORT SITE UNLESS APPROVED BY THE ENGINEER AND AIRPORT STAFF.
- CONTRACTOR UTILITIES:** STAGING AREAS DO NOT HAVE UTILITIES. ANY INSTALLATION AND REMOVAL OF UTILITIES REQUIRED BY THE CONTRACTOR SHALL BE COORDINATED WITH THE UTILITY COMPANIES AND SHALL BE THE SOLE RESPONSIBILITY OF THE CONTRACTOR. THE CONTRACTOR SHALL PROVIDE HIS/HER WATER FOR CONSTRUCTION ACTIVITIES OR WATER WILL ALSO BE AVAILABLE FROM THE AIRPORT FOR A FEE. THE CONTRACTOR SHALL PROVIDE TEMPORARY TOILET FACILITIES AT THE CONSTRUCTION SITE FOR THE USE BY HIS/HER EMPLOYEES AND SUB-CONTRACTORS THAT MEET ALL APPLICABLE REQUIREMENTS. CONTRACTOR EMPLOYEES SHALL NOT BE ALLOWED TO USE THE AIRPORT FACILITIES.
- PROTECTION AND REPAIR OF DAMAGE TO EXISTING CABLES:** LOCATION OF EXISTING FAA UNDERGROUND CABLES WILL BE FLAGGED ONE TIME BY THE LOCAL FAA AIRWAY FACILITIES SECTOR OFFICE PERSONNEL THROUGH COORDINATION WITH THE ENGINEER AND AIRPORT OPERATIONS. THESE FLAGS SHALL BE PROTECTED AND MAINTAINED BY THE CONTRACTOR AT ALL TIMES. IF FLAGS ARE LOST OR REMOVED BY THE CONTRACTOR, THEY WILL BE FLAGGED AGAIN AT THE CONTRACTOR'S EXPENSE. ALL UNDERGROUND CABLES SHALL BE PROTECTED AND DAMAGES REPAIRED EXPEDITIOUSLY AT THE CONTRACTOR'S EXPENSE AT NO ADDITIONAL COST TO THE OWNER.
- CONSTRUCTION LIMITS AND FLAGMEN:** ALL CONTRACTOR VEHICLES AND TRAFFIC SHALL REMAIN WITHIN THE DESIGNATED CONSTRUCTION LIMITS OR HAUL ROUTES. ABSOLUTELY NO CONTRACTOR VEHICLES WILL BE ALLOWED ON OTHER ACTIVE AIRFIELD OPERATIONS AREAS.
- PERMITS:** IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO OBTAIN AND PAY FOR ALL APPLICABLE PERMITS FOR CONSTRUCTION AND EQUIPMENT.
- COORDINATION OF CONSTRUCTION ACTIVITIES:** THE CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTAINING CONSTANT COORDINATION BETWEEN THE SUBCONTRACTORS IN ADDITION TO COORDINATION WITH THE ENGINEER AND AIRPORT STAFF. ALL CONSTRUCTION ACTIVITIES PLANNED BY THE CONTRACTOR SHALL BE REVIEWED AND APPROVED BY THE ENGINEER AND AIRPORT STAFF.
- AOA SPEED LIMIT:** THE CONTRACTOR SHALL OBEY AIRPORT SPEED LIMITS AS INDICATED BY AIRPORT OPERATIONS.
- UTILITIES:** IT WILL BE THE SOLE RESPONSIBILITY OF THE CONTRACTOR TO LOCATE AND PROTECT ANY PUBLIC UTILITIES THAT ARE IN OR ADJACENT TO THE WORK AREA. THE UTILITIES WILL BE FLAGGED ONE TIME BY THE VARIOUS UTILITY COMPANIES. THESE FLAGS SHALL BE PROTECTED AND MAINTAINED BY THE CONTRACTOR AT ALL TIMES. IF FLAGS ARE LOST OR REMOVED BY THE CONTRACTOR, THEY WILL BE FLAGGED AGAIN AT THE CONTRACTOR'S EXPENSE. ALL UTILITIES SHALL BE PROTECTED AND DAMAGES REPAIRED EXPEDITIOUSLY AT THE CONTRACTOR'S EXPENSE AT NO ADDITIONAL COST TO THE OWNER OR AIRPORT.
- EMPLOYEE PARKING:** NO CONTRACTOR EMPLOYEE VEHICLES WILL BE ALLOWED WITHIN THE AOA AREA. ALL EMPLOYEE AND VISITOR PARKING SHALL BE IN THE LOCATION DESIGNATED ON THE PLANS OR AS APPROVED BY AIRPORT OPERATIONS. CONTRACTOR SHALL NOT PARK OR STORE EQUIPMENT ON EXISTING ATCT PARKING LOT.
- TEMPORARY DRAINAGE:** THE CONTRACTOR SHALL PROVIDE AND INSTALL ANY DRAINAGE PIPE NECESSARY TO ENSURE THAT ANY HAUL ROUTE CONSTRUCTION SHALL NOT CREATE ANY PONDING WATER OR RESTRICT THE EXISTING DRAINAGE FLOW PATTERN. AT END OF PROJECT CONTRACTOR SHALL RESTORE ALL GRADES, PER DESIGN PLANS, AND REMOVE ALL TEMPORARY DRAINAGE PIPES AND FACILITIES AT NO ADDITIONAL COST TO OWNER.
- RESTORATION:** ALL BORROW, STOCKPILE STORAGE, HAUL ROUTE AREAS, AND AREAS DISTURBED WITHIN THE CONCOURSE A EXPANSION LIMITS SHALL BE RESTORED BY THE CONTRACTOR BY TOPSOILING, SEEDING, AND MULCHING PER THE SPECIFICATIONS. THIS WORK WILL BE CONSIDERED INCIDENTAL TO MOBILIZATION.
- SPILLAGE:** ANY SPILLAGE ON THE ROADWAYS OR AIRFIELD SHALL BE IMMEDIATELY CLEANED UP OR THE AIRPORT MAY DO SO AND REDUCE MONIES DUE TO THE CONTRACTOR TO USE EQUIPMENT AND PERSONNEL FOR THE CLEANUP.
- MAINTENANCE OF LOCAL TRAFFIC:** LOCAL TRAFFIC SHALL BE MAINTAINED AT ALL TIMES. A TRAFFIC CONTROL PLAN (TCP) SHALL BE PROVIDED BY THE CONTRACTOR PRIOR TO START UP OF CONSTRUCTION. THE TCP SHALL BE IN ACCORDANCE WITH THE MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES (MUTCD) PART VI, 2009 EDITION INCLUDING ALL REVISIONS AND THE VIRGINIA DEPARTMENT OF TRANSPORTATION (VDOT) ROADWAY STANDARD DRAWINGS. THE CONTRACTOR SHALL PROVIDE ALL TRAFFIC CONTROL DEVICES NECESSARY TO MAINTAIN LOCAL TRAFFIC FOR THE DURATION OF THE PROJECT. THE CONTRACTOR SHALL ENSURE THAT PERMANENT/TEMPORARY TRAFFIC CONTROL DEVICES DO NOT CONFLICT DURING THE CONSTRUCTION ACTIVITIES.
- CONSTRUCTION TRAILER AREA:** COORDINATE CONSTRUCTION TRAILER LOCATION AND DURATION OF PLACEMENT WITH AIRPORT STAFF PRIOR TO PLACEMENT OF TRAILER.

LEGEND

-  CONTRACTOR STAGING AREA
-  CONTRACTOR HAUL ROUTE



SCALE: 1" = 300'
 300' 150' 0 300'

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AIR TRAFFIC CONTROL TOWER

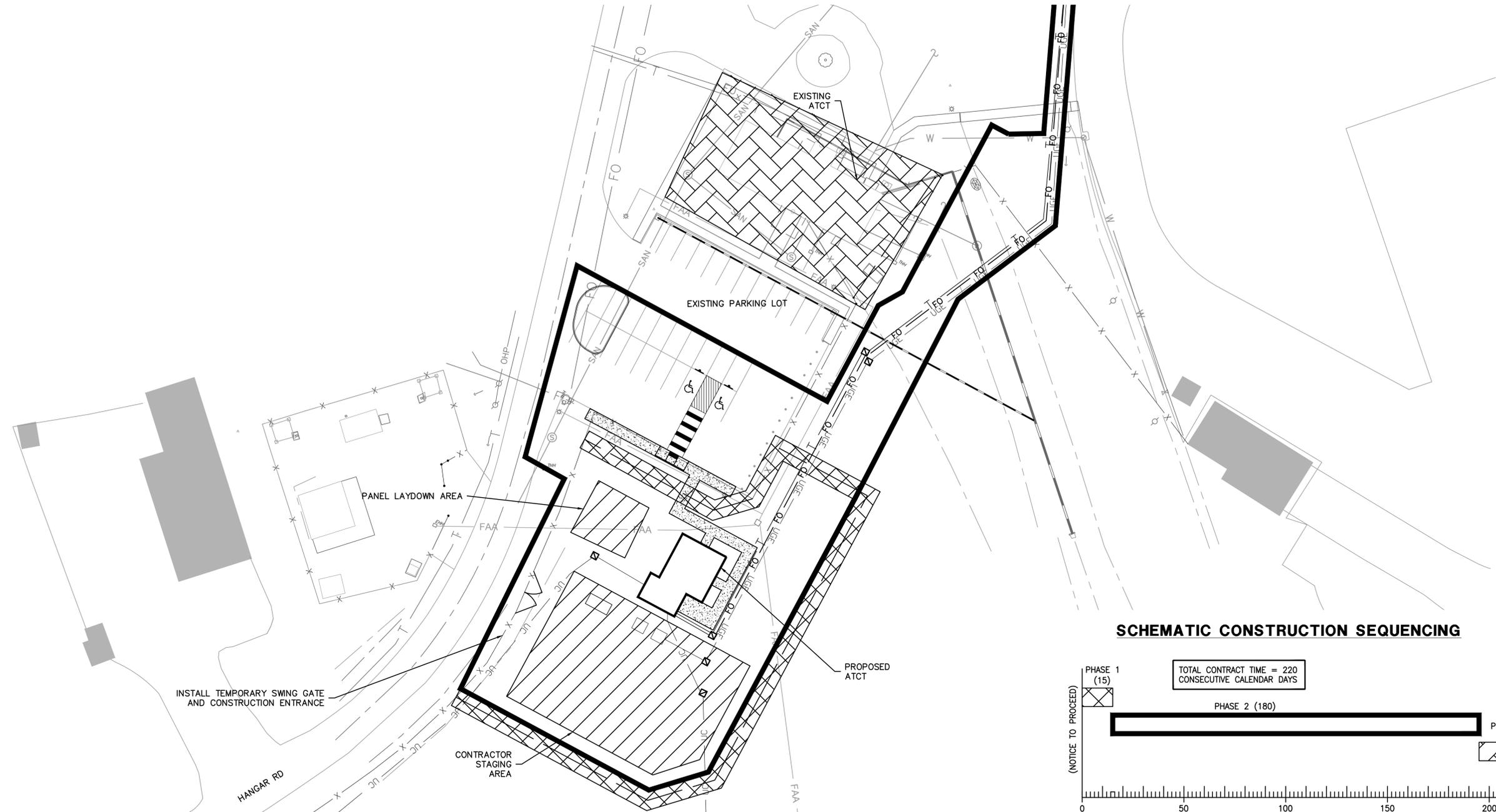
REVISIONS		
NO.	DESCRIPTION	DATE

DATE ISSUED: AUGUST 12, 2015
 REVIEWED BY: NJP
 DRAWN BY: SPC
 DESIGNED BY: SPC
 AEP PROJECT NUMBER
 222-0264-001
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 SHEET TITLE

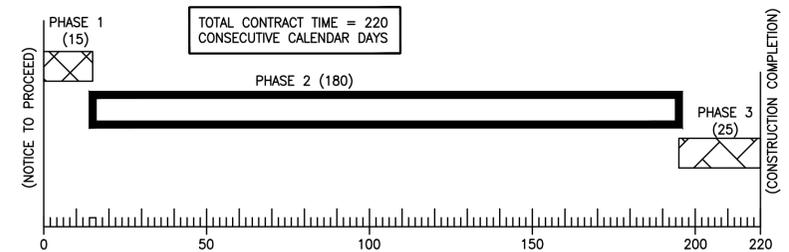
CONTRACT LAYOUT PLAN

SHEET NUMBER
C005

BID DOCUMENTS



SCHEMATIC CONSTRUCTION SEQUENCING



GENERAL PHASING NOTES

- IT IS THE CONTRACTOR'S RESPONSIBILITY TO BE APPRAISED OF, AND TO IMPLEMENT, THE GUIDELINES ESTABLISHED IN THE PROVISIONS UNDER SAFETY AND SECURITY.
- THE CONTRACTOR SHALL BE GIVING A SPECIFIC NUMBER OF CALENDAR DAYS TO PERFORM THE WORK AND IF THE CONTRACTOR DOES NOT COMPLETE THE CONSTRUCTION WITHIN THAT TIME FRAME, LIQUIDATED DAMAGES WILL BE ASSESSED UNLESS THE CONTRACTOR CAN SHOW JUST CAUSE FOR ANY DELAYS. IF THE ENGINEER OR AIRPORT FEELS THAT CONSTRUCTION IS PROCEEDING AT TOO SLOW A PACE, THE ENGINEER SHALL NOTIFY THE CONTRACTOR IN WRITING AND THE CONTRACTOR SHALL BE REQUIRED TO RESPOND IN WRITING JUSTIFYING THE IDENTIFIED DELAYS AND/OR LACK OF ADEQUATE EQUIPMENT. IT SHALL BE THE SOLE RESPONSIBILITY OF THE CONTRACTOR TO PROVIDE JUSTIFICATION FOR WAIVING OF ANY LIQUIDATED DAMAGES CHARGED TO THE CONTRACTOR.
- THE CONTRACTOR SHALL, AT ALL TIMES, COORDINATE HIS EFFORTS WITH THE ENGINEER. IF ANY PROBLEMS ARISE DURING THE CONSTRUCTION SEQUENCING, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE ENGINEER TO RESOLVE SAID PROBLEMS PRIOR TO CONTINUING THE WORK.
- THE CONTRACTOR SHALL PERFORM ALL FINAL CLEANUP WORK PRIOR TO A FINAL INSPECTION. THE CONTRACTOR SHALL ALSO CONTINUOUSLY CLEAN UP DURING EACH PHASE OF THE PROJECT.
- THE ACTIVE AIRPORT AREAS NEAR CONSTRUCTION ACTIVITIES WHICH HAS HAD ANY FOREIGN OBJECT DEBRIS (FOD) DEPOSITED BY AUTOMOBILE OR CONSTRUCTION EQUIPMENT OR BY WIND BLOWING DEBRIS OR MATERIALS ONTO THOSE ACTIVE AREAS SHALL BE CLEANED IMMEDIATELY. IT IS IMPERATIVE THAT NO DAMAGE BE DONE TO ANY AIRCRAFT DUE TO FOD. ANY DAMAGE DONE TO AIRCRAFT WHICH IS ATTRIBUTABLE TO FOD FROM THE CONSTRUCTION AREAS SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE WITH NO REIMBURSEMENT BY THE AIRPORT, ENGINEER OR THEIR AUTHORIZED REPRESENTATIVES.

- THE CONTRACTOR SHALL BE REQUIRED TO SUBMIT HIS RECOMMENDED FIELD OPERATIONS AREAS FOR STORAGE OF EQUIPMENT, SUPPLIES AND FIELD OFFICES TO THE ENGINEER AND AIRPORT MANAGEMENT AT THE PRECONSTRUCTION CONFERENCE FOR REVIEW, COMMENTS AND APPROVAL. ANY AREAS RECOMMENDED FOR STORAGE OF EQUIPMENT OVERNIGHT, FOR STORAGE OF FUELING FACILITIES, MATERIALS AND OFFICES SHALL BE APPROVED BY THE AIRPORT MANAGEMENT AND ENGINEER PRIOR TO MOBILIZATION OF ANY EQUIPMENT OR FIELD OFFICES AND CERTIFIED BY THE CONTRACTOR THAT THE FACILITIES MEET ALL APPLICABLE LOCAL, STATE, AND FEDERAL REQUIREMENTS.
- APPROPRIATE EROSION CONTROL MEASURES SHALL BE ACCOMPLISHED PRIOR TO BEGINNING THE RESPECTIVE PHASE. REMOVAL OF TEMPORARY EROSION CONTROL SHALL BE ACCOMPLISHED BY THE CONTRACTOR EITHER AT THE COMPLETION OF THE ASSOCIATED PHASE OR THEREAFTER AS DIRECTED BY THE RPR.
- THE COMPLETION OF ANY PHASE OF WORK AND SUBSEQUENT USAGE BY THE OWNER DOES NOT DEFINE FINAL ACCEPTANCE OF THE WORK IN THAT PHASE. WHEN ALL PHASES AND SUB-PHASES ARE COMPLETE AND A FINAL INSPECTION OF THE ENTIRE PROJECT HAS OCCURRED AND ALL ASSOCIATED PUNCH LIST ITEMS HAVE BEEN COMPLETED TO THE SATISFACTION OF THE AIRPORT MANAGEMENT AND ENGINEER, THEN THE ENTIRE PROJECT WILL BE ACCEPTED.
- THE CONTRACTOR SHALL SUBMIT TO THE ENGINEER AND OWNER A DETAILED CONSTRUCTION SCHEDULE 4 WEEKS PRIOR TO COMMENCING WORK. THE CONTRACT DOCUMENTS PROVIDE A CONSTRUCTION SEQUENCE AND SAFETY PARAMETERS WHICH MUST BE INCORPORATED INTO THE CONSTRUCTION SCHEDULE.
- ALL BARRICADES SHALL BE INSTALLED WHERE DIRECTED BY AIRPORT MANAGEMENT AND THE ENGINEER. THE LOCATIONS OF BARRICADES SHOWN ON THE PHASING PLAN REFLECT THE GENERAL VICINITY OF WHERE BARRICADES ARE TO BE INSTALLED. THE EXACT LOCATIONS SHALL BE DETERMINED PRIOR TO PLACEMENT.

- CRANE SHALL HAVE A CONSTRUCTION SAFETY FLAG AS DETAILED ON SHEET C004 AND SHALL BE LOWERED AT NIGHT IF NOT IN USE. IF CRANE IS IN USE DURING NIGHT TIME HOURS, AN OBSTRUCTION LIGHT SHALL BE INSTALLED AND OPERATING AT THE TOP OF THE CRANE.
- ALL MOTORIZED CONSTRUCTION VEHICLES AND EQUIPMENT WILL BE PARKED IN THE CONTRACTOR STAGING AREAS AT THE END OF EACH WORK DAY.

PHASE DESCRIPTIONS

PHASE 1: MOBILIZATION AND NEW SECURITY FENCING.

DURATION: 15 CONSECUTIVE CALENDAR DAYS.

WORK INCLUDES TEMPORARY CONSTRUCTION ITEMS, TRAFFIC CONTROL DEVICES, CONSTRUCTION ENTRANCE, PROPOSED SECURITY FENCE AROUND NEW AIR TRAFFIC CONTROL TOWER SITE.

AIRCRAFT OPERATIONAL LIMITATIONS: NONE.

PHASE 2: ATCT CONSTRUCTION AND UTILITY CONNECTIONS.

DURATION: 180 CONSECUTIVE CALENDAR DAYS.

WORK INCLUDES UTILITY CONNECTIONS, NEW SITE ACCESS AND OTHER SITE IMPROVEMENTS, NEW AIR TRAFFIC CONTROL TOWER CONSTRUCTION, AND WIND SENSOR MAST INSTALLATION.

AIRCRAFT OPERATIONAL LIMITATIONS: NONE.

ALL EQUIPMENT IN EXISTING ATCT TO BE RELOCATED TO NEW ATCT MUST BE COORDINATED IN ADVANCE AND SHALL NOT BE MOVED UNTIL APPROVED BY THE ENGINEER.

PHASE 3: EXISTING ATCT DEMOLITION AND COMMISSIONING OF NEW ATCT

DURATION: 25 CONSECUTIVE CALENDAR DAYS.

WORK INCLUDES COMMISSIONING OF NEW ATCT, DEMOLITION AND REMOVAL OF EXISTING ATCT, SECURITY FENCING, AND SITE RESTORATION, AND WIND SENSOR MAST CONNECTION.

AIRCRAFT OPERATIONAL LIMITATIONS: TEMPORARY CLOSURE OF RUNWAY 17-35

COMMISSIONING OF NEW ATCT IS INCLUSIVE OF CONTRACT BUT MAY EXTEND BEYOND CONTRACT TIME.

LEGEND

- CONTRACTOR STAGING AREA / PANEL LAYDOWN AREA
- PHASE 1 (15 CALENDAR DAYS)
- PHASE 2 (180 CALENDAR DAYS)
- PHASE 3 (25 CALENDAR DAYS)



SCALE: 1" = 30'
 30' 15' 0 30'

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REVISIONS

NO.	DESCRIPTION	DATE

DATE ISSUED: AUGUST 12, 2015
 REVIEWED BY: NJP
 DRAWN BY: SPC
 DESIGNED BY: SPC

AEP PROJECT NUMBER
222-0264-001

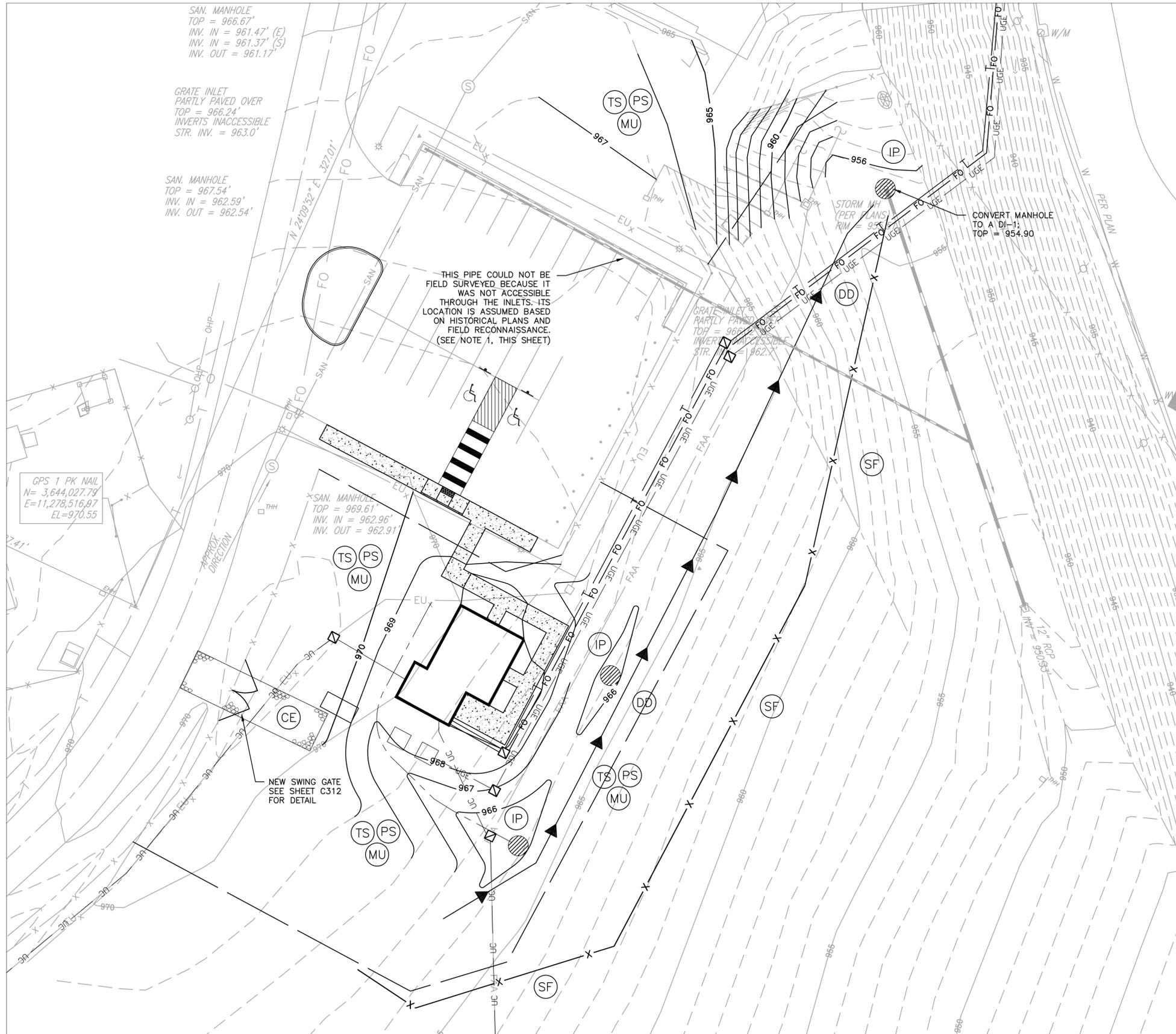
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 SHEET TITLE

PHASING PLAN AND NOTES

SHEET NUMBER

C006

BID DOCUMENTS



SAN. MANHOLE
 TOP = 966.67'
 INV. IN = 961.47' (E)
 INV. IN = 961.37' (S)
 INV. OUT = 961.17'

GRATE INLET
 PARTLY PAVED OVER
 TOP = 966.24'
 INVERTS INACCESSIBLE
 STR. INV. = 963.0'

SAN. MANHOLE
 TOP = 967.54'
 INV. IN = 962.59'
 INV. OUT = 962.54'

THIS PIPE COULD NOT BE FIELD SURVEYED BECAUSE IT WAS NOT ACCESSIBLE THROUGH THE INLETS. ITS LOCATION IS ASSUMED BASED ON HISTORICAL PLANS AND FIELD RECONNAISSANCE. (SEE NOTE 1, THIS SHEET)

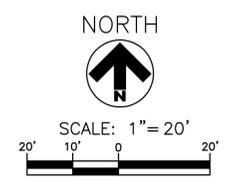
GRATE INLET
 PARTLY PAVED OVER
 TOP = 966.24'
 INVERTS INACCESSIBLE
 STR. INV. = 962.7'

CONVERT MANHOLE TO A DI-1;
 TOP = 954.90

GPS 1 PK NAIL
 N= 3,644,027.79'
 E=11,278,516.87'
 EL=970.55'

SAN. MANHOLE
 TOP = 969.61'
 INV. IN = 962.96'
 INV. OUT = 962.91'

NEW SWING GATE
 SEE SHEET C312
 FOR DETAIL



WARNING!!! THERE ARE A NUMBER OF AIRPORT, PUBLIC UTILITIES AND FAA LIGHTING, COMMUNICATIONS, UNDERGROUND CABLES AND PIPES TRAVERSING THE PROJECT AREA. THE ENGINEER HAS MADE EVERY ATTEMPT TO SHOW THE APPROXIMATE LOCATION OF ALL ITEMS. HOWEVER, THE ENGINEER IS NOT RESPONSIBLE FOR SHOWING OR LOCATING EVERY ITEM CURRENTLY IN PLACE. IT SHALL BE THE SOLE RESPONSIBILITY OF THE CONTRACTOR TO HAVE EVERY ITEM LOCATED, FLAGGED AND IDENTIFIED PRIOR TO STARTUP OF CONSTRUCTION. ANY DAMAGE DONE TO ANY OF THE EXISTING ITEMS SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR. THE CONTRACTOR SHALL IMMEDIATELY REPAIR ANY ITEM DAMAGED, CAUSED BY HIS ACTIONS, WITH NO ADDITIONAL COMPENSATION.

NOTES:
 1. DYE TESTING IN JUNE 2015 CONFIRMED LOCATION AND FLOW OF EXISTING PIPES NOT FIELD SURVEYED.

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AIR TRAFFIC CONTROL TOWER

HURT & PROFFITT INCORPORATED
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REVISIONS

NO.	DESCRIPTION	DATE

DATE ISSUED: AUGUST 12, 2015
 REVIEWED BY: BLC
 DRAWN BY: WCH
 DESIGNED BY: PSB
 AEP PROJECT NUMBER
 222-0264-001

SHEET TITLE
**PHASE 2
 EROSION AND
 SEDIMENT CONTROL**

SHEET NUMBER
C102

BID DOCUMENTS

NOTES

1. SEE SHEET C301 FOR PROPOSED SITE PLAN.
2. SEE BUILDING DEMOLITION SPECIFICATIONS FOR BUILDING REMOVAL AND ENVIRONMENTAL ABATEMENT INFORMATION.
3. SEE C-AB SHEET SERIES FOR EXISTING BUILDING DRAWINGS.
4. CONTRACTOR SHALL SALVAGE SIGNS MOUNTED ON EXISTING SECURITY FENCE TO BE REMOVED. SIGNS SHALL BE PLACED ON NEW SECURITY FENCE. COST ASSOCIATED WITH SIGN INSTALLATION SHALL BE INCLUDED IN THE COST OF THE PROPOSED SECURITY FENCING PAY ITEM.
5. REMOVAL OF UTILITIES, RETAINING WALL, FENCING, AND ALL OTHER ITEMS SERVING THE EXISTING ATCT SHALL BE INCLUDED IN THE COST FOR BUILDING DEMOLITION.
6. BORROW EXCAVATION AND SITE GRADING REQUIRED TO BRING THE DEMOLISHED TOWER AREA TO PROPOSED GRADE SHALL BE INCLUDED IN THE COST FOR BUILDING DEMOLITION.

LEGEND

-  CONCRETE PAVEMENT DEMOLITION
-  BUILDING DEMOLITION
-  ASPHALT PAVEMENT DEMOLITION

REVISIONS

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 DESIGNED BY: SPC

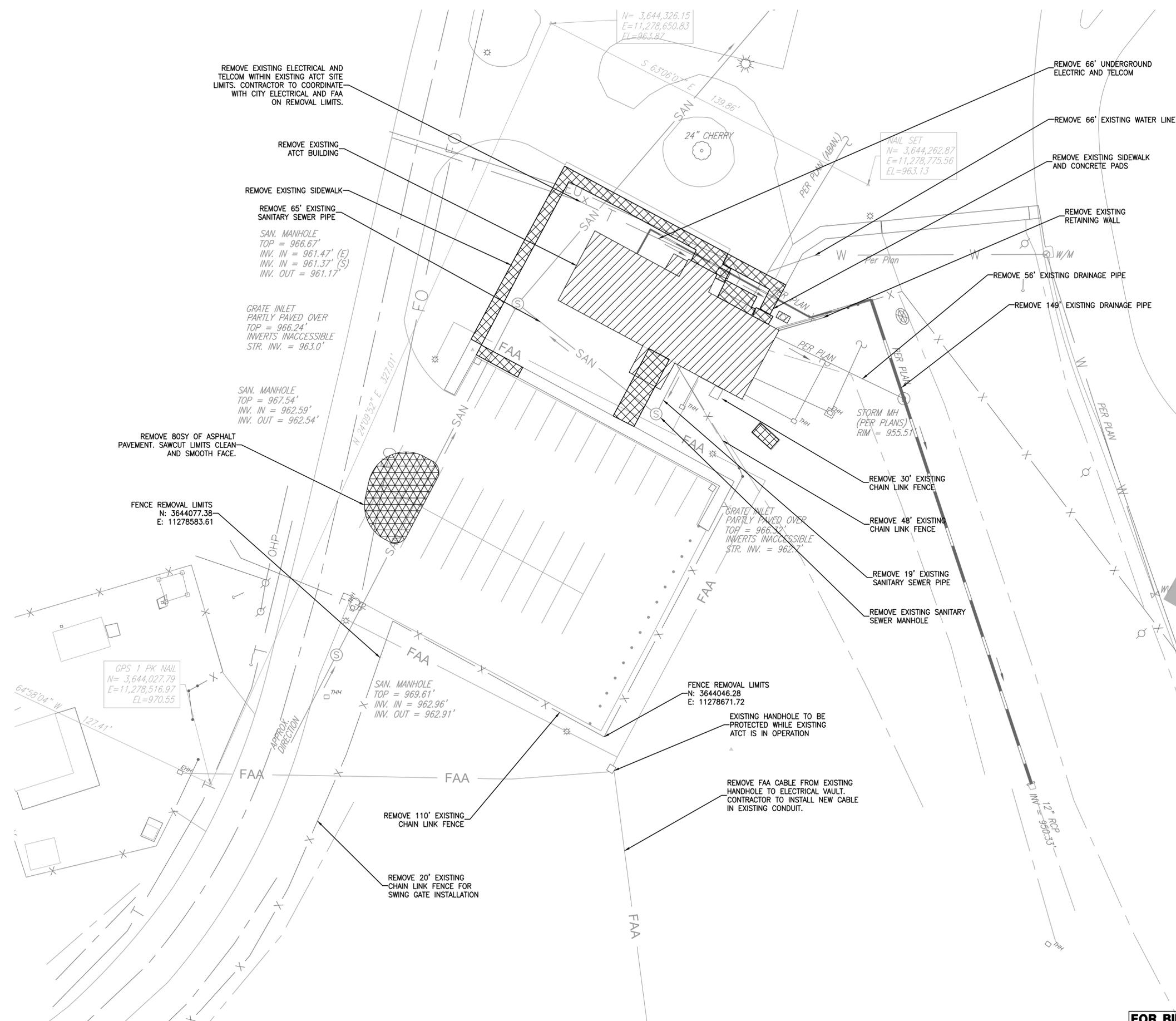
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 SHEET TITLE

DEMOLITION PLAN

SHEET NUMBER
C201

BID DOCUMENTS



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NOTES

1. SEE SHEET C200 FOR DEMOLITION PLAN.
2. SEE SHEET C401 FOR UTILITY PLAN AND PROFILE.
3. SEE SHEET C601 FOR ELECTRICAL SITE PLAN.
4. CONTRACTOR SHALL RESTORE EXISTING TOWER AREA WITH BACKFILL PER GEOTECHNICAL RECOMMENDATIONS, 4" TOPSOIL AND SEED AND MULCH.

LEGEND

-  PROPOSED SIDEWALK PAVEMENT
-  PROPOSED AOA FENCE
-  PROPOSED SANITARY SEWER
-  PROPOSED WATER MAIN
-  PROPOSED STORM SEWER
-  PROPOSED DRAINAGE MANHOLE
-  PROPOSED DRAINAGE INLET

REVISIONS

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 REVIEWED BY: BLC
 DRAWN BY: WCH
 DESIGNED BY: PSB

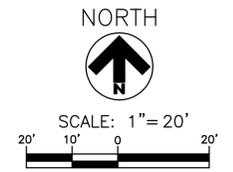
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PROPOSED GRADING PLAN

SHEET NUMBER
C302
 BID DOCUMENTS



WARNING!!! THERE ARE A NUMBER OF AIRPORT, PUBLIC UTILITIES AND FAA LIGHTING, COMMUNICATIONS, UNDERGROUND CABLES AND PIPES TRAVERSING THE PROJECT AREA. THE ENGINEER HAS MADE EVERY ATTEMPT TO SHOW THE APPROXIMATE LOCATION OF ALL ITEMS. HOWEVER, THE ENGINEER IS NOT RESPONSIBLE FOR SHOWING OR LOCATING EVERY ITEM CURRENTLY IN PLACE. IT SHALL BE THE SOLE RESPONSIBILITY OF THE CONTRACTOR TO HAVE EVERY ITEM LOCATED, FLAGGED AND IDENTIFIED PRIOR TO STARTUP OF CONSTRUCTION. ANY DAMAGE DONE TO ANY OF THE EXISTING ITEMS SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR. THE CONTRACTOR SHALL IMMEDIATELY REPAIR ANY ITEM DAMAGED, CAUSED BY HIS ACTIONS, WITH NO ADDITIONAL COMPENSATION.



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CG-12

GENERAL NOTES:

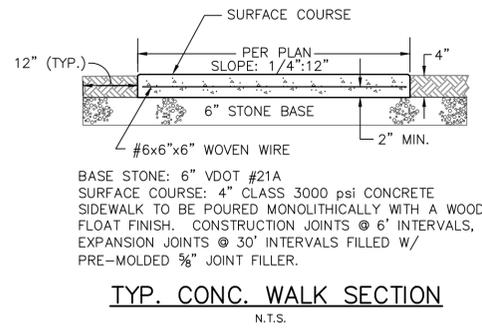
- THE DETECTABLE WARNING SHALL BE PROVIDED BY TRUNCATED DOMES.
- DETECTABLE WARNING TO BE CLASS A-3 CONCRETE (CLASS A-4 IF PRECAST) WITH SLIP RESISTANT INTEGRAL SURFACE COVERING THE FULL WIDTH OF THE RAMP FLOOR BY 2 FOOT IN LENGTH IN THE DIRECTION OF PEDESTRIAN TRAVEL. OTHER TYPES OF MATERIAL WITH THE TRUNCATED DOMES DETECTABLE WARNING MAY BE USED WITH THE APPROVAL OF THE ENGINEER.
- SLOPING SIDES OF CURB RAMP MAY BE POURED MONOLITHICALLY WITH RAMP FLOOR OR BY USING PERMISSIBLE CONSTRUCTION JOINT WITH REQUIRED BARS.
- IF RAMP FLOOR IS PRECAST, HOLES MUST BE PROVIDED FOR DOWEL BARS SO THAT ADJOINING FLARED SIDES CAN BE CAST IN PLACE AFTER PLACEMENT OF PRECAST RAMP FLOOR. PRECAST CONCRETE SHALL BE CLASS A-4.
- REQUIRED BARS ARE TO BE NO. 5 X 8" PLACED 1' CENTER TO CENTER ALONG BOTH SIDES OF THE RAMP FLOOR, MID-DEPTH OF RAMP FLOOR. MINIMUM CONCRETE COVER 1/2".
- CURB / CURB AND GUTTER SLOPE TRANSITIONS ADJACENT TO CURB RAMPS ARE INCLUDED IN PAYMENT FOR CURB / CURB AND GUTTER.
- CURB RAMPS ARE TO BE LOCATED AS SHOWN ON THE PLANS OR AS DIRECTED BY THE ENGINEER. THEY ARE TO BE PROVIDED AT INTERSECTIONS WHEREVER AN ACCESSIBLE ROUTE WITHIN THE RIGHT OF WAY OF A HIGHWAY FACILITY CROSSES A CURB REGARDLESS OF WHETHER SIDEWALK IS EXISTING, PROPOSED, OR NONEXISTENT. THEY MUST BE LOCATED WITHIN PEDESTRIAN CROSSWALKS AS SHOWN ON PLANS OR AS DIRECTED BY THE ENGINEER, AND SHOULD NOT BE LOCATED BEHIND VEHICLE STOP LINES, EXISTING LIGHT POLES, FIRE HYDRANTS, DROP INLETS, ETC. ACCESSIBLE ROUTES PROVIDE A CONTINUOUS UNOBSTRUCTED, STABLE, FIRM AND SLIP RESISTANT PATH CONNECTING ALL ACCESSIBLE ELEMENTS OF A FACILITY THAT CAN BE APPROACHED, ENTERED AND USED BY PEDESTRIANS.
- RAMPS MAY BE PLACED ON RADIAL OR TANGENTIAL SECTIONS PROVIDED THAT THE CURB OPENING IS PLACED WITHIN THE LIMITS OF THE CROSSWALK AND THAT THE SLOPE AT THE CONNECTION OF THE CURB OPENING IS PERPENDICULAR TO THE CURB.
- TYPICAL CONCRETE SIDEWALK IS 4" THICK. WHEN THE ENTRANCE RADI CANNOT ACCOMMODATE THE TURNING REQUIREMENTS OF ANTICIPATED HEAVY TRUCK TRAFFIC, REFER TO STANDARD CG-15, COMMERCIAL ENTRANCE (HEAVY TRUCK TRAFFIC) FOR CONCRETE DEPTH.
- WHEN CURB RAMPS ARE USED IN CONJUNCTION WITH A SHARED USE PATH, THE MINIMUM WIDTH SHALL BE THE WIDTH OF THE SHARED USE PATH.
- COST OF DETECTABLE WARNING SURFACE SHALL BE INCLUDED IN THE PAY ITEM 033000 SIDEWALK.

TRUNCATED DOME DETAIL

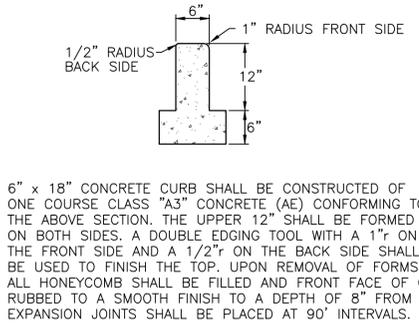
DETECTABLE WARNING DETAIL

CG-12 DETECTABLE WARNING SURFACE GENERAL NOTES
VIRGINIA DEPARTMENT OF TRANSPORTATION

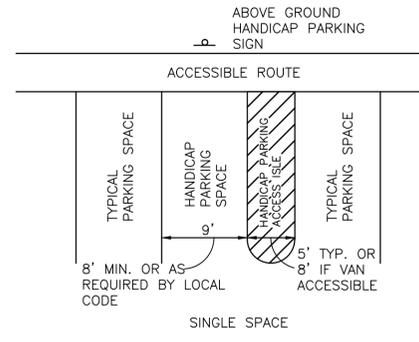
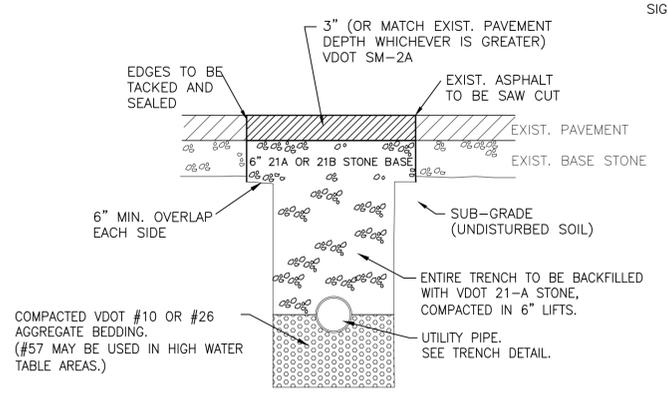
SHEET 1 OF 4	SPECIFICATION REFERENCE 105 502
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NOTE:
COST FOR SUB-BASE PREPARATION AND BASE COURSE SHALL BE INCLUDED IN COST FOR ITEM 033000 SIDEWALK

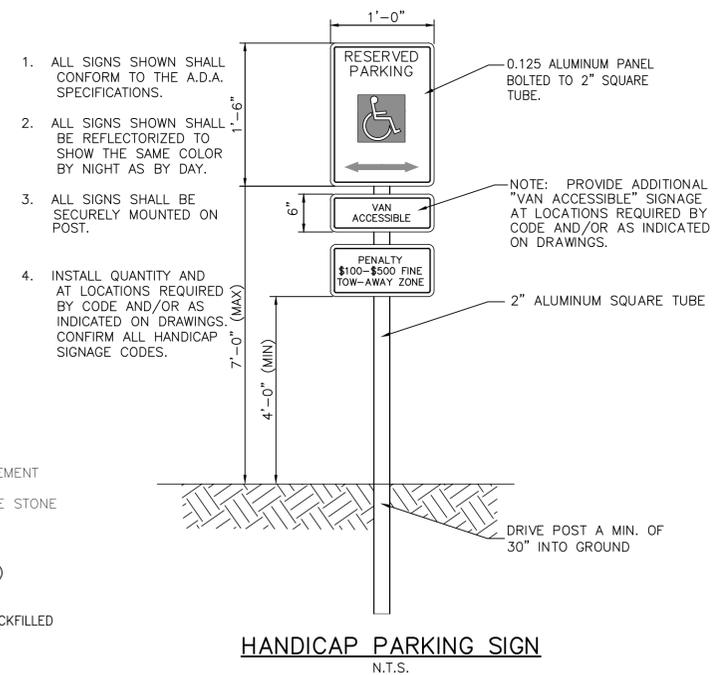


NOTE:
ALL BASE COURSE PREPARATION ASSOCIATED WITH PROPOSED CURB CONSTRUCTION SHALL BE INCLUDED IN THE COST FOR ITEM 033000 6\" CURB



HANDICAP PARKING DETAIL

- NOTES:
- TWO ADJACENT HANDICAP SPACES MAY SHARE THE SAME PARKING ACCESS ISLE.
 - HANDICAP PARKING SPACES AND ACCESS ISLES SHALL BE LEVEL WITH SURFACE SLOPES NOT EXCEEDING 2% IN ANY DIRECTION.
 - 1 OUT OF EVERY 8 HANDICAP SPACES, BUT NOT LESS THAN 1, SHALL BE VAN ACCESSIBLE AND SHALL HAVE PROPER SIGNAGE NOTING "VAN ACCESSIBLE".
 - HANDICAP AND VAN ACCESSIBLE PARKING SPACES SHALL HAVE PROPER SIGNAGE AS PER LOCAL CODE AND A.D.A. REQUIREMENTS.
 - HANDICAP SPACES SHALL COMPLY WITH LOCAL CODES AND A.D.A. REQUIREMENTS.



- ALL SIGNS SHOWN SHALL CONFORM TO THE A.D.A. SPECIFICATIONS.
- ALL SIGNS SHOWN SHALL BE REFLECTORIZED TO SHOW THE SAME COLOR BY NIGHT AS BY DAY.
- ALL SIGNS SHALL BE SECURELY MOUNTED ON POST.
- INSTALL QUANTITY AND AT LOCATIONS REQUIRED BY CODE AND/OR AS INDICATED ON DRAWINGS. CONFIRM ALL HANDICAP SIGNAGE CODES.

REVISIONS		
NO.	DESCRIPTION	DATE

DATE ISSUED: AUGUST 12, 2015
REVIEWED BY: BLC
DRAWN BY: WCH
DESIGNED BY: PSB
AEP PROJECT NUMBER
222-0264-001
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SHEET TITLE

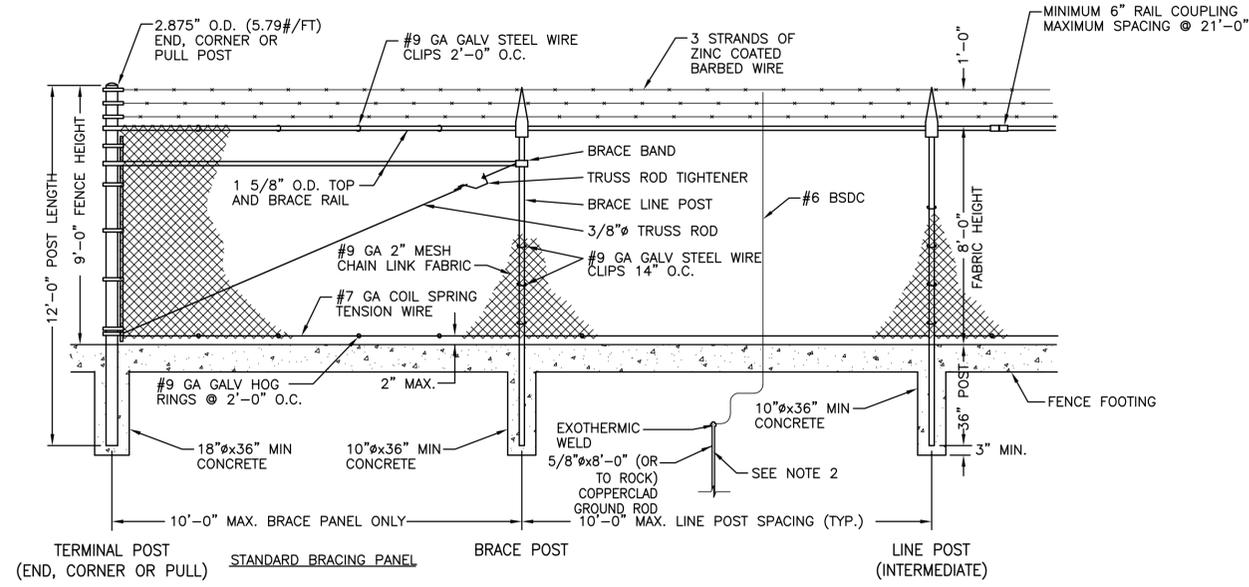
SITE DETAILS

SHEET NUMBER
C311

BID DOCUMENTS

CHAIN LINK FENCE NOTES:

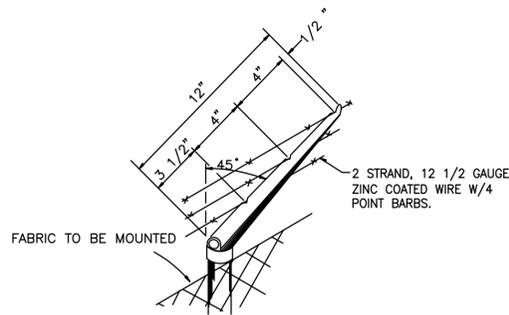
- FENCE SHALL MEET THE REQUIREMENTS INCLUDED WITHIN NCDOT STANDARD DRAWING 866.01 FOR G1 SYSTEMS UNLESS MODIFIED BY THE ABOVE DETAILS. CONSTRUCTION METHODS SHALL BE IN ACCORDANCE WITH THESE DRAWINGS.
- FENCE SHALL BE 6' HIGH WITH 2"x2" BLACK VINYL COATED 9 GAUGE WIRE MESH. ALL DIMENSIONS, SIZES, GAUGES, WEIGHTS OR THICKNESSES INCLUDED SHALL BE CONSIDERED MINIMUM ACCEPTABLE DIMENSIONS.
- ALL POSTS SHALL BE CAPPED.
- CONCRETE SHALL BE PER NCDOT SPECIFICATION SECTION 866.
- WIRE OR FABRIC ON FENCE SHALL BE ON THE SIDE OF POSTS AWAY FROM THE PROPERTY.
- FENCE LINES SHALL BE CLEARED OF ALL OBSTRUCTIONS AND SMOOTH GRADED TO THE GENERAL CONTOUR OF THE ADJACENT GROUND FOR A MINIMUM WIDTH OF 5' EACH SIDE OF FENCE LINE. STUMPS AND ROOTS NOT INTERFERING WITH FENCE CONSTRUCTION SHALL BE CHIPPED TO GROUND LEVEL.
- THE FENCE SHALL BE CONSTRUCTED VERTICAL, STRAIGHT AND TRUE TO LINE, THE LONGITUDINAL GRADIENT SHALL PARALLEL THE GENERAL SLOPE OF THE GROUND.
- GROUND RODS SHALL BE INSTALLED ON 500' MAXIMUM SPACING ALONG CONTINUOUS FENCE, AND AT POWER LINE CROSSINGS, INCIDENTAL TO THE COST OF THE FENCE.
- EACH SWING GATE SHALL BE GROUNDED WITH ONE GROUND ROD ON THE HINGE SIDE GATE POST.
- EACH GATE LEAF FRAME SHALL BE CONNECTED TO THE GATE POST BY A BRAIDED FLEXIBLE COPPER STRAP AND APPROVED CLAMP. GROUND RODS SHALL BE 3/4" DIA X 10' LENGTH COPPER CLAD STEEL ROD WITH A MAXIMUM RESISTANCE OF 10 OHMS. GROUND CABLE SHALL BE A MINIMUM #6 BARE SOLID TINNED COPPER. GROUND CONDUCTOR SHALL BE EXOTHERMICALLY WELDED TO THE GROUND ROD.
- GROUND CONDUCTOR SHALL BE CONNECTED TO THE FENCE FABRIC WITH SPLIT BOLTS OR NON REVERSIBLE COMPRESSION CONNECTOR AT THREE LOCATIONS PER GROUND ROD. GROUND CONDUCTOR SHALL BE CONNECTED TO THE FENCE POSTS WITH APPROVED GROUND CLAMPS, LUGS OR EXOTHERMIC WELD.
- IF GROUNDING IS REQUIRED THROUGH EXISTING PAVEMENT, CONTRACTOR SHALL CORE A 4" HOLE THRU THE EXISTING SURFACE TO ACCOMMODATE THE INSTALLATION OF THE GROUND ROD. CONTRACTOR SHALL BE RESPONSIBLE FOR FILLING CORED HOLE.
- REFER TO THE FENCE GROUNDED DETAILS FOR ADDITIONAL INFORMATION REGARDING THE INSTALLATION OF PROTECTIVE GROUNDED OF FENCING.



1 PERMANENT AOA FENCE DETAIL (GRASSED AREAS)
SCALE: N.T.S.

PERMANENT AOA FENCE NOTES:

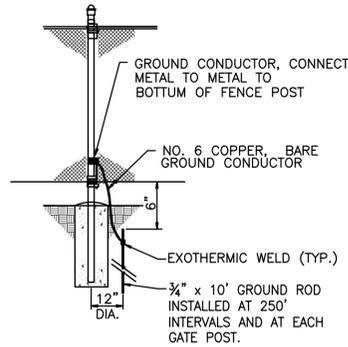
- TOP OF 3/4" x 10' COPPER CLAD GROUND ROD SHALL BE 1 FOOT BELOW GRADE.
- SECTIONS OF FENCE, REGARDLESS OF LENGTH, EACH SIDE OF ANY GATES, GAPS AT SPLICES OR PLASTIC CONNECTORS, SHALL ALL BE GROUNDED INDEPENDENTLY UTILIZING GROUND CONNECTORS. THE #6 BSDC SHALL BE ATTACHED TO THE TOP, CENTER, AND BOTTOM OF THE FENCE FABRIC AND TO EACH STRAND OF BARBED WIRE BY A BIMETALLIC FASTENER. THE TOP OF 5/8" COPPER CLAD GROUND ROD SHALL BE 6 INCHES BELOW GRADE. THE MAXIMUM SPACING SHALL BE 500'.



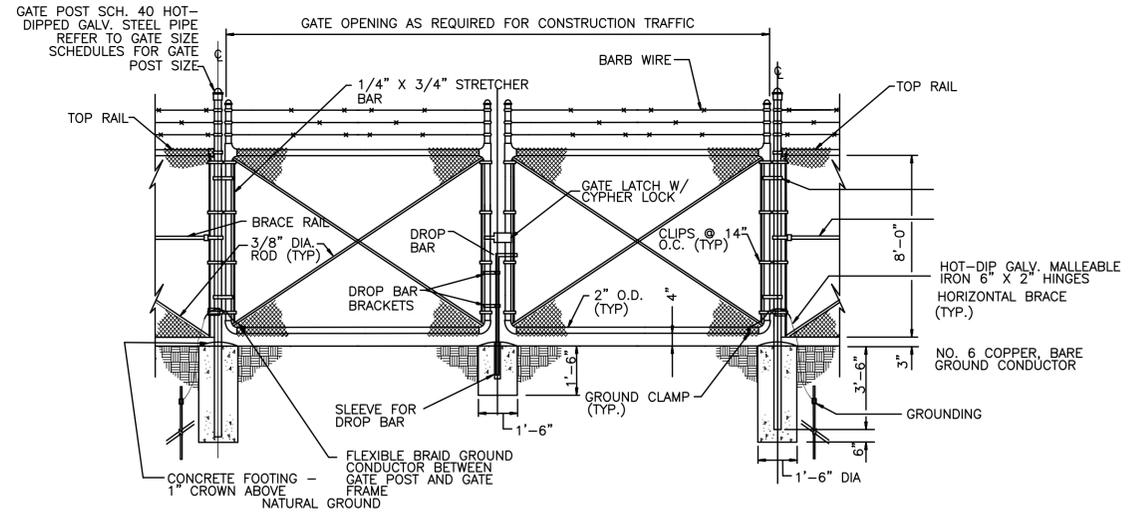
2 BARB WIRE MOUNTING DETAIL
SCALE: NTS

NOTES:

- PULL POSTS WITH APPROPRIATE DIAGONAL TRUSS RODS AND TRUNBUCKLES SHALL BE INSTALLED EVERY 350' TO MAINTAIN TIGHT TENSION ON THE FABRIC AND BARBED WIRE
- ELECTRICAL GROUNDS SHALL BE CONSTRUCTED AT 300' INTERVALS AND WHERE ANY POWER LINE PASSES OVER THE FENCE



3 TYPICAL GROUNDED DETAIL
SCALE: NTS



4 SWING GATE DETAIL (UNPAVED AREAS)
SCALE: N.T.S.

TEMPORARY SWING GATE NOTES:

- CONTRACTOR SHALL INSTALL TEMPORARY SWING GATE FOR CONSTRUCTION TRAFFIC ACCESS WHERE SHOWN ON THE CONTRACT LAYOUT PLAN.
- CONTRACTOR SHALL REMOVE EXISTING FENCING AND INSTALL TEMPORARY ACCESS GATE. AFTER CONSTRUCTION IS COMPLETE, THE CONTRACTOR SHALL REPLACE THE TEMPORARY CONSTRUCTION GATE WITH SECURITY FENCING THAT MATCHES THE EXISTING SECURITY FENCE.
- ALL COSTS ASSOCIATED WITH THE MATERIALS, LABOR, MAINTENANCE AND REMOVAL OF TEMPORARY GATES AND FENCING SHALL BE INCIDENTAL TO THE F-162-5.4 TEMPORARY SWING GATE PER EACH.

GATE SIZE SCHEDULE:

SINGLE LEAF GATES		DOUBLE LEAF GATES	
OPENING	GATE POST SIZE	OPENING	GATE POST SIZE
3'-6"	2.875" O.D.	8'-12"	2.875" O.D.
7'-10"	4" O.D.	14'-24"	4" O.D.
11'-12"	4" O.D.	26'-36"	6.625" O.D.
13'-18"	6.625" O.D.	38'-40"	8.625" O.D.
19'-20"	8.625" O.D.		

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4308 WARDS ROAD
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AIR TRAFFIC CONTROL TOWER

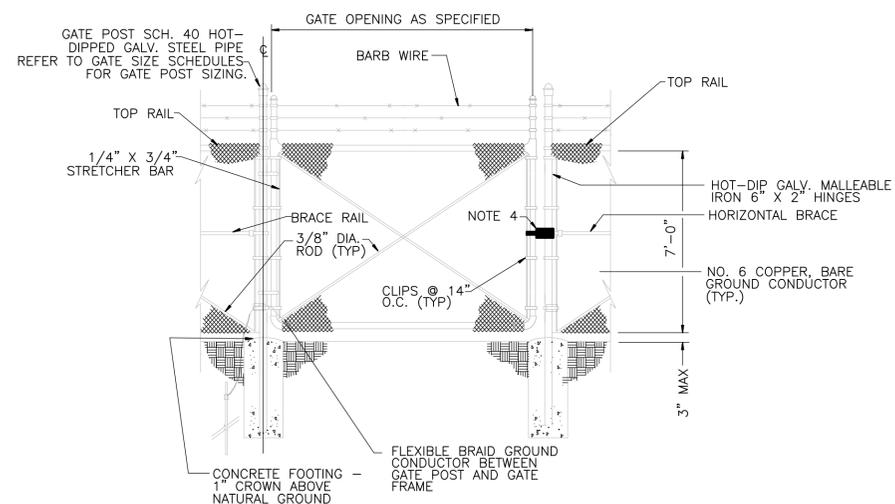
REVISIONS

NO.	DESCRIPTION	DATE

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SECURITY FENCING AND GATE DETAILS (SHEET 1 OF 2)

SHEET NUMBER
C312
BID DOCUMENTS

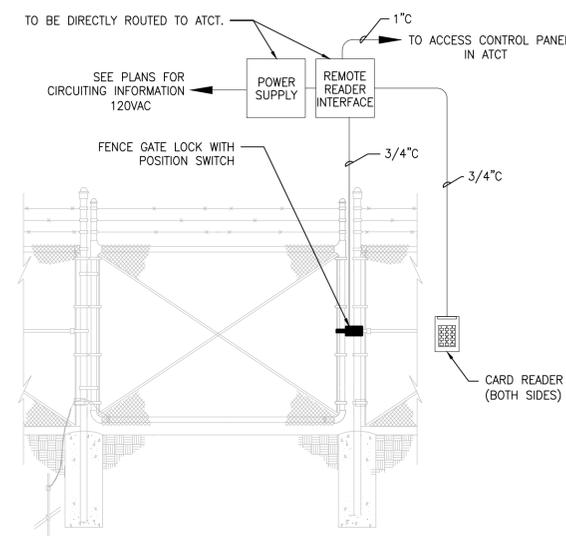


1 SINGLE-LEAF SWING PEDESTRIAN GATE

SCALE: NTS
 C313

NOTES:

1. 5' PEDESTRIAN SWING GATE TO HAVE SPRING LOADED CLOSE DEVICE.
2. PERSONNEL MUST USE CARD ACCESS TO EXIT ATCT AREA.
3. PERSONNEL MUST USE CARD ACCESS AND KEYPAD TO ENTER ATCT AREA.
4. LOCK AND CARD READER. SEE DETAIL THIS SHEET FOR ADDITIONAL INFORMATION.



2 PEDESTRIAN GATE LOCK DETAIL

SCALE: NONE
 C313

GATE ACCESS CONTROL NOTES:

1. CONTRACTOR SHALL INTERFACE WITH GATE OPERATOR CONTROL PANEL. CONNECT GATE OPERATOR GATE POSITION SWITCH RELAY TO ACCESS CONTROL SYSTEM INPUT OR PROVIDE WIDE GAP BALANCED MAGNETIC SWITCH TO MONITOR GATE POSITION.
2. GATE SHALL OPEN AT UNSECURED OR SECURED SIDE WHEN VALID ACCESS CARD IS PRESENTED AT CARD READER. LOOP DETECTOR ACTIVATES CARD READER. SETUP OPEN/CLOSE SEQUENCE AND REQUIREMENTS WITH OWNER. COORDINATE CARD READER SELECTION WITH TERMINAL PACKAGE ACCESS CONTROL SYSTEM.
3. PROVIDE ALL CABLING, CONDUIT, INTERCONNECTIONS, SYSTEM SET UP, ETC. AS REQUIRED BY MFR. REQUIREMENTS FOR A FULLY OPERATIONAL SYSTEM. PROVIDE SHOP DRAWINGS SHOWING ALL COMPONENTS AND CONNECTIONS.

REVISIONS		
NO.	DESCRIPTION	DATE

DATE ISSUED: AUGUST 12, 2015
 REVIEWED BY: NJP
 DRAWN BY: SPC
 DESIGNED BY: SPC

AEP PROJECT NUMBER
 222-0264-001

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 SHEET TITLE

**SECURITY FENCING
 AND GATE
 DETAILS
 (SHEET 2 OF 2)**

SHEET NUMBER
C313

**BID
 DOCUMENTS**

NOTES

- SEE SHEET C200 FOR DEMOLITION PLAN.
- SEE SHEET C401 FOR UTILITY PLAN AND PROFILE.
- SEE SHEET C601 FOR ELECTRICAL SITE PLAN.
- CONTRACTOR SHALL RESTORE EXISTING TOWER AREA WITH BACKFILL PER GEOTECHNICAL RECOMMENDATIONS, 4" TOPSOIL AND SEED AND MULCH.

STRUCTURE TABLE

- CONNECT TO EXISTING
- VDOT MH-1
 TOP = 965.2
 INV = 959.50 (24" HDPE IN)
 INV = 959.50 (8" HDPE OUT-CONTROL ORIFICE)
 INV = 962.10 (8" HDPE OUT-OVERFLOW)
- VDOT DI-1
 TOP = 965.0
 INV = 961.30 (12" HDPE IN)
 INV = 961.80 (6" PVC IN)
 INV = 960.10 (24" HDPE OUT)
- VDOT DI-1
 TOP = 965.00
 INV = 962.00 (12" HDPE OUT)
- CLEANOUT
 TOP = 968.9
 INV = 964.40 (6" PVC)

PIPE TABLE

- | | |
|-----|-------------------------------|
| 1-2 | 16LF± - 8" HDPE @ 2.0% (MIN.) |
| 2-3 | 120LF - 24" HDPE @ 0.5% |
| 3-4 | 70LF - 12" HDPE @ 1.0% |
| 3-5 | 65LF - 6" PVC @ 4.0% |

LEGEND

-  PROPOSED SIDEWALK PAVEMENT
-  PROPOSED AOA FENCE
-  PROPOSED SANITARY SEWER
-  PROPOSED WATER MAIN
-  PROPOSED STORM SEWER
-  PROPOSED DRAINAGE MANHOLE
-  PROPOSED DRAINAGE INLET

REVISIONS

NO.	DESCRIPTION	DATE

DATE ISSUED: AUGUST 12, 2015

REVIEWED BY: BLC

DRAWN BY: WCH

DESIGNED BY: PSB

AEP PROJECT NUMBER
222-0264-001

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SHEET TITLE

**STORMWATER
 PLAN**

SHEET NUMBER

C321

**BID
 DOCUMENTS**



WARNING!!!
 THERE ARE A NUMBER OF AIRPORT, PUBLIC UTILITIES AND FAA LIGHTING, COMMUNICATIONS, UNDERGROUND CABLES AND PIPES TRAVERSING THE PROJECT AREA. THE ENGINEER HAS MADE EVERY ATTEMPT TO SHOW THE APPROXIMATE LOCATION OF ALL ITEMS. HOWEVER, THE ENGINEER IS NOT RESPONSIBLE FOR SHOWING OR LOCATING EVERY ITEM CURRENTLY IN PLACE. IT SHALL BE THE SOLE RESPONSIBILITY OF THE CONTRACTOR TO HAVE EVERY ITEM LOCATED, FLAGGED AND IDENTIFIED PRIOR TO STARTUP OF CONSTRUCTION. ANY DAMAGE DONE TO ANY OF THE EXISTING ITEMS SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR. THE CONTRACTOR SHALL IMMEDIATELY REPAIR ANY ITEM DAMAGED, CAUSED BY HIS ACTIONS, WITH NO ADDITIONAL COMPENSATION.

FOR BIDDING ONLY-NOT FOR CONSTRUCTION